



Valencia Commerce Center

PLANNING NOTEBOOK
Draft April 2025



Valencia Commerce Center Planning Notebook

April 2025

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Chapter One

Introduction

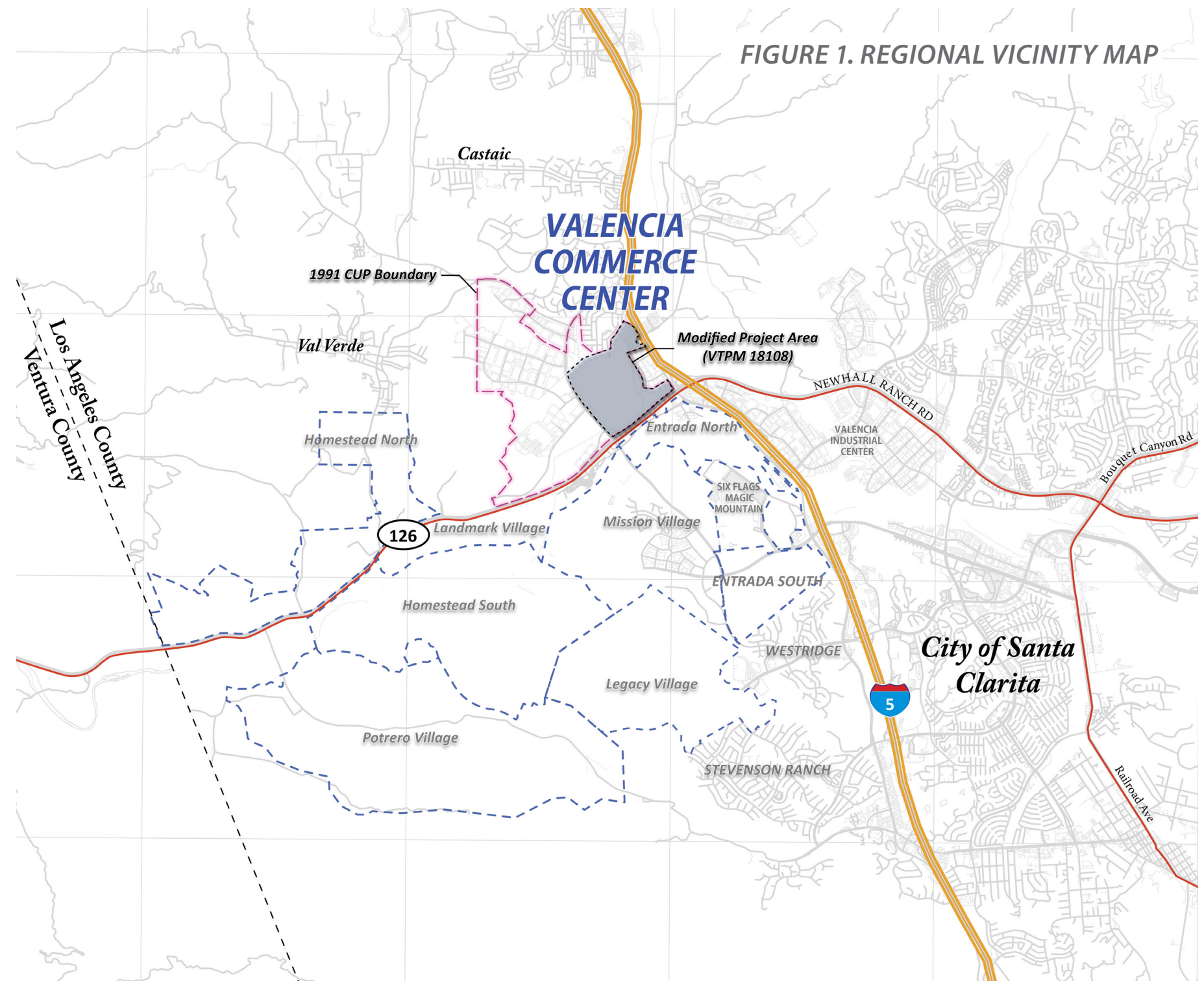


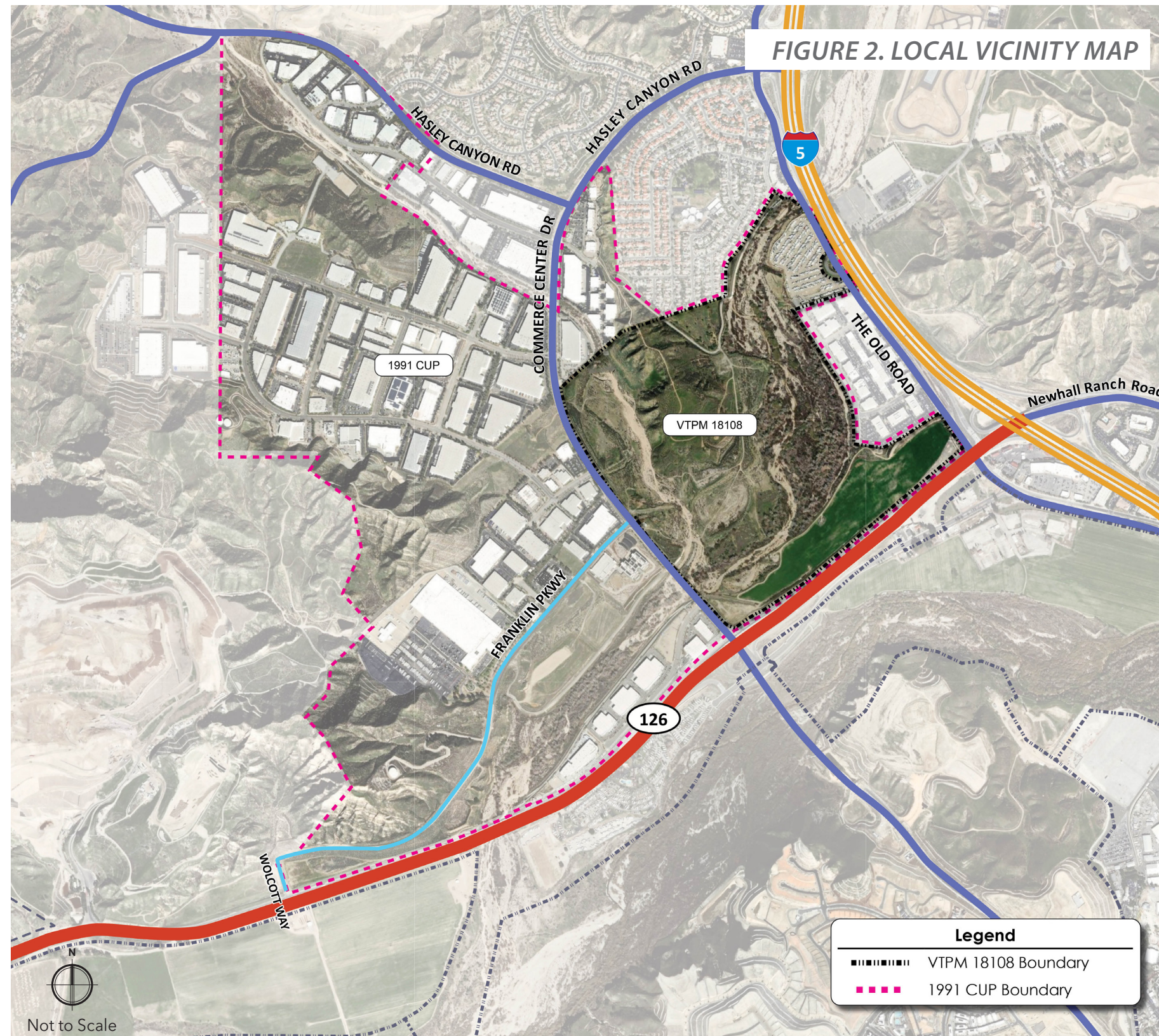
1.1 Community Vision Statement

The Valencia Commerce Center (VCC) is envisioned to be a high-level-of-design development that will be consistent with the standards set forth within other portions of Valencia. Ultimately the site will represent a positive contribution to the mosaic of villages woven throughout the Santa Clarita Valley.

It is part of the original Newhall Ranch area and falls under the authority of the master Conditional Use Permit (CUP) approved in 1991. VCC will be developed with the same care and quality as all of the other communities built within Valencia on the ranch property. The development will incorporate the commitment to implement the net zero greenhouse program through a combination of on- and off-site emissions; reducing measures discussed further below in Section 2.11, Net Zero GHG Community of this Planning Notebook. The benefits of a net zero GHG community are not limited to the reduction of greenhouse gas emissions; instead, the measures that will be implemented to reduce the community's greenhouse gas emissions also will serve to reduce the community's generation of criteria air pollutants, quantity of vehicle miles traveled and demand for energy-related resources.

The Valencia Commerce Center is located to the west of Interstate 5 (I-5) and The Old Road, north of State Route 126 (SR-126), and east of Commerce Center Drive, see Figures 1 and 2.



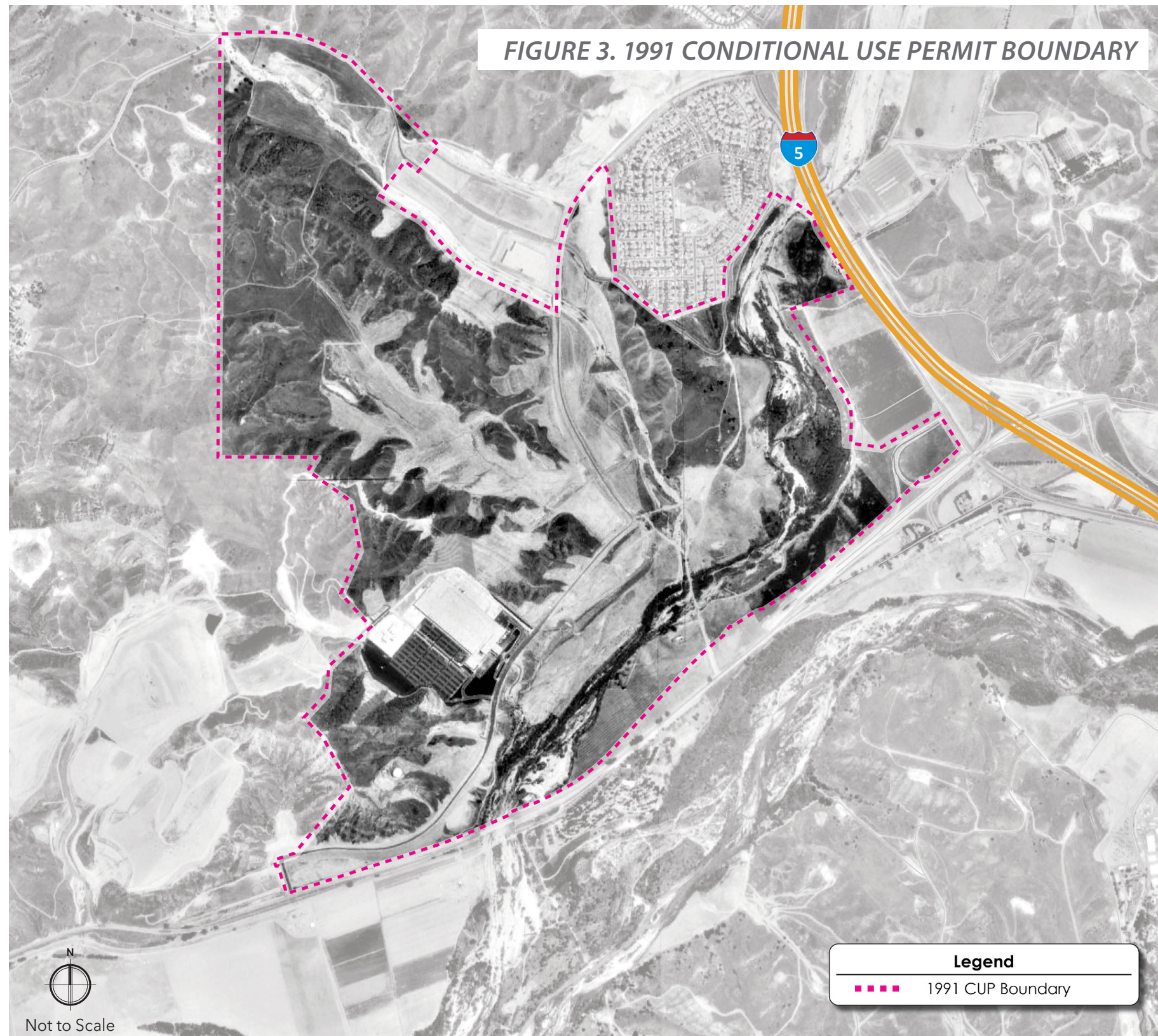


1.2 Planning Principles

The overall objectives guiding the planning vision for the Valencia Commerce Center is to create a development which will be known throughout California as a premier location for business, and locally as an asset to the Santa Clarita Valley. In addition, the Commerce Center should be viewed as a type of community, one oriented towards businesses, its environment and their employees. The following Planning Principles will help guide the project to obtain its objectives:

1. Create a land plan which meets the needs of users for a variety of lot sizes and locations, and which provides flexibility to combine lots to serve major users and to meet market needs.
2. In addition to the predominant industrial and office uses, provide space for retail and service commercial uses and recreation.
3. Continue the provisions of the approved Valencia Commerce Center Conditional Use Permit.
4. Generate jobs for the adjacent communities and support the local economy through development of approximately 3.4 million square feet of industrial and commercial business park development.
5. Buffer the Commerce Center uses from Hasley Canyon Road and residential homes with techniques such as grade differences, berms, walls and landscaping as appropriate. Use changes in topography to create business identity where the Center is seen from the Freeway and Highway 126.
6. Provide a network of sidewalks and bicycle and jogging trails within the Center to promote employee recreation and pedestrian circulation as an alternative to the automobile. Provide for future connections of the internal pedestrian system to the Santa Clarita regional trail.
7. Restore and enhance the riparian vegetation and appearance of Castaic Creek to maintain biotic values and create a visual asset for the Center.
8. Develop a comprehensive and coordinated plan for landscape, hardscape, monumentation and business signage which creates a strong identity for the Valencia Commerce Center and enhances its aesthetic quality and function.





1.3 1991 VCC Conditional Use Permit

The Valencia Commerce Center was originally approved for development by Los Angeles County through the issuance of various entitlements (including CUP 87-360) and certification of an Environmental Impact Report (EIR) (SCH No. 1987-123005) in 1991. The existing VCC entitlement allows a total of approximately 12.6 million square feet of industrial/business park space at buildout.

In 2017, a State-certified EIR was approved for the undeveloped portion of the VCC development area. The Newhall Ranch Resource Management and Development Plan and Spineflower Conservation Plan (RMDP/SCP) and the State-Certified EIR (SCH No. 2000011025) assumed up to 3.4 million square feet of non-residential floor area in the Modified Project development area.

The VCC Modified Project area consists of approximately 328.5 acres of an undeveloped portion of the partially completed VCC industrial/business park center located west of I-5 and north of State Route 126 (SR-126). The VCC project area is generally comprised of vacant land, RV storage, and some agricultural uses adjacent to SR-126. The site is bisected by Hasley Creek and Castaic Creek, and elevations range from approximately 980 to 1,200 feet AMSL.

The VCC project area is located within the existing, built-out portions of the VCC industrial/business park center and surrounded by existing single-family residential and industrial uses to the north. Existing industrial uses, The Old Road, and the I-5 freeway are located to the east. SR-126 is located to the south, and the previously developed portion of the VCC industrial/business park is located to the west.

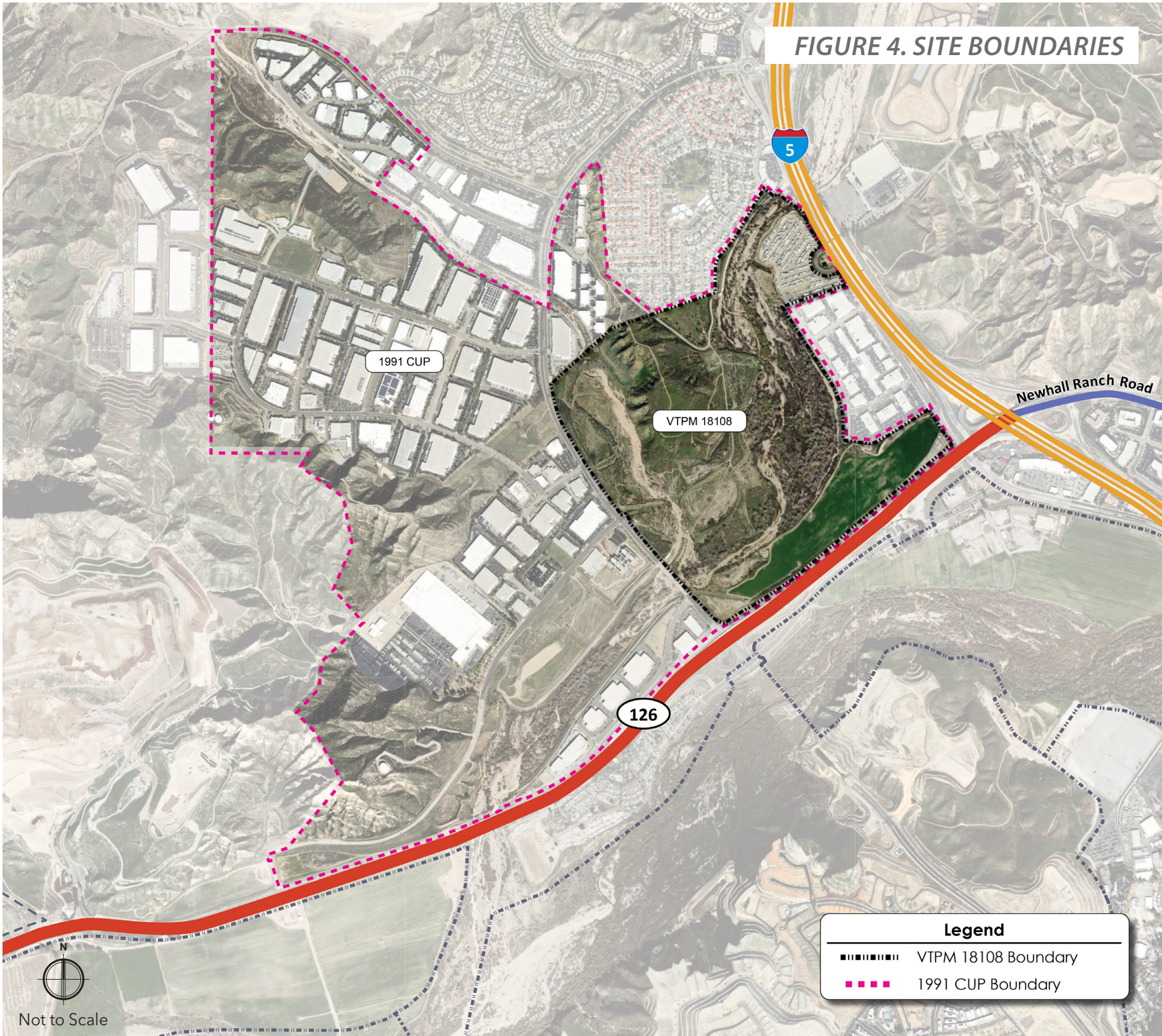
The majority of the land areas within the Modified Project area of VCC would be developed with industrial/business park uses, with open space provided along the banks of Hasley and Castaic Creeks. The proposed development would comply with the Development Program overlay, including design provisions associated with CUP 87-360, as applicable.

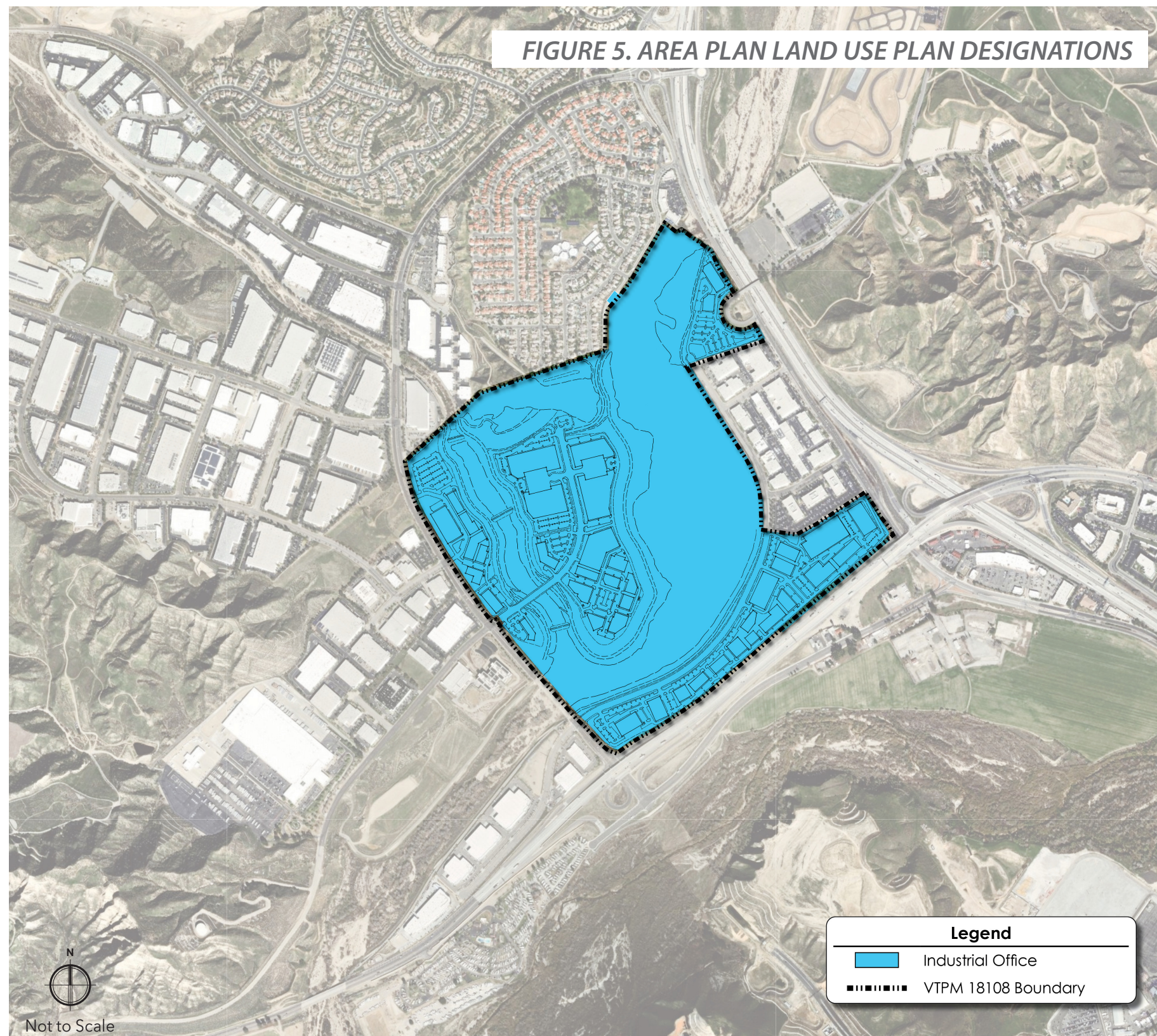
Valencia Commerce Center

SUMMARY OF VCC ENTITLEMENTS AND DEVELOPMENT

VCC Development History	Million Square Feet
County's prior approval of VCC (with authority from the 1991 CUP)	12.6
Existing, previously constructed development within VCC	7.7
Modified Project development proposed within VCC (consistent with the 2017 Approved Project as analyzed in the State-certified EIR) and within the Vesting Tentative Parcel Map (VTPM) boundary	3.4
Remaining square footage authorized under the approved CUP ^a	1.5

^aAdditional portions of Valencia Commerce Center remain undeveloped and, while not part of the Modified Project, may be built out in the future in conformance with the existing County-approved entitlements. *Source: FivePoint, 2024.*





1.4 Santa Clarita Valley Area Plan (One Valley, One Vision)

The Modified Project area has been identified as an area slated for future growth by the 2012 Santa Clarita Area Plan “One Valley, One Vision” (OVOV), the comprehensive long range planning document adopted by the Los Angeles County Board of Supervisors. The plan for VCC meets the dual goals identified within the Area Plan of creating a unique, readily identifiable community while simultaneously providing a coordinated approach to orderly development and creation of new jobs.

The Area Plan has identified the site as the land use designation Industrial Office (BP-Business Park) which allows for mixed employment districts in areas accessible to transportation and visible from freeways and major arterials and is intended to promote the development of master-planned environments with a high quality of design and construction; see Figure 5, Area Plan Land Use Plan Designations. A maximum Floor Area Ratio (FAR) of 2.0 is allowed, and specific uses will be determined by the underlying zoning designation. Additional uses include:

1. Offices;
2. Medical services;
3. Research and development;
4. Light assembly and fabrication;
5. Warehousing and distribution; and
6. Supportive commercial uses.

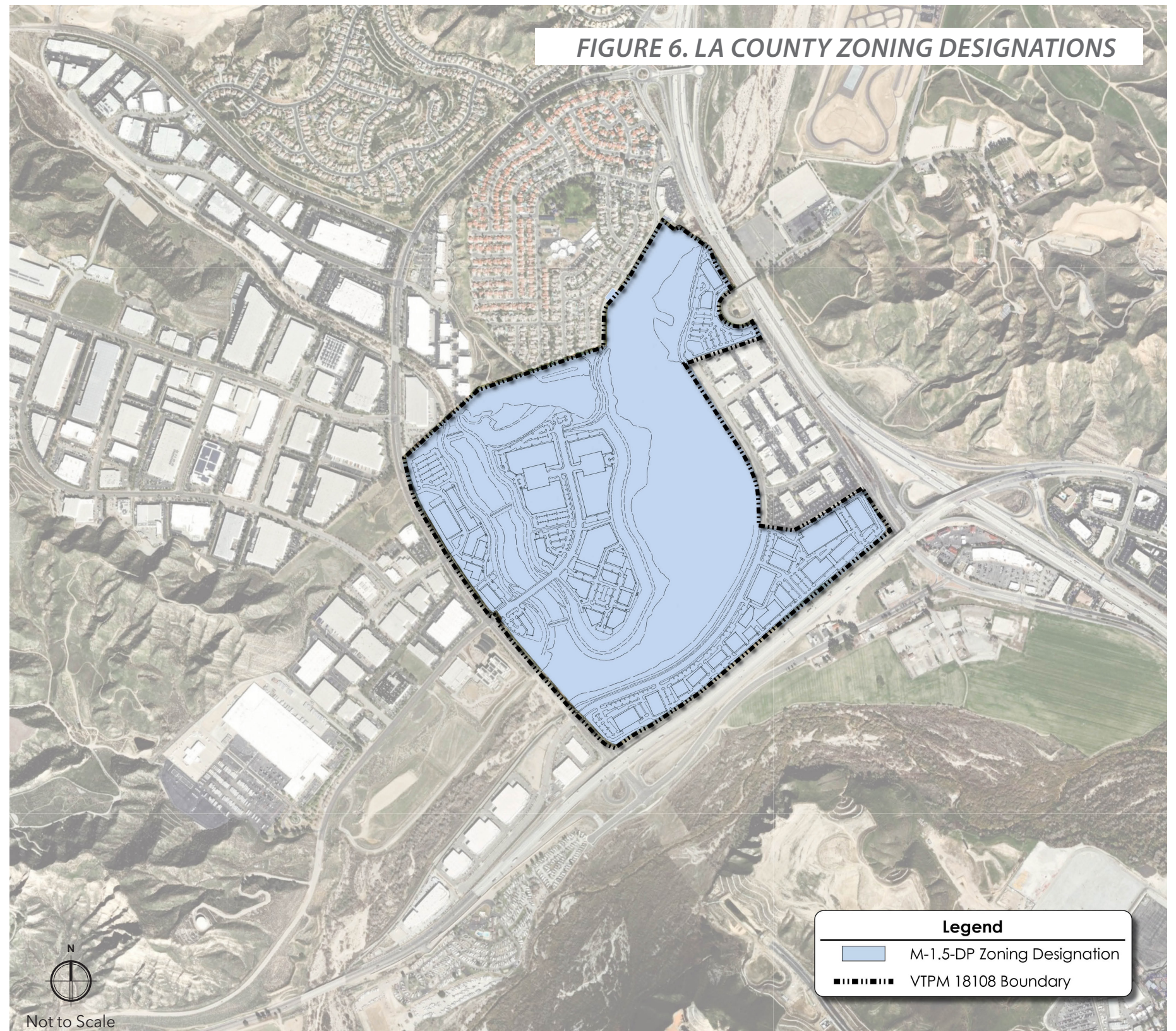
The Modified Project area of VCC implements the vision of the Santa Clarita Valley Area Plan by incorporating land uses and environmental development consistent with the Area Plan goals and by promoting development concurrent with the provision of adequate infrastructure, economic vitality, and an improved quality of life.

1.5 Zoning

The VCC Modified Project area is zoned for M-1.5-DP—Restricted Heavy Manufacturing Industrial planned development. A maximum Floor Area Ratio (FAR) of 1.0 is allowed in this zone.

It would be consistent with the State-certified EIR and would be developed with the uses allowed by the VCC entitlements and existing zoning. The Modified Project does not involve any new or additional buildout of the VCC Planning Area beyond what was analyzed in the State-certified EIR, and the Modified Project would not result in any buildout of VCC beyond the existing VCC entitlements.

The Los Angeles County Planning and Zoning Code, also known as the Zoning Ordinance, designates land use and development standards for unincorporated areas within the County, as shown in Figure 6, LA County Zoning Designations.



Chapter Two

Development Plan



2.1 Proposed Land Use Plan

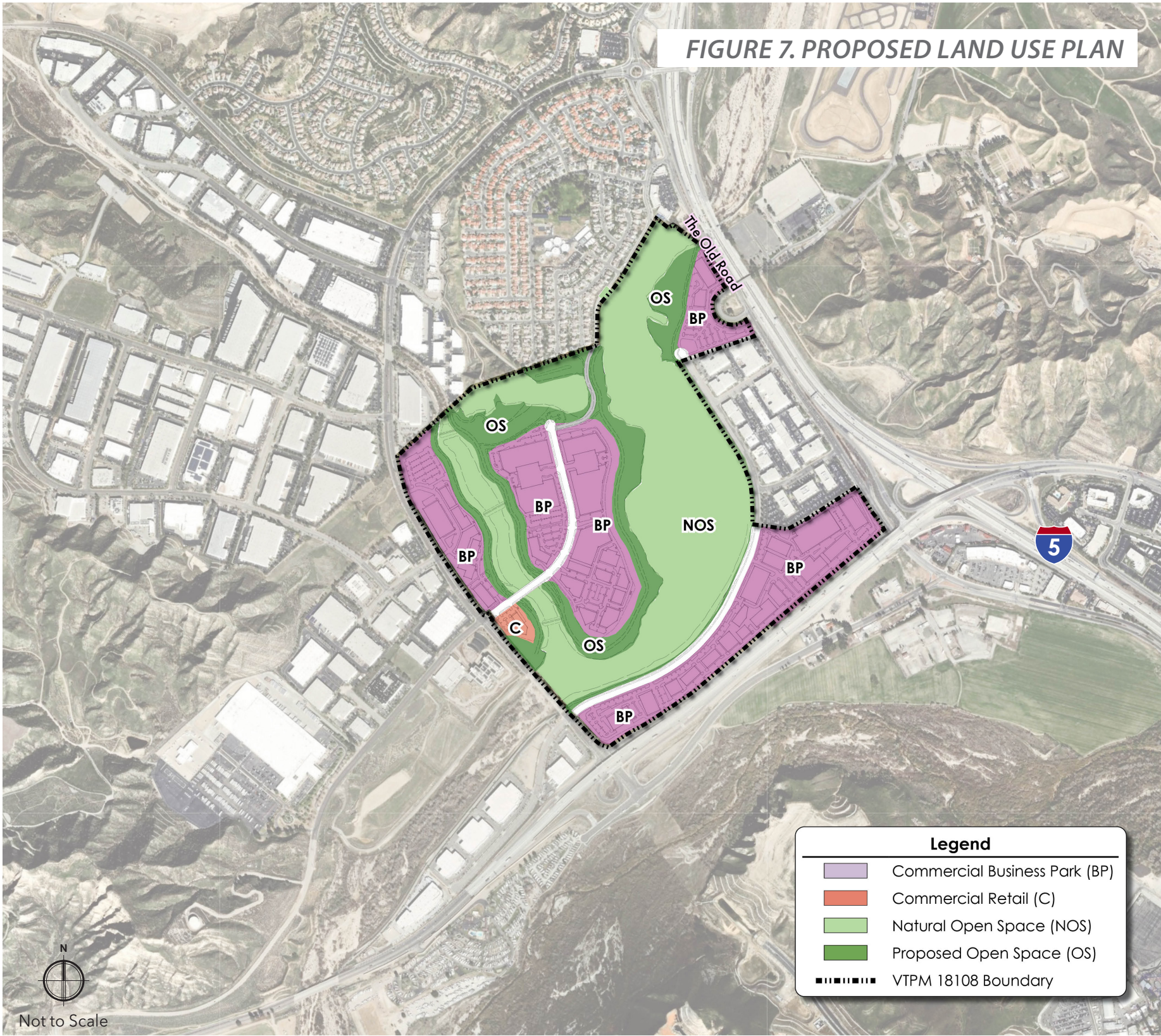
The existing VCC entitlement previously approved by the County allows approximately 12.6 million square feet of industrial/business park space at buildout. The Modified Project includes approximately 3.4 million square feet of industrial/business park space within the County-approved and partially completed VCC industrial/business park center. The Land Use Plan proposes to incorporate 132.6 acres of Commercial Business Park with approximately 195.9 acres of Open Space.

Development proposed within the Vesting Tentative Parcel Map (VTPM) No. 18108 is illustrated in Figure 7, Proposed Land Use Plan. As shown, the majority of the land areas would be developed with industrial/business park uses, with open space provided along the banks of Hasley and Castaic Creeks.

The proposed land uses are consistent with the provisions in the 1991 Conditional Use Permit, the Santa Clarita Valley Area Plan, and existing LA County Zoning Designation. The 1991 CUP allows for a maximum FAR of 0.6, while the Area Plan has a maximum FAR of 2.0 and the M-1.5 DP- Restricted Heavy Manufacturing Industrial Zone allows a maximum FAR of 1.0. The proposed VCC development would also comply with the Development Program overlay, including design provisions associated with CUP 87-360, as applicable.

LAND USE SUMMARY

Land Use	Acreage	Target Density/FAR	Proposed SqFt.
Commercial Business Park	132.6	2.0	3.4 mil.
Open Space	195.9	--	--
Total	328.5		



EXAMPLES OF COMMERCIAL BUSINESS PARK

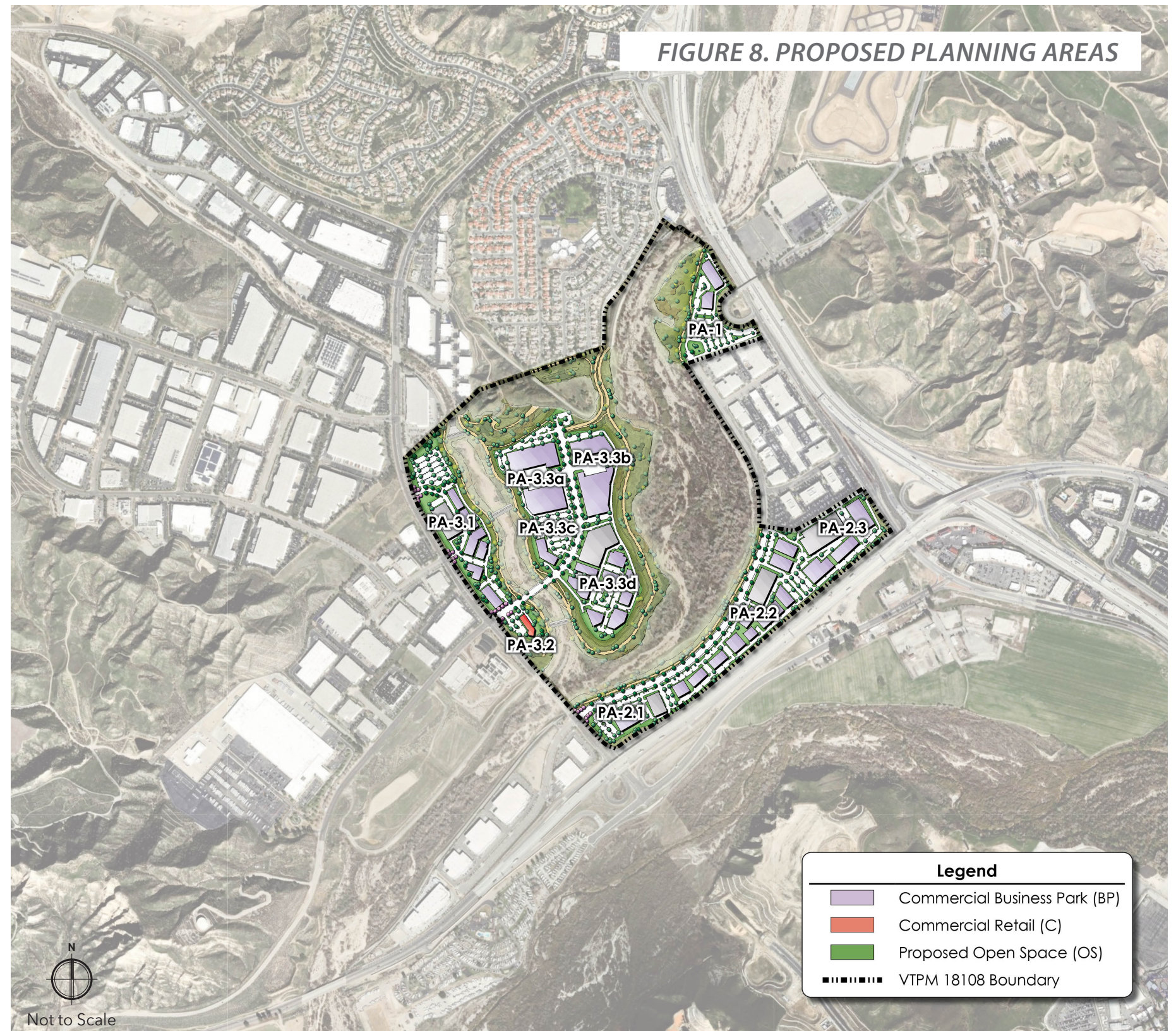


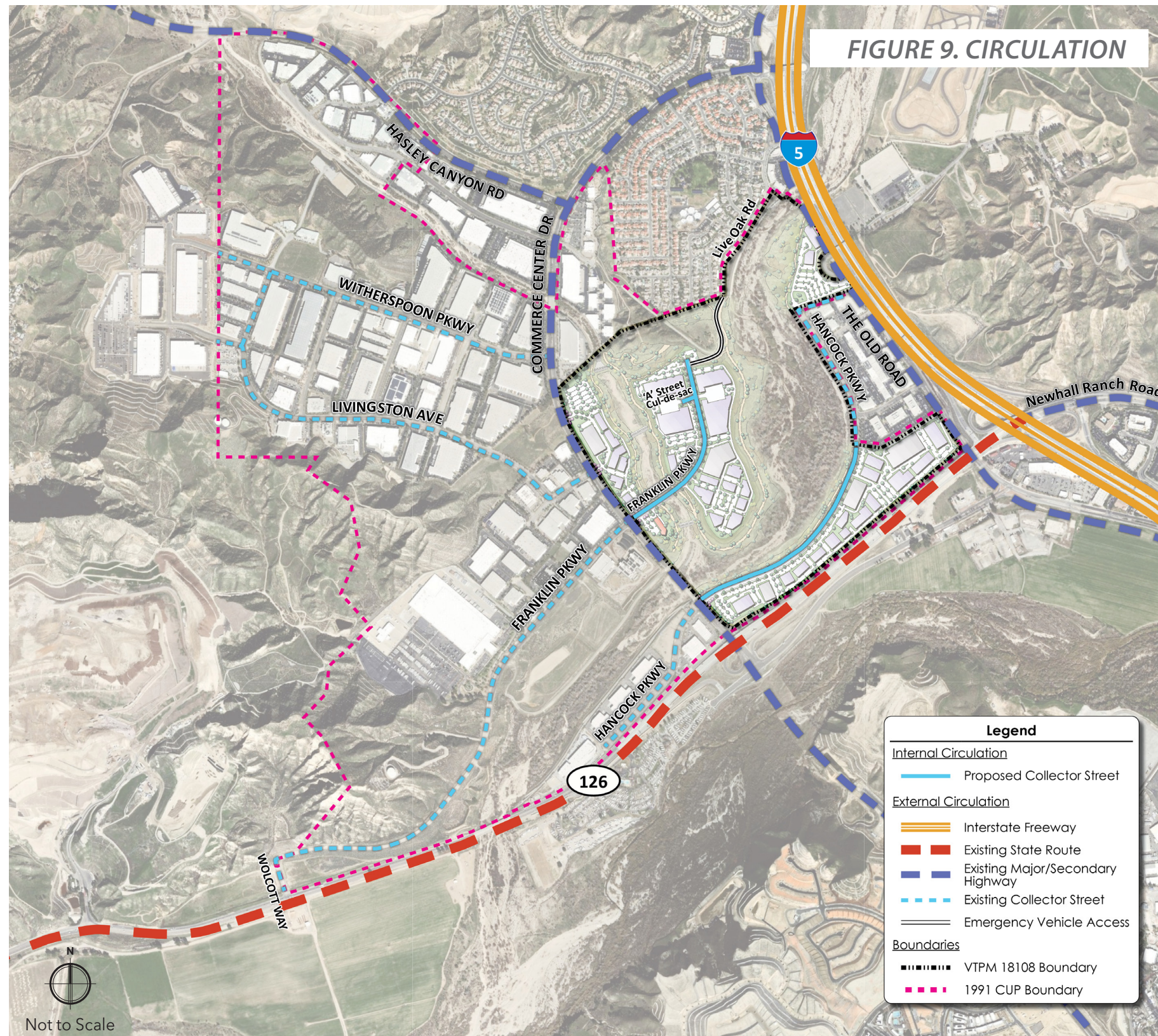
2.1.1 COMMERCIAL INDUSTRIAL/BUSINESS PARK LAND USE

The Project includes approximately 3.4 million square feet of industrial/business park. In accordance with County Code Section 22.22.010, the Restricted Heavy Manufacturing Zone (Zone M-1.5) allows for light and restricted heavy industrial uses, including the manufacture, assembly, distribution, and storage of goods with low to medium nuisance impacts, but excluding raw-materials production, processing, or bulk handling.

2.2 Proposed Planning Areas

The Modified Project area in VCC will be organized into planning areas (PAs), most of which would include office buildings and associated surface and structured parking. Within the northern central portion (PA-3.3a and PA-3.3b), industrial buildings would be developed surrounded by surface parking. In addition, retail uses and surface parking would be provided in the central area of VCC along Commerce Center Drive (PA-3.2). In order to provide development flexibility over the course of buildout, the density of development and location of buildings within individual PAs may be adjusted so long as the overall average density in VTPM 18108 is not increased. A total of 195.9 acres of open space would be provided, of which approximately 157 acres would be natural areas within Hasley and Castaic Creeks.





2.3 Mobility and Street Sections

The proposed circulation system for VCC enhances access to adjacent communities and the regional transportation system. The interior roadway system includes a clear hierarchy of streets providing connectivity within each neighborhood while also supporting a multi-modal network and connection to public bus stops.

Hancock Parkway and Franklin Parkway would be extended from Commerce Center Drive east into the Planning Area. The Hancock Parkway extension would connect to an existing segment of Hancock Parkway, extending to the northeastern-most portion of the VCC Planning Area. In addition, emergency access would be provided from the terminus of Franklin Parkway to Live Oak Road to the northeast. Parking would be provided in a combination of surface lots and parking structures throughout the office, industrial, and retail areas. As discussed below, a parking permit would be required to authorize shared and reciprocal parking across lot lines.

While emphasizing safety and meeting County requirements, street widths have been designed to be as narrow as possible to reduce runoff and maximize groundwater recharge. Refer to Figures 10-11, Street Sections for proposed street and sidewalk widths.

FIGURE 10. COMMERCIAL COLLECTOR STREET SECTION

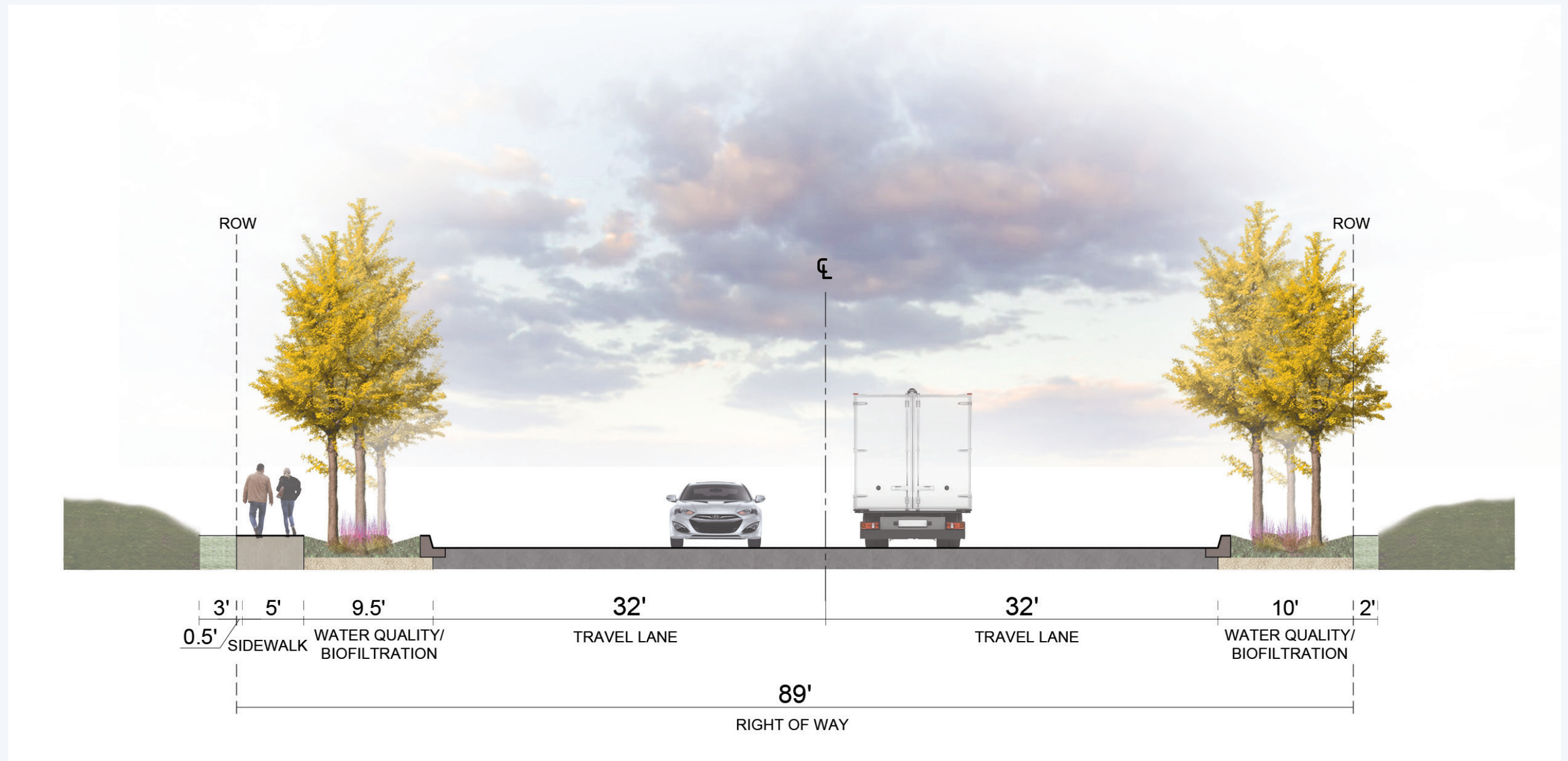
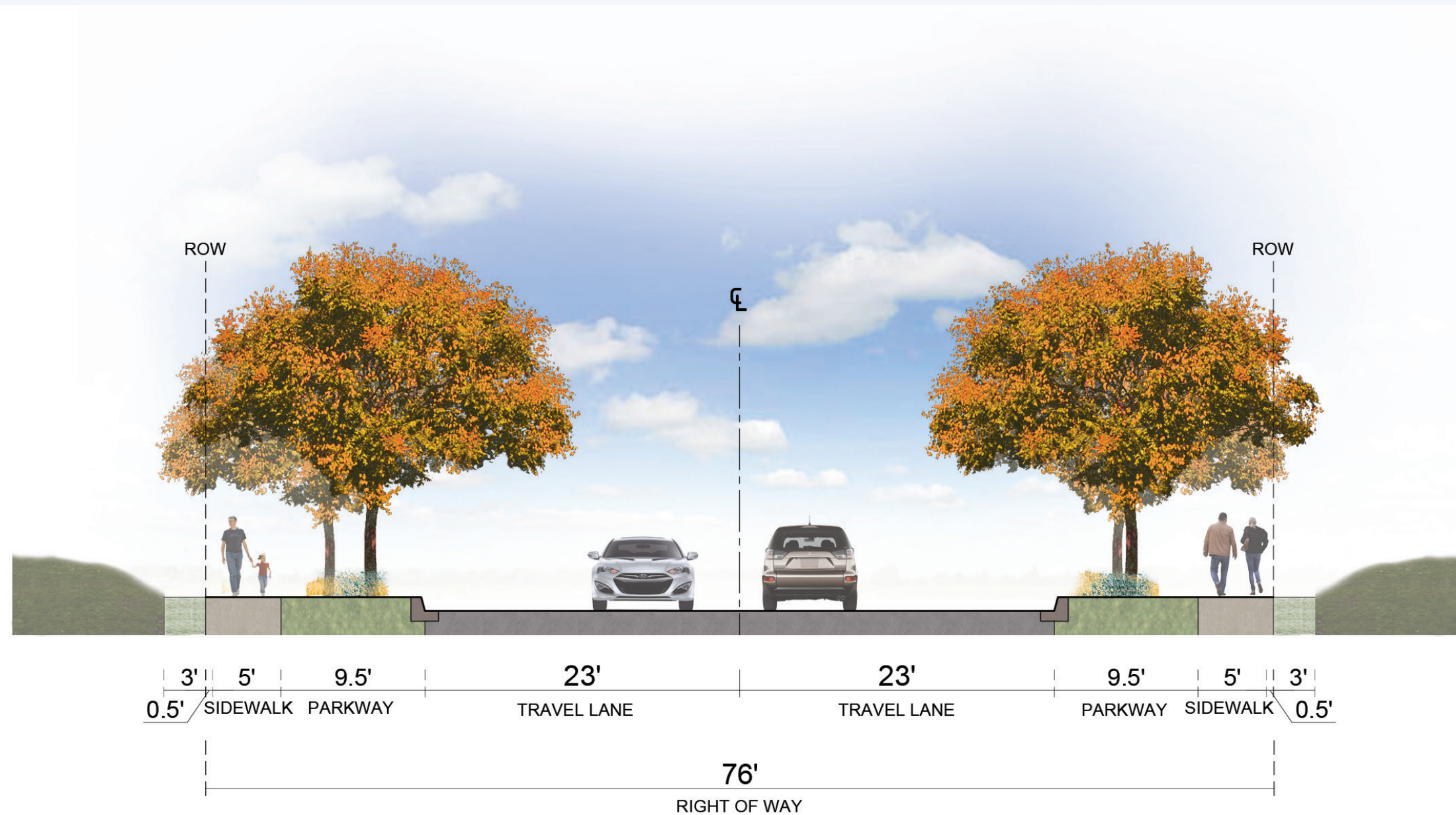


FIGURE 11. COMMERCIAL CUL-DE-SAC STREET SECTION



2.4 Streetscapes

Landscaping along public streets is designed to provide a unified appearance along street frontages, reinforce the street hierarchy, and establish identities of place, particularly at intersections within VCC.

2.5.1 COMMERCE CENTER DR

Commerce Center Drive is the most significant circulation connection within the project area, serving as the major north-south primary highway. Through a combination of strong landscape elements and clear graphic signage, Commerce Center Drive will be immediately recognizable as the most prominent street in the business park. The feeling of a grand boulevard will be created by using a formal linear pattern of street trees in the parkway and median, with project entry monumentation creating a sense of arrival. As the primary vehicular and pedestrian artery for the Center, Commerce Center Drive will have sidewalks on both sides of the street. Buildings will maintain a 65 foot setback from the curb and parking areas will be set back 40 feet and screened by landscaped berms and/or headlight walls to establish the appearance of a spacious boulevard.

2.5.2 INTERIOR STREETS

2.5.2.1 Frontage

Uniform use of different street tree types and landscaped parkways will create an attractive foreground for individual businesses. Buildings will be set back a minimum of 40 feet from the curb and cars will be screened by a headlight wall or berm. Sidewalks on one side of the street will offer employees a convenient means to reach the commercial and recreational uses in the Center. Long term maintenance of the interior landscaped streets will be maintained by the VCC POA.

2.5.2.2 Corner Conditions

On corner lots the uniform streetscene is maintained along two frontages. Buildings will continue to be set back at least 40 feet from the curb behind the landscaped parkway. However, on the street sideyard side, building and parking will be set back at least 25 feet from the curb and parking shall be landscaped, and screened from view by a headlight wall or berm.

EXAMPLES OF STREETSCAPE

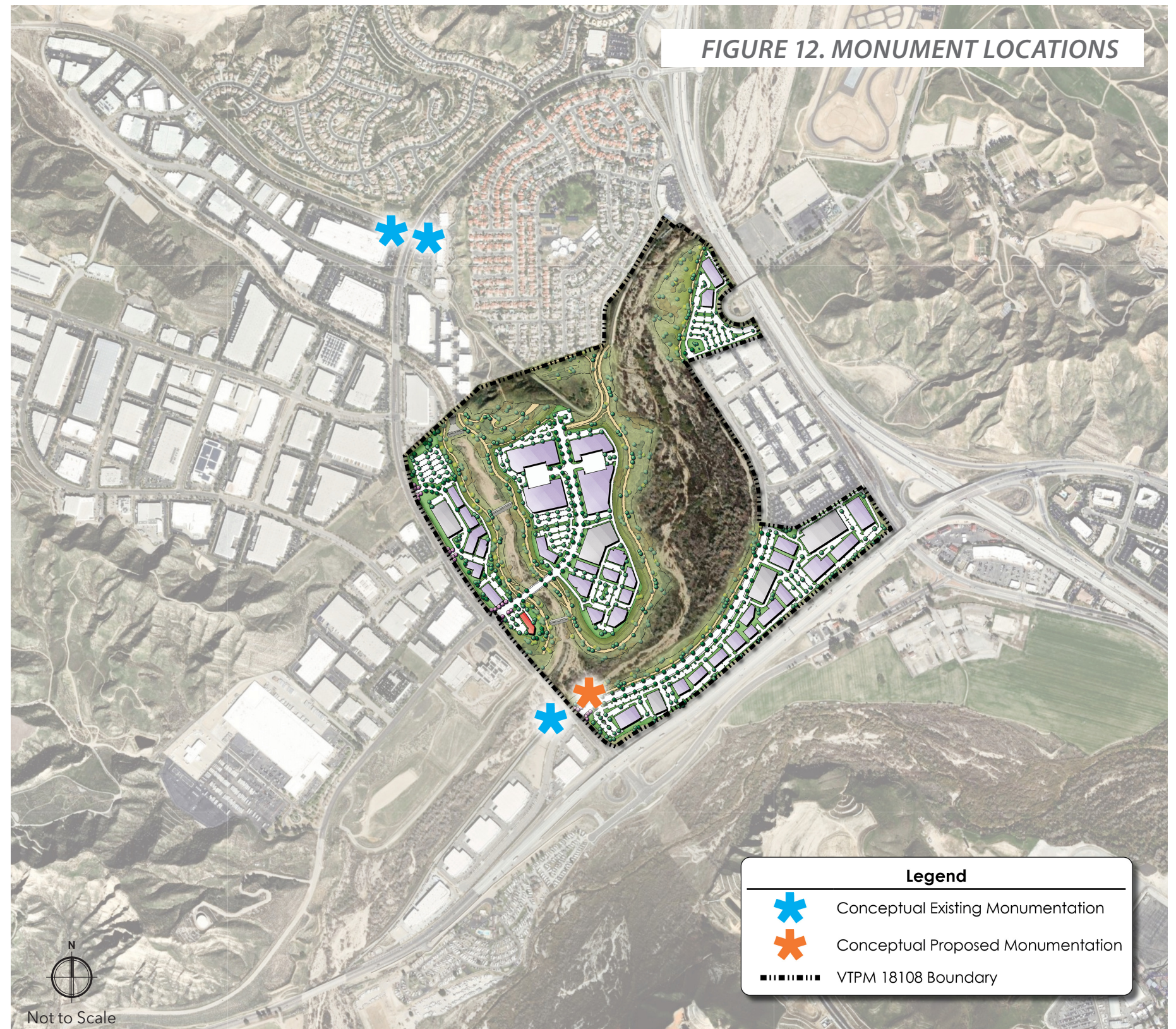


2.5 Entries, Signage and Monumentation

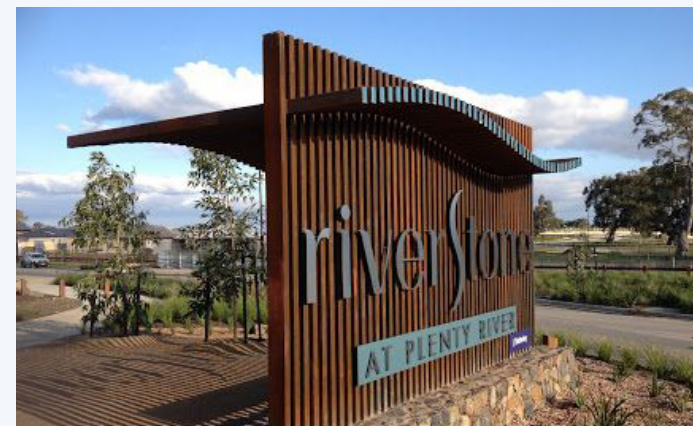
Project signage would be limited primarily to ground-level and wayfinding pedestrian/vehicular signage and building identification signage. Project signage would be in keeping with the character of other nearby residential communities and commercial districts, and any associated lighting would be kept to the minimum sufficient to provide visibility and interest without creating bright light spots or light spillover.

2.5.1 BUSINESS SIGNAGE

The design of the tenant identification monuments is intended to provide uniformity of business signage and parity among owners and tenants. Business signage must be consistent with the VCC design guidelines; and will be required to be reviewed and approved by the VCC Property Owners Association (POA) architectural review committee for conformance with the design guidelines.

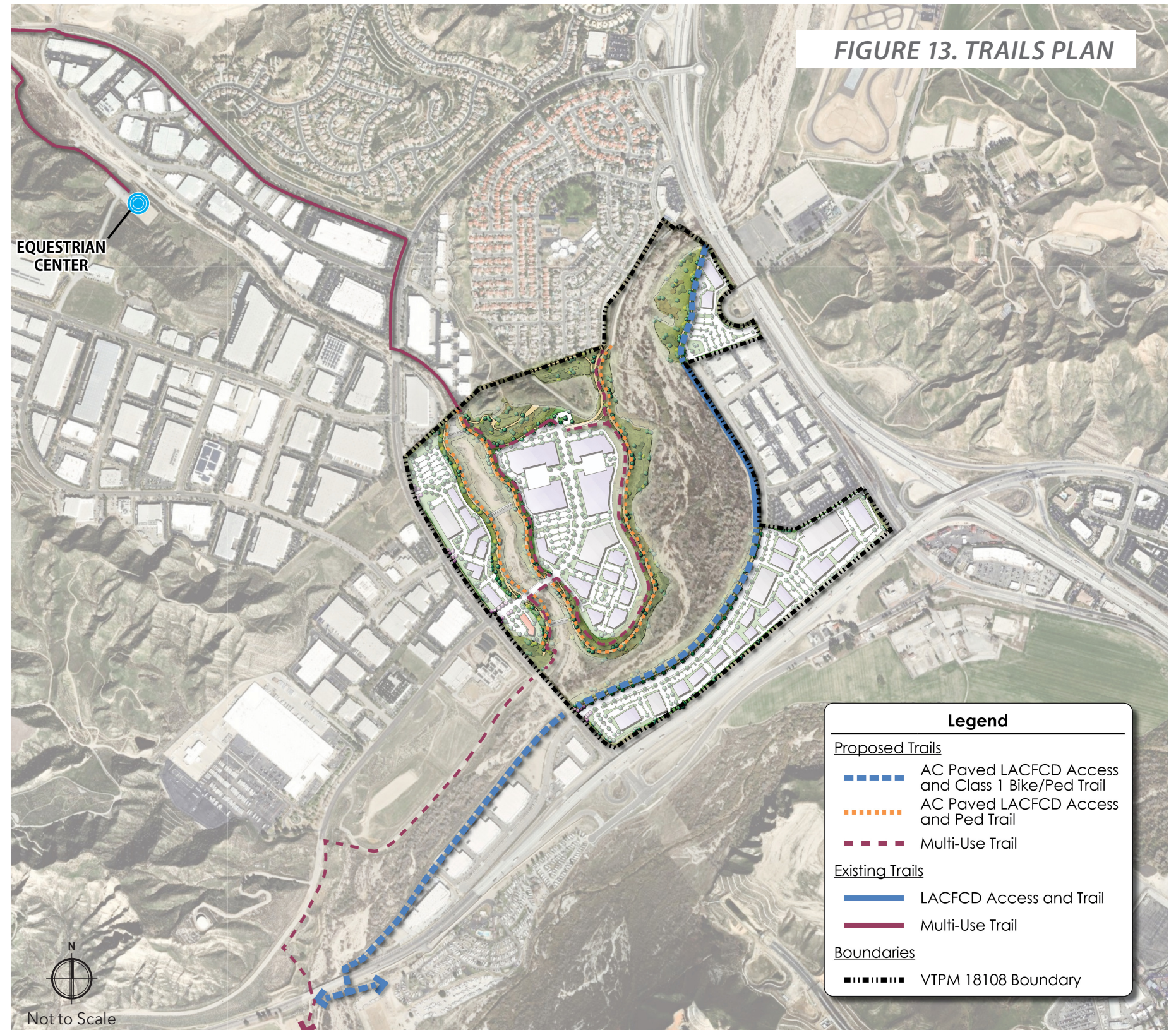


EXAMPLES OF ENTRIES, SIGNAGE AND MONUMENTATION



2.6 Pedestrian Connectivity and Trails

As illustrated in Figure 13, Trails Plan, a series of multi-use trails and paved pedestrian trails (also providing County flood control access) would be introduced along the banks of Hasley Creek and Castaic Creek. These include an extension of the existing Hasley Canyon Trail under Commerce Center Drive through the VCC Planning Area, connecting to the Live Oak residential neighborhood to the north and eventually connecting to a regional river trail planned as part of the nearby Landmark Village project. Hasley Canyon Trail is a 1.67-mile multi-purpose trail located northwest of the VCC Planning Area, which can be accessed along Commerce Center Drive and Hasley Canyon Road through the existing portion of the VCC industrial/business park, with connection to the Hasley Canyon Equestrian Center in the hills to the north. Additionally, sidewalks are proposed on the east side of Commerce Center Drive as part of the VCC improvements. Also within VCC, a Class I Bike Route/pedestrian trail (also providing County flood control access) would be developed along the south/east bank of Castaic Creek adjacent to the Hancock Parkway extension. Project trail improvements will connect to an extensive system of existing and proposed trails in the region.





2.7 Landscape Design

As part of the Modified Project, a comprehensive landscape plan would be implemented to enhance the existing natural features in the vicinity. In accordance with the County's drought-tolerant landscaping requirements (County Code Title 31), at least 75 percent of the Modified Project's landscaped area would contain plants from the Los Angeles County Drought-Tolerant Plant List. In addition, native trees, including oak trees, would be planted and enhanced with complementary native vegetation. Landscaping would be integrated into the design of structures and would include drought-tolerant plants and shrubs, street trees, and landscaped parkways.

Landscaping should preserve and enhance views, soften edges of buildings, and provide shade, screening, and buffering. It may not be used as a mask to justify poor building design.

2.7.1 STREETSCAPES

Site grading shall be designed to be compatible with streetscape grades and to minimize the need for handrails or pedestrian ramps within the site.

2.7.2 ENTRIES/KEY INTERSECTIONS

Entries and key intersections should be clearly defined, inviting and consistent with surrounding communities. Entries should have articulation and color for identity interest and integration with overall building forms.

2.8 Design Principles

VCC South relies on many of the same design concepts used within other Valencia projects such as preserving open space and natural features through clustering of development, grading in terraces to preserve existing landforms, increasing residential density within a limited distance from village centers, and providing pedestrian-friendly streets. Construction of the community will be consistent with the County of Los Angeles' Low Impact Development Standards Manual, which includes a discussion of many of these concepts as integral components of sustainable development.

The project design is governed by the VCC Design Guidelines (2018), which is used to derive the following design guidelines outlined in this section. The VCC business park is governed by CC&Rs that are managed by the Property Owners Association and Architectural Review Committee. Photos accompanying the design principles are conceptual in nature and are meant to generally illustrate application to the community.

BUSINESS PARK DESIGN PRINCIPLES



Architectural styles shall provide variety and a high degree of unification within business areas. Eco-friendly and sustainable building features will be incorporated in all commercial area to achieve a vibrant community that is both socially and environmentally sustainable.



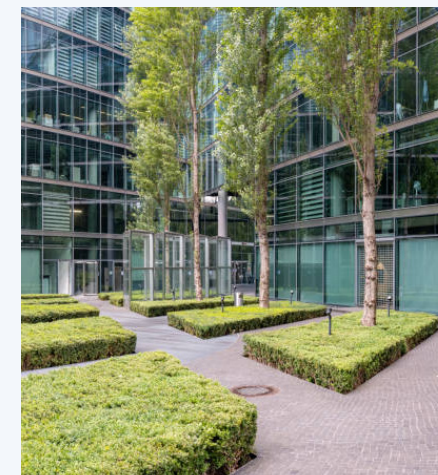
Convenient and safe pedestrian and bicycle connections are to be provided throughout commercial areas, while potential conflicts with vehicles are to be avoided.



Zero Net Energy buildings will produce as much energy as they use over the course of a year.



Shady, tree lined streetscapes provide a pleasant pedestrian experience.



Public spaces such as plazas and courtyards present an opportunity to incorporate placemaking elements, bringing a unique ambiance.



Landscaping is to be used appropriately to provide shade and reduce the visual impact of parking areas, loading docks, and trash areas.



2.9 Site Design

2.9.1 OPEN SPACE PRESERVATION

An outstanding amenity of the VCC area is the interconnected trail system which provides access and linkage to the regional trail system across the business park site. The trail system sits alongside the preserved open space. Enhanced environmental protections are implemented through increased avoidance/protection of wetlands and natural habitat along the banks of Hasley Creek and Castaic Creek.

As part of the VCC development, buried bank protection would be installed along the banks of both Hasley and Castaic Creeks to protect against flooding and erosion. The existing natural channel bed of Hasley Creek would be maintained and generally follow the existing sinuous creek alignment. The series of grade control structures included in the 2017 Approved Project's design would be eliminated, although two grade control structures would be installed to maintain the stability of the natural streambed. In addition, more of the Castaic Creek floodplain would be retained as compared to the 2017 Approved Project since the planned bank protection alignments on the east and west banks would be pulled back from the creek bed. These represent environmentally beneficial changes to the natural open space of the project.

2.9.2 BUILDING ARTICULATION

Articulation refers to the arrangement of buildings and parking areas, the size and location of pedestrian spaces, and how these features relate to one another.

1. Buildings are to be located on each site in a manner that is efficient, appropriate to site conditions, supportive of the overall architectural composition and compatible with nearby projects throughout the VCC.
2. Buildings shall be located to enhance project visibility and identity, while maintaining compatible relationships with adjacent projects and street views.
3. Buildings shall be oriented so that loading and service areas are screened from view from streets and public areas.
4. Buildings shall be arranged to provide convenient access to entrances and efficient on-site circulation for vehicles and pedestrians.
5. Buildings shall be arranged to provide landscape outdoor plazas or entries.
6. Visitor parking shall be convenient to public building entries, as shown below.
7. Indoor and outdoor break areas shall be provided convenient to major office areas.

2.9.3 GREEN BUILDING STANDARDS

The Modified Project would comply with the County's Green Building Standards Code (Title 31), which addresses sustainability via appropriate planning and design, water and energy efficiency and conservation, waste diversion, and tree planting requirements. In order to minimize water usage, Project landscaping would include drought-tolerant plants in compliance with the County's drought-tolerant landscaping requirements (Title 31). Finally, stormwater handling and treatment would be implemented to protect streams, groundwater, surface water quality, and natural drainage characteristics in compliance with the Low Impact Development Standards (County Code Chapter 12.84).

EXAMPLES OF GREEN BUILDING STANDARDS



EXAMPLES OF PARKING LOT LANDSCAPE



2.9.4 ACCESS

Vehicular access to individual sites is limited to minimize disruption of traffic flow. All access to public streets is subject to approval by Los Angeles County. On-site vehicular circulation should be clear and direct.

2.9.5 PARKING LOT LANDSCAPE

Designated spaces will be provided in convenient locations for disabled, carpool, alternate fuel vehicles, motorcycles and bicycles as required by the State of California and Los Angeles County.

Parking area landscaping is required for the screening of large parking areas to limit their visual impact. Landscaped islands shall be provided at the ends of stall rows to break up parking areas. Creation of large planting islands (tree groves) is encouraged as opposed to small pockets of individual trees (VCC Design Guidelines Section 4H. Parking Lot Landscape).

2.9.6 ARCHITECTURAL DESIGN AND FAÇADE

Buildings should incorporate interconnection and lapping of forms and heights to relieve monotony and break long expanses of blank walls. Exterior forms should be designed to reflect scale and minimize visual impact on surrounding uses. All building sides should be treated equally with consistent architectural concept. Buildings should feature simple designs with clearly defined openings and entrances. Any accessory buildings and enclosures, whether attached to the main building or not, shall be of similar design and materials. (VCC Design Guidelines Section 5B. Mass, Form, and Scale).

2.9.7 ROOF FORM

1. Visible vertical support
2. Horizontal planes and parapets
3. Varied but proportional parapet height
4. Roofing materials hidden from off-site view

2.9.8 MATERIALS AND COLORS

Building materials and colors should complement the upscale corporate image of the Valencia Commerce Center. Building materials are to be durable, aesthetically pleasing and relatively maintenance-free. The following materials are recommended for the walls of the buildings, but are not limited to:

1. Concrete tilt up. Textured forms or sandblasting should be utilized for tilt-up concrete panels. Smooth concrete uses should be either integral color or painted.
2. Masonry, such as brick or split face concrete block in limited accent areas. Precision block is not permitted as a finish material, except as a limited accent.
3. Tinted and colored glass and bronze or black anodized window frames. Reflective glass shall be minimized.
4. Roofs. Concrete tile, clay tile (in commercial areas only) and colored or pre-finished standing seam metal roofs. Flat roof designs require crushed rock or similar uniform coverage treatment when the roof is visible from adjacent properties.
5. Colors. While neutral and grey colors are acceptable, soft pastels and earth colors with selected accent colors are strongly recommended.

Unacceptable materials include corrugated metal siding, precision concrete block and common plywood sheathing. Buildings of mobile/modular construction are prohibited (*VCC Design Guidelines Section 5C. Materials and Colors*).

2.9.9 SIDEWALKS AS LINEAR OPEN SPACE

1. Street trees in parkways would provide sidewalks with shade and beautification along the street corridors.
2. Interior paths and sidewalks shall provide direct access to building entrances and to public right-of-way.

EXAMPLES OF SIDEWALKS



EXAMPLES OF LIGHTING



2.10 Lighting

Project lighting shall be designed to ensure visibility and safety while minimizing light spillover and skyglow. As part of the Modified Project's security features, roadways, entryways, and parking areas shall be well illuminated and designed to eliminate areas of concealment. Measures such as light control devices on fixtures and careful fixture placement would be implemented to ensure minimal light spillover onto native habitat areas. Fixtures may include post lights, building-mounted fixtures, and landscape lighting, all of which would be carefully placed and directed to reduce glare and maximize comfort, security, and visibility.

2.10.1 VEHICULAR

Vehicular fixtures may include post lights, building-mounted fixtures, and landscape lighting, all of which shall be carefully placed and directed to reduce glare and maximize comfort, security, and visibility. All exterior on-site lighting shall be shielded and confined within site boundaries. No direct rays or glare are to shine onto public streets or adjacent lots. To reinforce identity and unity, all exterior lighting shall be consistent in height, spacing, color and type of fixture throughout the building site and compatible throughout VCC and the surrounding communities.

2.10.2 PEDESTRIAN

Pedestrian walkways and building entries shall be illuminated to provide for pedestrian orientation and to clearly identify a secure route between parking areas and points of entry to the building.

Courtyards, arcades and seating areas shall be illuminated to promote pedestrian use and safety. A variety of lighting may be used to create interest and special effects in coordination with the character and function of the area.

2.11 Net Zero GHG Community

In 2016, Newhall Land proposed to make Newhall Ranch the first community in the United States of its scale to achieve net zero emissions of greenhouse gases from both construction and operation. Subsequently, the State of California (specifically, the California Department of Fish and Wildlife) completed a comprehensive Environmental Impact Report and adopted 13 mitigation measures that apply to Valencia Commerce Center (as well as the Newhall Ranch Specific Plan, Entrada South Planning Area, and other nearby Newhall villages) to reduce the community's greenhouse gas emissions to net zero.

The County of Los Angeles imposed the same 13 mitigation measures to reduce greenhouse gas emissions to net zero at Newhall Ranch in connection with its re-approval of the Landmark Village and Mission Village communities within the Newhall Ranch Specific Plan area.

Designed in collaboration with the California Air Resources Board, the State expert agency on climate change, Net Zero GHG Community represents a new standard of environmental sustainability and a lasting investment in the future. The program involves implementation of the measures, including the following:

1. Greenhouse Gas Reduction within Valencia Commerce Center:

- » Commercial business park, industrial office buildings and public facilities will be designed to achieve zero net energy, and thereby be designed to integrate enhanced energy-efficient design and renewable energy generation technology.
- » The Valencia Transportation Demand Management Plan, managed by a TMO, will be implemented to reduce vehicle miles traveled and associated tailpipe emissions. Components of the Transportation Demand Management Plan include, but are not limited to, car-share and bike-share programs; transit fare subsidies; and, tech-enabled mobility platforms.
- » Electric vehicle charging stations will be installed in on-site commercial and development areas.





2. Greenhouse Gas Reduction within Los Angeles County:

- » In partnership with Climate Resolve, a leading nonprofit group dedicated to combating climate change, a building retrofit program will be implemented to improve the energy efficiency of existing buildings in disadvantaged communities within the County.
- » Electric vehicle charging stations will be installed in publicly accessible locations county-wide and within the SCAG region.

3. Greenhouse Gas Reduction outside of Los Angeles County

- » The Newhall Ranch GHG Reduction Plan will be implemented to fully offset all remaining greenhouse gas emissions to net zero through activities that directly reduce/sequester emissions. Potential examples of such activities include forest preservation for carbon sequestration in California and elsewhere.
- » A methane capture program to reduce emissions at California dairy farms and elsewhere.

2.12 Site Specific Sustainability

In addition to implementing the net zero greenhouse gas emissions program, the project will incorporate many other environmentally sustainable practices, including:

1. Locate Development Near Existing Infrastructure.

The location within a planned urbanized area directly adjacent to regional transportation and transit areas avoids construction of unnecessary infrastructure and utilization of undue natural resources.

2. Provide Industrial Offices and Open Space.

Industrial business park and open space uses are included within close proximity to each other.

3. Provide Jobs Accessible to Surrounding Villages.

Upon completion, Valencia Commerce Center will include a jobs/housing balance, providing the opportunity for residents to work close to home and minimize vehicle miles traveled.

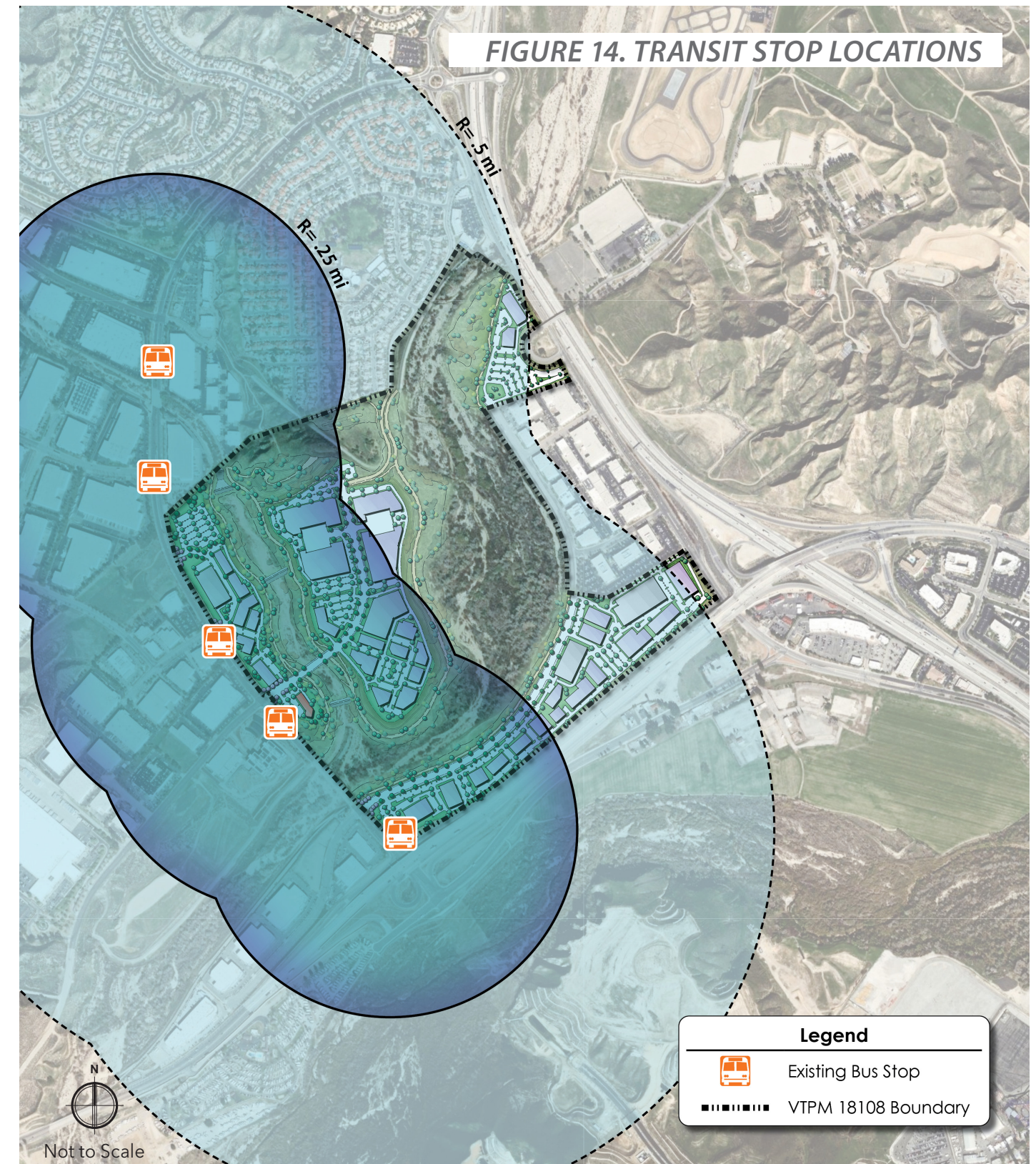
4. Create Walkable Business Park.

The extensive trail system along the banks of Hasley and Castaic Creeks brings adjacent residential neighborhoods within a convenient walking or biking distance of all site features, connecting jobs, open spaces, parks, and recreation facilities to residences.

5. Encourage Alternative Transportation Methods.

The Valencia Transportation Demand Management Plan, as discussed above, provides a comprehensive mechanism to encourage alternative modes of transportation for the community's residents, employees, students and visitors. An extensive trail system would promote walking and biking within Valencia Commerce Center, and will reduce reliance on the automobile and vehicular trips. Five existing bus stops along Commerce Center Drive, as well as a multi-purpose trail will connect the business parks to the surrounding community. Additionally, a Class I Bike Route and pedestrian trail along the south bank of Castaic Creek will provide access for pedestrians and County flood control.

FIGURE 14. TRANSIT STOP LOCATIONS





6. Promote Green Building Practices.

A comprehensive green building design program will be implemented to design commercial buildings to zero net energy buildings. The community will also comply with the Los Angeles County Green Building Standards Code which includes water efficiency and conservation, waste diversion, and tree planting requirements.

7. Preserve Sensitive Resource Areas.

Hasley and Castaic Creeks in the site will be a designated conservation area, incorporating a permanent protection and management system designed to reduce the permanent impacts to wetlands and related biological resources. This protection plan would result in an increase of open space, restored drainage areas and habitat for species.

8. Minimize Impermeable Surfaces.

Pavement area will be reduced by utilizing minimized street widths, and incorporating pervious or unpaved surfaces where possible, reducing the effects of urban runoff. Green open spaces along the internal circulation would increase soft surfaces and enhance permeability throughout the site.

9. Conserve and Re-use Water.

Recycled water from the Valencia Water Reclamation Plant will be used for irrigation of community areas when available. A minimum of 75% of the landscaped area will contain plant materials from the Los Angeles County Drought Tolerant Plant List. Native materials will be used near open space areas to complement existing vegetation.

10. Stormwater Management.

Stormwater management will conform to the current Los Angeles County Low Impact Development Standards Manual. Parking lot and roof runoff in commercial areas will be directed to landscape parkways or porous pavement areas to provide infiltration and stormwater quality treatment.

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Chapter Three

Implementation



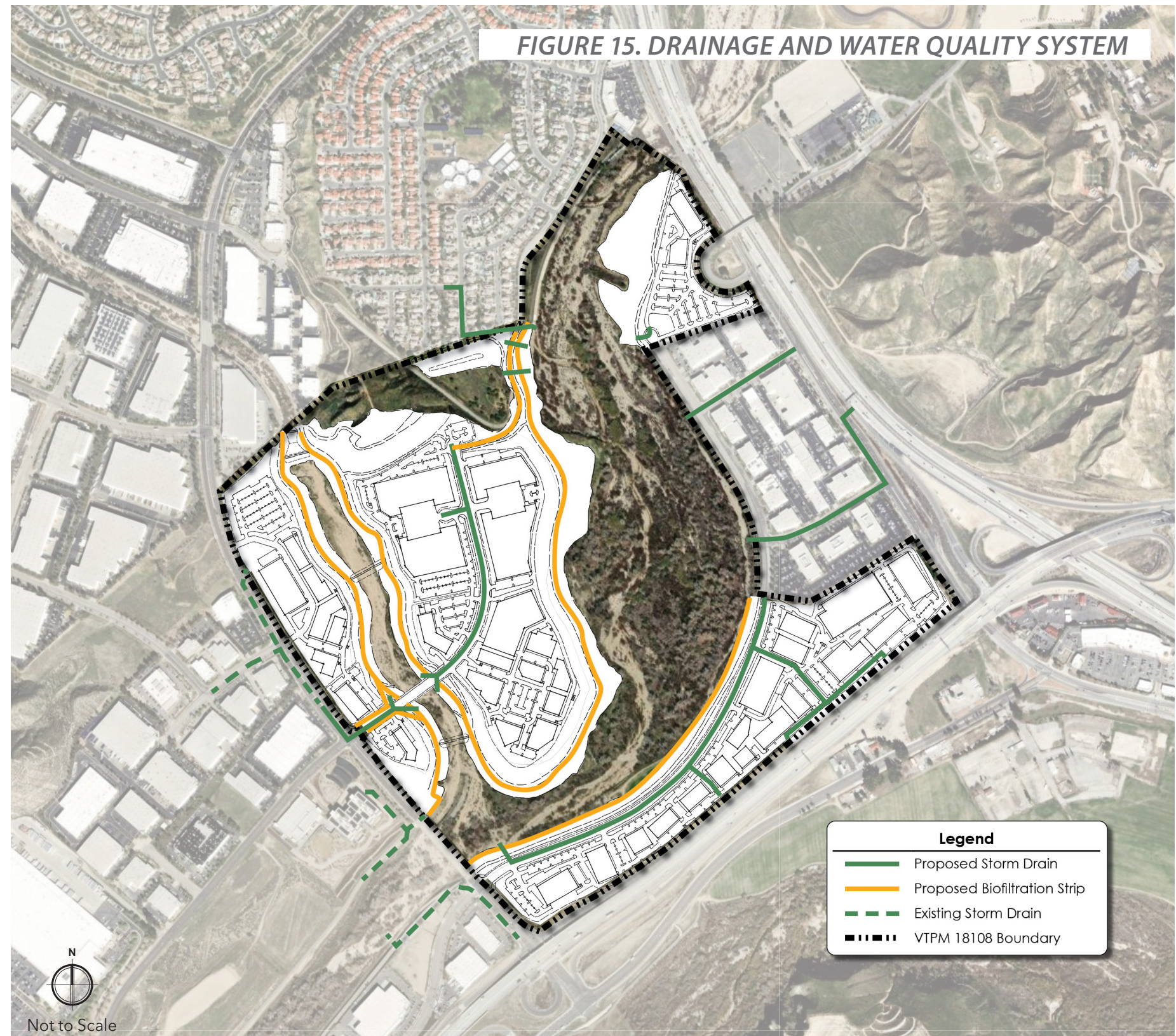
3.1 Infrastructure and Utilities

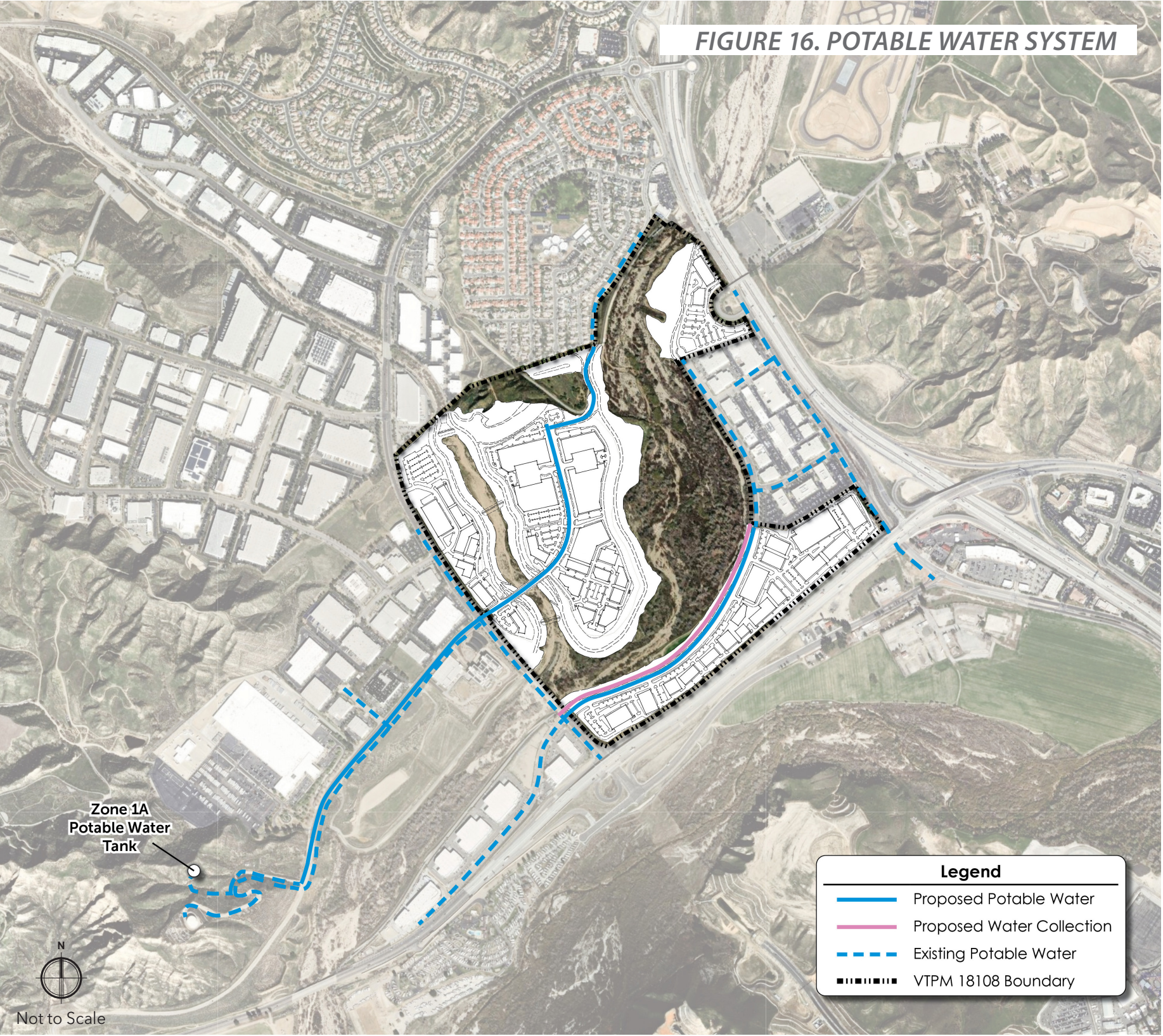
Facilities and infrastructure proposed within VTPM 18108 would include roads and trails, potable and recycled water systems, a sanitary sewer system, dry utilities systems, and drainage and water quality improvements.

3.1.1 DRAINAGE AND WATER QUALITY

A comprehensive drainage and flood protection system has been designed for VCC to maintain storm flows from the community during and after buildout.

The Modified Project would include all necessary drainage and water quality infrastructure to protect proposed development, minimize impacts to Hasley and Castaic Creeks and ensure compliance with NPDES permit requirements. A system of storm drains, biofiltration strips, and low impact development features would be installed, as illustrated in Figure 15, Drainage and Water Quality System. As shown, the storm drain system would flow to Hasley and Castaic Creeks. Water quality devices and energy dissipators at creek outlets would be installed.





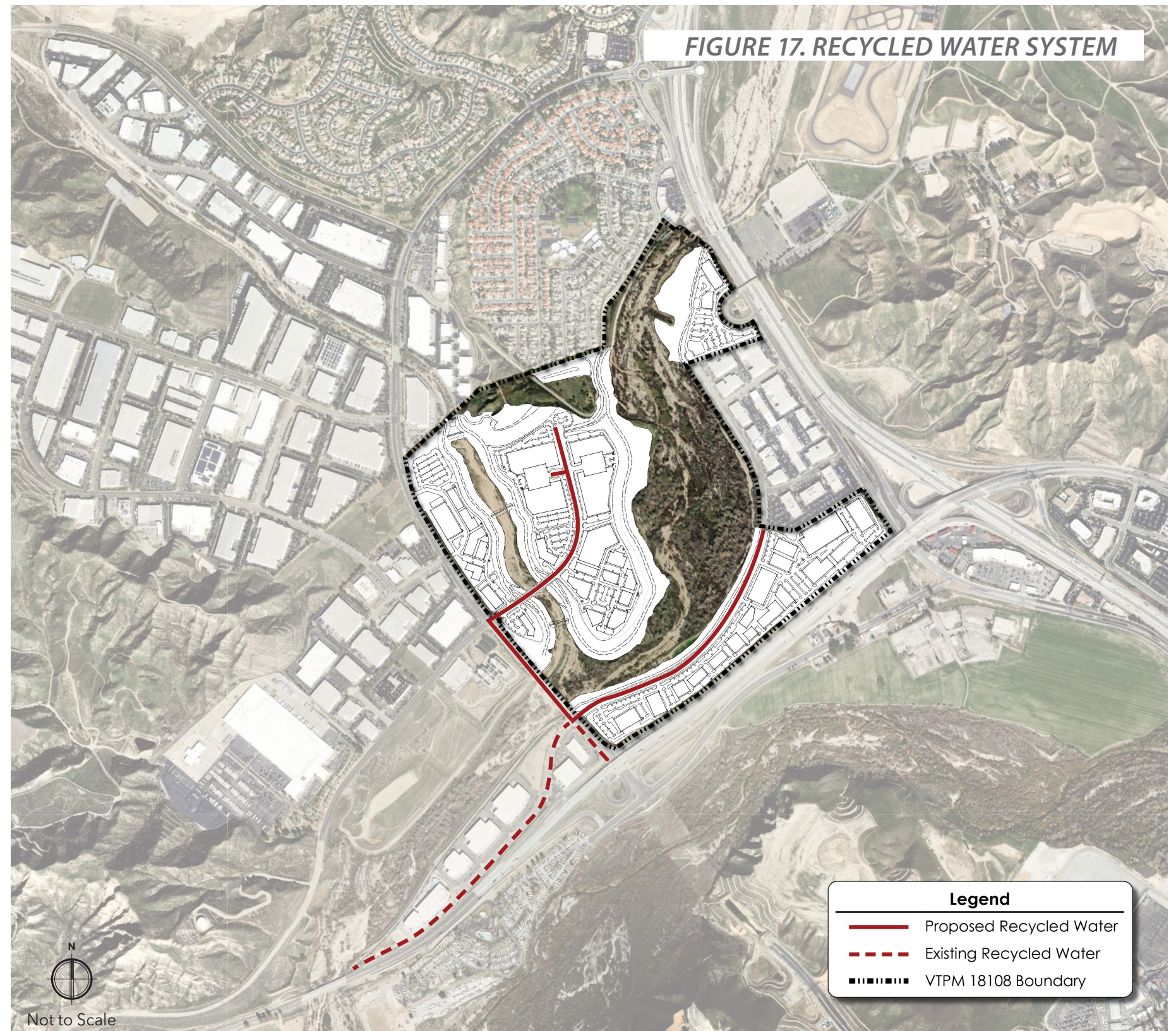
3.1.2 POTABLE WATER

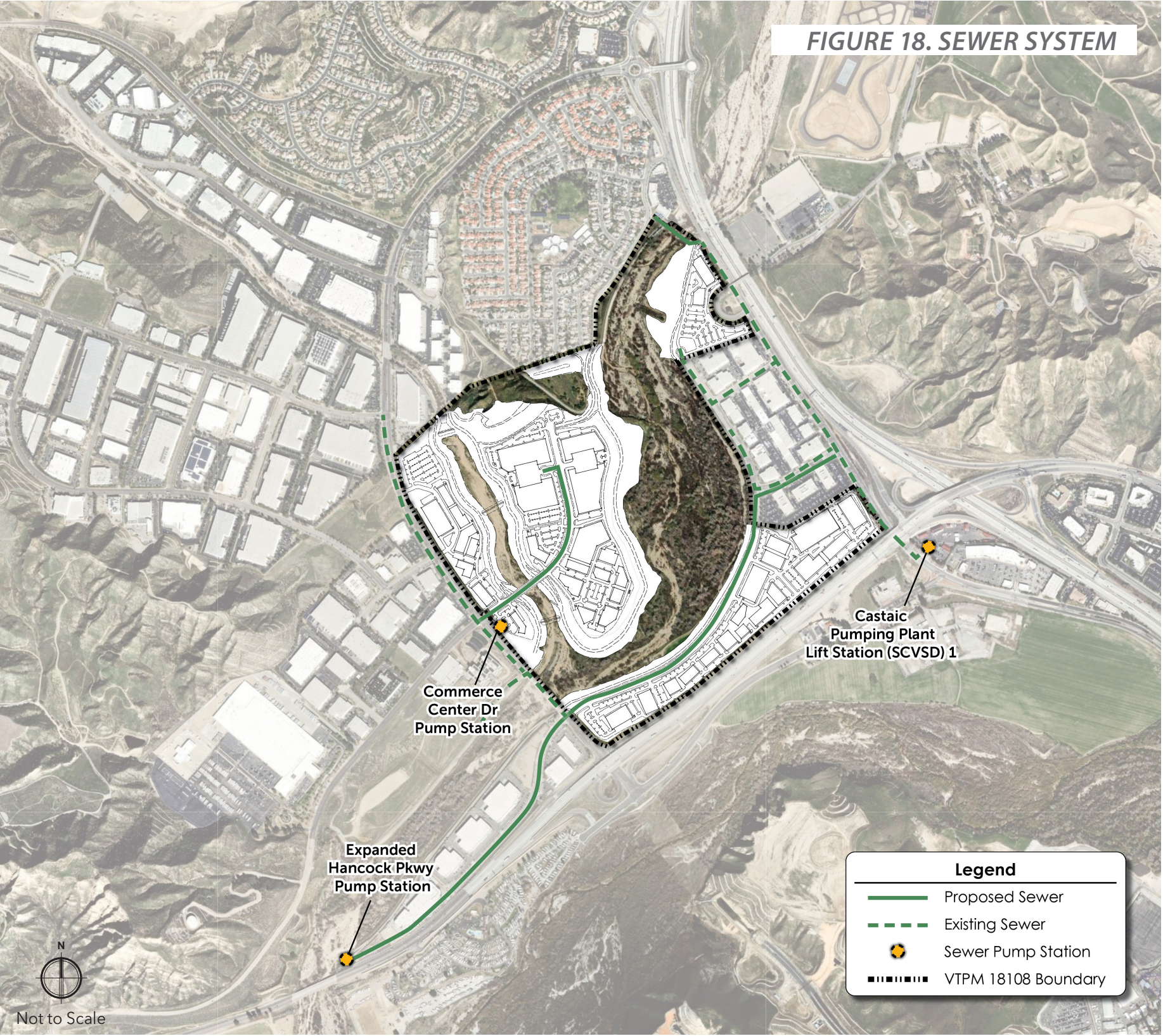
The VCC Potable Water System would include a water line to be installed along Franklin Parkway and extend off-site to the west to connect to a water tank within the developed portion of VCC (planned and approved as part of VTTM 53108 [Landmark Village]). At the northern terminus of Franklin Parkway, the proposed water line would follow the emergency access route to the northeast and connect to an existing water line in the Live Oak community. A water line would also be installed in Hancock Parkway and connect to existing supply lines in Commerce Center Drive to the west and near Turnberry Lane to the east. The northeastern-most portion of VCC (PA-1) would be served by an existing water line connecting to The Old Road.

Valencia Commerce Center

3.1.3 RECYCLED WATER

A recycled water system would be installed under Franklin Parkway, Hancock Parkway, and Commerce Center Drive, which would connect to an existing 24-inch recycled water line at the intersection of Hancock Parkway and Commerce Center Drive, as depicted in Figure 17, Recycled Water System. Both potable and recycled water would be provided by Santa Clarita Valley Water.





3.1.4 SEWER

The sewer system would consist of 8- to 12-inch sewer lines within the proposed extensions of Franklin Parkway and Hancock Parkway, connecting to existing lines in Commerce Center Drive and Hancock Parkway west of Commerce Center Drive, which flows westerly to the existing Hancock Parkway Pump Station. Phased upgrades to the Hancock Parkway Pump Station would be needed to accommodate the additional flows from VCC, and the existing Commerce Center Drive Pump Station located on-site would be abandoned. Due to the removal of the Commerce Center Drive Pump Station, the existing force main to that pump station would be re-routed to the east along the Hancock Parkway extension to connect to the 24-inch trunk main in The Old Road, which drains to the SCVSD Pumping Plant.

3.2 Development Standards

The following is a summary of the development standards that builders are required to adhere to. Specific development standards are found in the VCC Design Guidelines, CC&Rs, and LA County Municipal Code, Title 22 (Planning and Zoning), Division 3 (Zones), Chapter 22.22 (Industrial Zones) for the zone M-1.5 Restricted Heavy Manufacturing Industrial and Division 6 (Development Standards).

	Commerce Center Drive		The Old Road		Hasley Canyon Road		Lots Adjoining State Route 126		All Others	
	Building	Parking	Building	Parking	Building	Parking	Building	Parking	Building	Parking
Minimum Building/ Parking Setbacks	65'	40'	65'	40'	65'	53'	10'	10'	40'	20' and 15'
Frontyard	65'	40'	65'	40'	As required per street ^b : N/A		40'	20' and 15' ^a	40'	20' and 15'
Street Sideyard (Corner Lot)	25'	20'	25'	20'	N/A		25'	20' and 15' ^a	25'	20' and 15'
Channel Sideyard	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'
Interior Sideyard	0'	5'	0'	5'	0'	5'	0'	5'	0'	5'
Street Rearyard	As required per street ^b		As required per street ^b		65'	53'	Thru lot: 40'	Thru lot: 20'	Thru lot: as required per street ^b	
Channel Rearyard	5'	5'	5'	5'	5'	5'	10'	10'	5'	5'
Rearyard	0'	5'	0'	5'	0'	5'	Facing State Route 126: 10'		0'	5'
Building Height	-	-	-	-	35'	N/A	-	-	-	-
Floor Area Ratio (FAR)	The maximum allowable FAR shall be 0.6 (1991 Conditional Use Permit)									

^a20' setback where sidewalk exists, 15' where street has no sidewalk.
^bAs required per street: Setbacks are dictated by the requirements of the street in the rearyard.

3.3 Conditional Use Permit Conditions

VCC was originally approved for development by Los Angeles County through the issuance of various entitlements (including CUP 87-360) and certification of an EIR (SCH No. 1987-123005) in 1991.

3.4 Application and Review Process

The Los Angeles County Department of Regional Planning will oversee the site development plan review and approval process required for construction of any building within VCC. The preliminary process from plan preparation to building permit submittal is outlined in the flow chart below.

Once a preliminary design package for a development area has been prepared, the builder will first submit plans to the Architectural Review Committee of which Newhall Land is a member. The submittal package will be either approved or sent back to the builder for plan revisions. Once approved by the Architectural Review Committee, the package will be submitted to LA County for final building permit process.

3.4.1 DIAGRAM OF DEVELOPMENT REVIEW PROCESS

