



County of Los Angeles **WILLOWBROOK** TOD SPECIFIC PLAN

August 2018

Revised May 2023

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County of Los Angeles

WILLOWBROOK

TRANSIT-ORIENTED DISTRICT

SPECIFIC PLAN

August 2018
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Introduction

1.1 Specific Plan Location

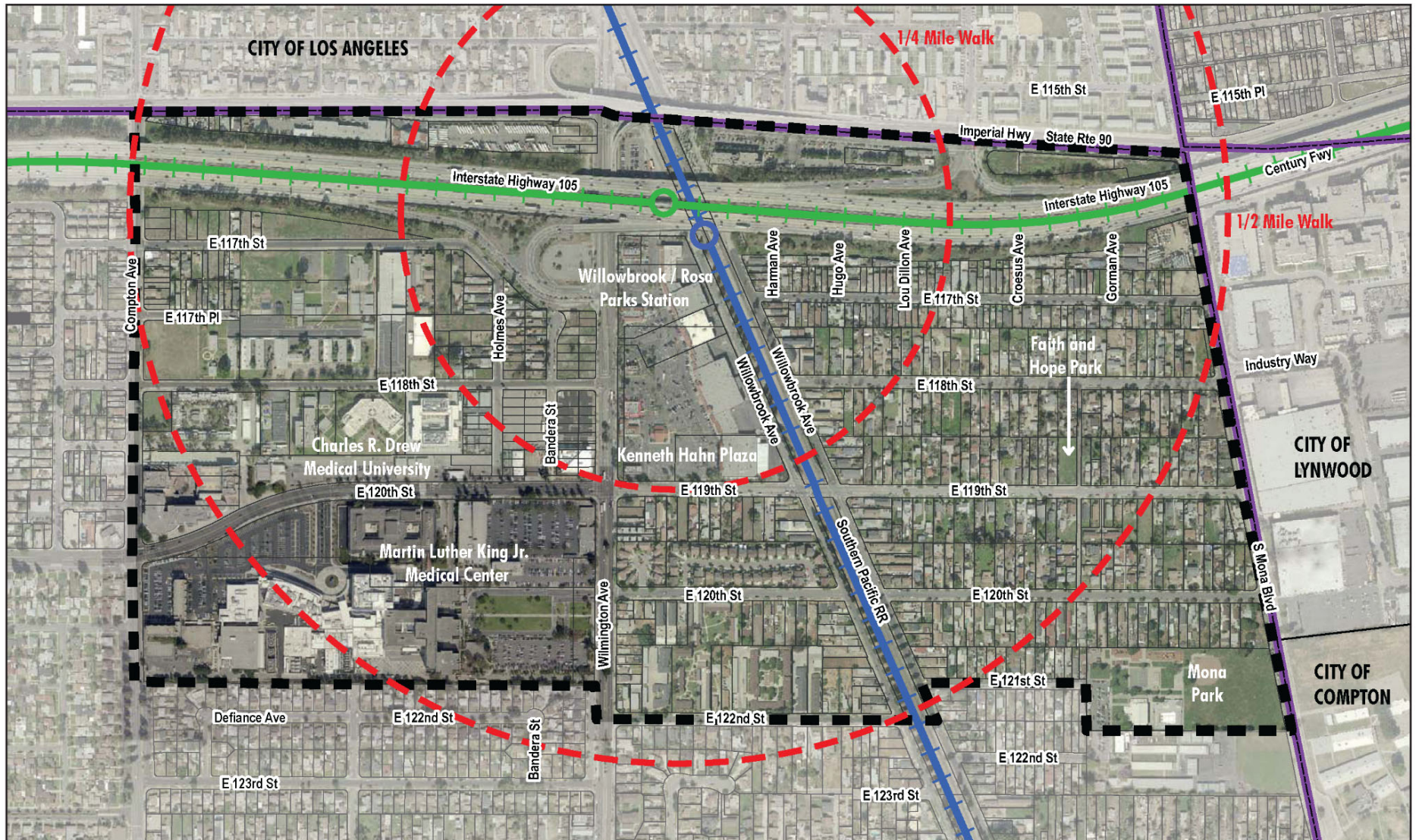
The Willowbrook Transit-Oriented District (TOD) Specific Plan (Willowbrook TOD Specific Plan or Specific Plan) area (Plan Area) is located in the unincorporated community of Willowbrook within the County of Los Angeles (County). It is located along the I-105 freeway and at the junction of the Metro A (formerly Blue) and C (formerly Green) Lines. The plan area is approximately ten miles south of downtown Los Angeles and is bordered by the incorporated cities of Los Angeles to the north, and Lynwood and Compton to the east.

The Willowbrook TOD Specific Plan is focused around the Willowbrook/Rosa Parks Station, a transfer station for the Metro A Line and C Line (see Figure 1-1). The Plan Area is approximately 312 acres. Some of the key facilities located within the Plan Area are Martin Luther King, Jr. (MLK) Medical Center, Charles R. Drew University of Medicine and Science (CDU), Kenneth Hahn Plaza, Willowbrook Library, and MLK Center for Public Health.

***What is a Specific Plan?*¹**

A specific plan is a tool to systematically implement the General Plan within an identified project area. Specific plans are used to ensure that multiple property owners and developers adhere to a common plan or coordinate multiple phases of a long-term development. Specific plans must further the goals and policies of the General Plan. Specific plans must be consistent with the General Plan. No local public works project may be approved; no tentative map or parcel map for which a tentative map was not required may be approved, and no zoning ordinance may be adopted or amended within an area covered by a specific plan unless it is consistent with the adopted specific plan. The authority for preparation and adoption of specific plans is set forth in the California Government Code section 65450 et seq.

¹ Chapter 6: Land Use Element, Los Angeles County General Plan.



-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro C Line
-  Metro A Line
-  1/4 Mile and 1/2 Mile radius around Willowbrook/Rosa Parks Station

**Figure 1-1
Specific Plan Area**

WILLOWBROOK TOD SPECIFIC PLAN



1.2 Background

The Los Angeles County General Plan 2035 (General Plan) identifies TODs as priority policy areas throughout the County. To create vibrant and healthy TODs, the County envisions developing specific plans for all of the TODs that are located in the unincorporated areas.

The Willowbrook TOD Specific Plan establishes a clear vision for the Willowbrook community, allows development that will lead to community benefits, and facilitates economic development. The Specific Plan is accompanied by a Program Environmental Impact Report (PEIR), which was prepared for the Specific Plan. The PEIR provides environmental clearance for specified levels of development intensity to potentially streamline future CEQA review.

1.3 Purpose

The purpose of the Willowbrook TOD Specific Plan is to revitalize the community within the project area and to improve access to all modes of transportation including transit, walking, and bicycling. Consistent with the goals and policies outlined in the General Plan, the Willowbrook TOD Specific Plan will:

- encourage transit-oriented development,
- promote active transportation,
- allow development that reduces vehicles miles traveled,
- allow development that creates community benefits, and
- streamline the environmental review process for future projects.

The Specific Plan is anticipated to facilitate development, especially residential and employment-generating uses, proximate to the Willowbrook/Rosa Parks Station. The primary objectives of the Specific Plan are to identify land use options that include mixed uses, increased housing opportunities, and neighborhood-serving retail uses. In addition, the Specific Plan is intended to foster a healthy community by improving pedestrian linkages between the Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, MLK Medical Center, CDU, future mixed-use areas, and existing residential neighborhoods.

1.4 Relevant Studies and Plans

The following recent and ongoing planning projects have provided direction and established a foundation for the Willowbrook TOD Specific Plan:

- Willowbrook Area Access Improvements, County of Los Angeles Department of Public Works (2015-2017)
- Willowbrook Community Parks and Recreation Plan (2016)
- Los Angeles County General Plan 2035 (2015)
- Rosa Parks Station Improvements Planning, Metro (2014-2015)
- Los Angeles County Transit-Oriented Districts Access Study, Southern California Association of Governments (SCAG) and County of Los Angeles (2013)
- Implementation Plan for the MLK Medical Campus and the Willowbrook Wellness Community, Initiative for Inner City Competitiveness, Next Street and Genesis LA (2013)

- Martin Luther King Medical Center Campus Master Plan & the Willowbrook MLK Wellness Community Vision (2012)
- County of Los Angeles Bicycle Master Plan (2012)
- Rosa Parks Metro Station Master Plan & Transit-Oriented District (2010)
- Martin Luther King, Jr. Medical Center and Surrounding Project Area, Urban Land Institute (ULI) Technical Assistance Panel (2009)
- Imperial-Wilmington Rosa Parks Metro Blue Line Rail Station Non-Motorized Access Plan, City and County of Los Angeles, Metro, (2009)

1.5 Community Engagement

Prior to and throughout the planning process for the Willowbrook TOD Specific Plan, staff from the County's Department of Regional Planning (DRP) conducted community outreach, engaging and receiving input from community groups and stakeholders including the following (in alphabetical order):

- Best Start Watts-Willowbrook
- Caltrans
- CDU
- City of Los Angeles
- City of Lynwood
- Commercial and residential developers of both market rate and affordable housing
- Concerned Citizens of Willowbrook, Dr. Delores Glass
- Gabrielino Band of Mission Indians - Kizh Nation
- Housing Authority of the City of Los Angeles (HACLA)
- Kimco Realty Corporation
- Metro
- MLK Community Hospital
- Watts Century/Latino
- Watts Labor Community Action Committee

In addition, DRP staff met regularly with a County Project Task Force, comprised of representatives from various County departments, and received input and feedback about the goals and direction of the Specific Plan.

DRP staff met with representatives of the Gabrielino Band of Mission Indians - Kizh Nation, to determine whether known tribal cultural resources are present in the Plan Area. While specific resources have not yet been discovered, the Plan Area is proximate to a known early trade route that connected to the coast at San Pedro. Additionally, its characteristics of being relatively near historical water sources and hunting grounds would have made the Plan Area favorable to supporting habitation. As is common in Southern California, it is possible that artifacts with tribal significance could be discovered in the Plan Area in activities that involve ground disturbance. Therefore, these activities should be undertaken with care to protect potential resources.

First Nations

The project site is located within the territory of the native population known today as the Gabrieleno. Prior to European contact, the Gabrieleno occupied a diverse area that included the watersheds of the Los Angeles, San Gabriel, and Santa Ana rivers; the Los Angeles basin and foothills; and offshore islands of Santa Catalina, San Nicolas, and San Clemente. In ancient times the village areas adjoined and overlapped with each other, at least during the Late Prehistoric and Protohistoric periods where resources were shared by adjoining villages. The village lifestyle was focused around commerce with local and long distance trade a prominent function of the community. Resources were attained through a hunter-gatherer lifestyle while living in permanent communities located near the presence of water, a stable food supply, and some measure of protection from flooding. Houses were made of tule mats on a framework of willow poles. Basketry and steatite vessels were used rather than ceramics; ceramics became common only toward the end of the Mission Period in the nineteenth century. Villages in and around the project area exhibited similar organization and resource procurement strategies and were based on clan or lineage groups with their home base sites marked by midden deposits often with bedrock mortars and fire hearth locations. Average community populations generally ranged from 50-100 inhabitants, although larger settlements existed. Trade routes provided for daily travel, recreation, and commerce activities to occur over thousands of years. During seasonal rounds to exploit natural resources, small groups would move along trading routes within their traditional territory in search of specific plants, minerals, and animals. Their gathering strategies often left behind signs of special use sites at the locations of the resources. As well, ceremonial areas would contain buried caches of relics that were used for ceremony and would be buried and dug up every time a ceremony was conducted. These buried caches were stored in the earth much like a storage cabinet would be used in a home for protection from damage and the elements. However, there were also buried caches of relics that were never meant to be dug up and were placed in a specific orientation in the earth.

1.6 Specific Plan Goals and Policies

The goals and policies set the framework for the Willowbrook TOD Specific Plan. They serve as guidelines for decision-making and provide direction for future activities. The goals and policies were derived from input received from the community and stakeholders during the community engagement process, County Project Task Force, and DRP staff during the planning process. The seven major goals identified for the Plan Area are as follows:

1. Preserve and enhance the character of the Willowbrook community.
2. Improve the Willowbrook/Rosa Parks Station and its environs.
3. Encourage transit-oriented development.
4. Provide affordable housing opportunities.
5. Promote active transportation and reduce vehicle miles traveled.
6. Improve quality of life for existing residents with improvements to the public realm.
7. Improve economic vitality and employment opportunities.

Goal 1: Preserve and enhance the character of the Willowbrook community.

Policy 1.1: Where appropriate, preserve the character of the residential neighborhoods.

Policy 1.2: Provide a mix of land uses in the Mixed-Use Zones to accommodate employment, retail, and residential uses, as well as local-serving amenities.

Policy 1.3: Preserve existing neighborhood-serving retail at Kenneth Hahn Plaza.

Policy 1.4: Improve compliance with County zoning and building codes on private property by expanding code enforcement by various County Departments.

Policy 1.5: Facilitate the expansion of the MLK Medical Center and CDU campus that is compatible and sensitive to the surrounding neighborhoods.

Policy 1.6: Coordinate with CDU to integrate the planned growth of the University's campus with the surrounding community, including creating pedestrian linkages and open space connections with other area institutions and the Willowbrook/Rosa Parks Station.

Policy 1.7: Implement the concepts and ideas contained in the *MLK Medical Center Campus Master Plan & the Willowbrook MLK Wellness Community Vision* by encouraging well-designed and continuous pedestrian paths and connections between the Willowbrook/Rosa Parks Station and the employment, campus, retailing, and residential areas.

Goal 2: Improve the Willowbrook/Rosa Parks Station and its environs.

Policy 2.1: Coordinate with Metro to enhance pedestrian and bicycle connections from the Willowbrook/Rosa Parks Station to the surrounding community.

Policy 2.2: Coordinate with Metro to enhance safety and circulation between the various transit modes at the Willowbrook/Rosa Parks Station to encourage transit use.

Policy 2.3: Work with Metro and Kenneth Hahn Plaza to create better connections and access to the surrounding employment, campus, retail, and residential areas.

Goal 3: Encourage transit-oriented development.

Policy 3.1: Provide a variety of housing choices within walking distance of the Willowbrook/Rosa Parks Station.

Policy 3.2: Implement mixed-use zoning in targeted areas to promote employment-generating uses proximate to housing and the Willowbrook/Rosa Parks Station.

Policy 3.3: Incentivize lot consolidation where appropriate to facilitate the development of cohesive projects in Mixed-Use Zones.

Goal 4: Provide affordable housing opportunities.

Policy 4.1: Preserve existing stock of affordable housing.

Policy 4.2: Promote housing affordability through diversification of housing choices (ownership, rental, single-family, multi-family) for varied income groups.

Goal 5: Promote active transportation and reduce vehicle miles traveled.

Policy 5.1: Provide a multi-modal transportation system of complete streets.

Policy 5.2: Enhance access to transit and the Willowbrook/Rosa Parks Station.

Policy 5.3: Provide a connected pedestrian and bicycle network that links Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, new mixed-use areas, CDU, MLK Medical Center and residential neighborhoods.

Policy 5.4: Facilitate mixed-use development that maximizes pedestrian connectivity and minimizes the need for vehicle travel.

Goal 6: Improve quality of life for existing residents with improvements to the public realm.

Policy 6.1: Enhance the public realm with street trees, street furniture, bicycle facilities, sidewalks and pedestrian paths.

Policy 6.2: Provide a consistent canopy of shade trees throughout the Specific Plan area to enhance pedestrian comfort.

Policy 6.3: Provide pedestrian-scaled lighting to improve safety and enhance pedestrian environment.

Policy 6.4: Encourage outdoor dining and seating areas and other pedestrian-friendly uses in mixed use areas.

Policy 6.5: Explore joint use agreements with schools to better utilize existing and future open space resources.

Policy 6.6: Require new development to provide public open space as a community benefit, as appropriate. Consider providing incentives to developers for such provisions.

Policy 6.7: Build pocket parks and community gardens on available County-owned vacant lots.

Goal 7: Improve economic vitality and employment opportunities.

Policy 7.1: Create economic opportunities for the Willowbrook community by fostering a complementary variety of employment, retail, residential, and institutional uses.

Policy 7.2: Build on the Willowbrook community’s economic base as a “healthcare cluster” by working with appropriate partners to provide workforce development opportunities for local residents.

Policy 7.3: Facilitate public-private partnerships to share responsibility for implementing this Specific Plan and achieving its goals.

Policy 7.4: Encourage a mix of national brand and local merchant businesses.

Policy 7.5: Efficiently manage the supply and demand of parking to accommodate customer, commuter, and resident parking and encourage the use of shared parking where possible.

Land Use and Urban Design Framework

2.1 Introduction

The goal of the Land Use and Urban Design Framework is to develop the Plan Area as a system of cohesive and integrated uses, spaces, structures, and environments. A clear framework provides visitors and residents with an understanding of how to easily and safely find their way around and to efficiently identify uses and activities. Arrangement of land uses into identifiable subareas, improved streetscapes, connected open spaces, and enhanced landmarks will help make the Plan Area accessible and distinctive.

This Chapter describes the approach to land use and urban design that will promote and allow transformation of the Plan Area into a vibrant transit-oriented district, while strengthening its connections to the adjacent residential neighborhoods and the rest of the Willowbrook community. This chapter also presents the short-term and long-term land use strategies for the various institutions and facilities in the Plan Area, which will be important in achieving the community goals.

2.2 Land Use and Urban Design Framework

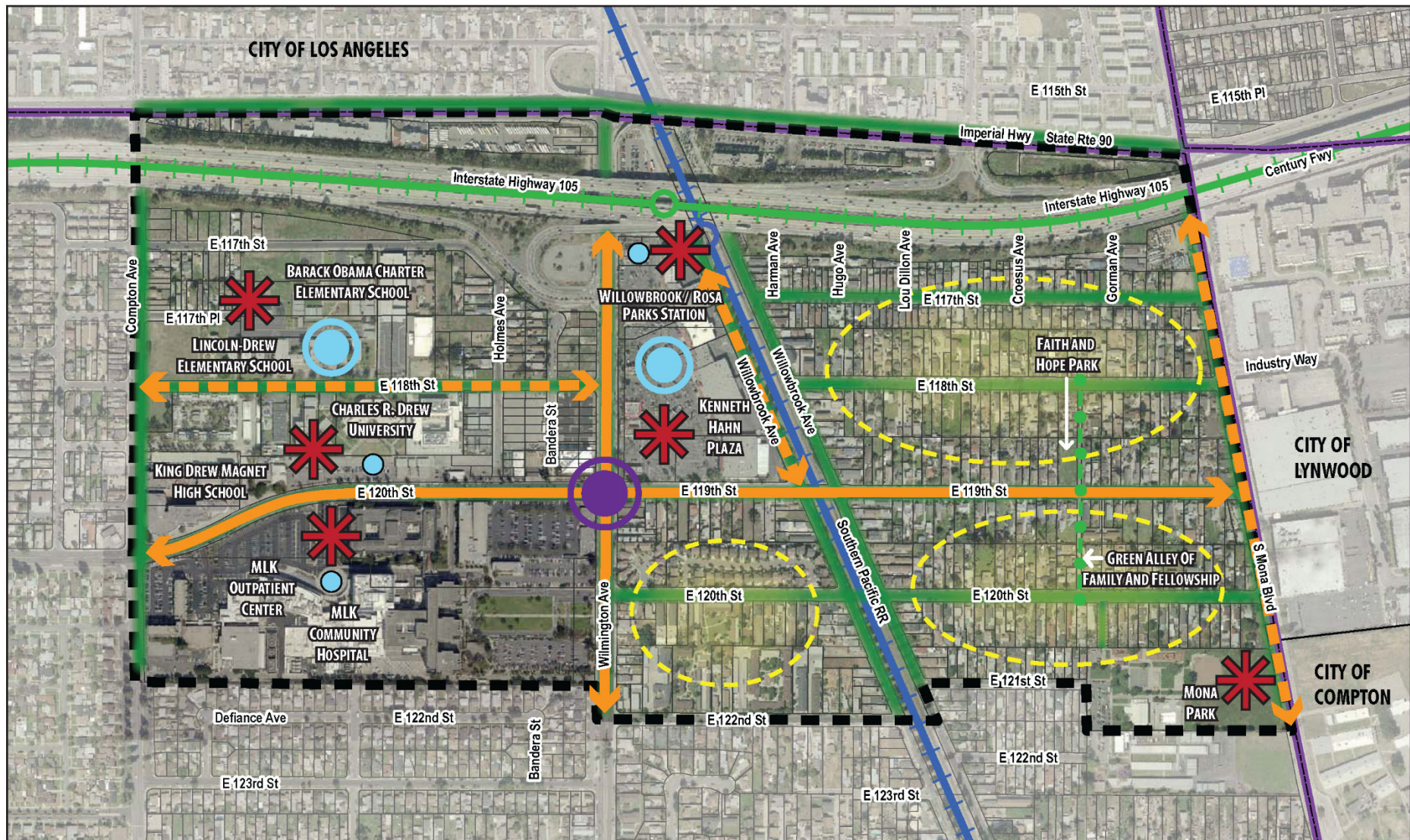
Elements of the Land Use and Urban Design Framework, as illustrated in Figure 2-1, include:

Specific Plan Subareas

The major activity centers in the Plan Area include the Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, MLK Medical Center campus, CDU campus, and Mona Park. Section 2.3 describes the distinct subareas into which the Plan Area can be divided. The existing uses in, and strategies for, planned and future uses in these subareas are also discussed in this chapter.

Mobility Improvements

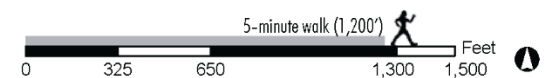
Improvements to the vehicular, pedestrian, bicycle, and transit network that enhance connectivity and ease of movements for active transportation modes are further described in Chapter 4. Road diets for major pedestrian connections are further described in Chapter 4.



- | | | |
|------------------------------------|--------------------------------------|---|
| County of Los Angeles | Major Activity Centers | Entry Plazas |
| Willowbrook TOD Specific Plan Area | Streetscape Improvements | Future Open Space/Plaza |
| Metro C Line | Primary Pedestrian Connections | Major Intersection |
| Metro A Line | Secondary Pedestrian Connections | Protect and Enhance Residential Neighborhoods |
| | Green Alley of Family and Fellowship | |

Figure 2-1
Urban Design Framework

WILLOWBROOK TOD SPECIFIC PLAN



Pedestrian and Bicycle Connections

The Urban Design Framework establishes enhanced pedestrian and bicycle connections along the street network between the Willowbrook/Rosa Parks Station and the activity centers. Additional bicycle facilities, such as a Class I bicycle path or bikeway on Mona Boulevard, to complete the bicycle network within the Plan Area are also described in Chapter 4. Enhancements to the pedestrian experience such as pedestrian-oriented intersection improvements and wider sidewalks, where feasible, are also included.

Major Intersection – Wilmington Avenue and East 120th/East 119th Street

The two major streets within the Plan Area are Wilmington Avenue in the north-south direction and East 120th/East 119th Street in the east-west direction. These two streets connect the major activity centers to each other, as well as connect across the rail tracks with the residential neighborhoods to the east. They also intersect to form the pedestrian core or “heart” of the Plan Area. Design concepts for special paving treatments at this intersection are described in Chapter 5.

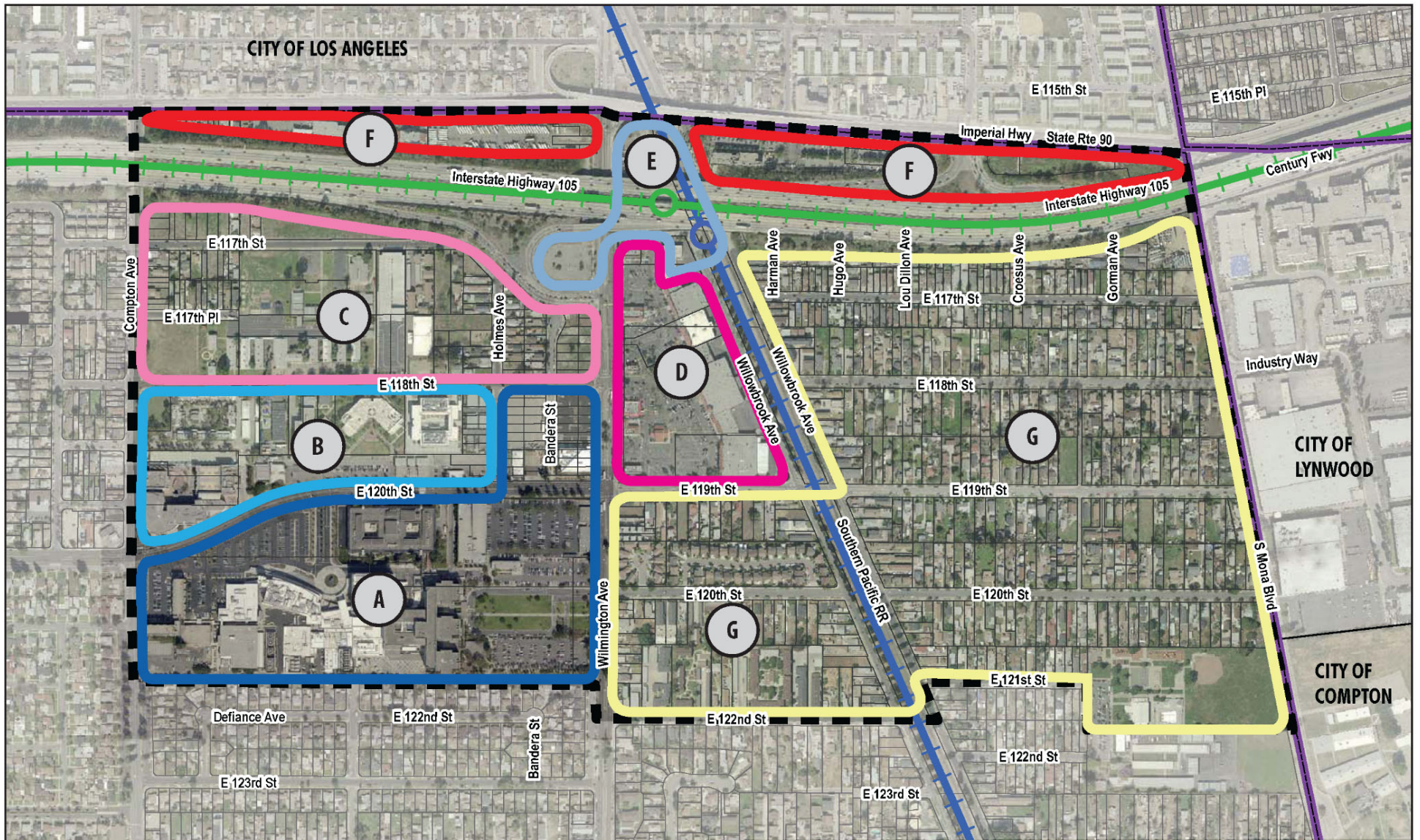
Streetscape and Open Spaces

Another important element of the Urban Design Framework is improved streetscapes and connected open spaces. These streetscape improvements are intended to enhance and unify the visual and spatial experience of the driver, pedestrian, and the bicyclist, and help provide key linkages between the activity centers and the residential neighborhoods in the Plan Area. Recommended streetscape improvements consist of an interrelated palette of street trees, street furniture, street lighting, wayfinding signage and landscaping. These elements are described in Chapter 5.

Existing and planned open space resources in the Plan Area include Mona Park located along Mona Boulevard in the southeast corner of the Specific Plan area. Faith and Hope Park is located mid-block along East 119th Street and will be linked to Mona Park by the Green Alley of Family and Fellowship. Other important open space resources are the MLK Fitness Garden located primarily along 120th Street at the MLK Center for Public Health and the proposed Fellowship Garden of Love at Holmes Community Garden that will be located at the corner of East 118th Street and Holmes Avenue. The Specific Plan encourages the creation of additional public open space as a part of new or expanded mixed-use developments in the Plan Area.

Residential Neighborhoods

The preservation and enhancement of the existing residential neighborhoods is an important element of the Urban Design Framework. Key strategies include maintaining the density and character of the neighborhoods (Chapters 2 and 3) such that local equestrians will continue to feel comfortable riding their horses on residential streets and ensure that residents can continue to keep chickens and use outdoor clotheslines. Further strategies include improving safety and comfort for navigating the street network (Chapter 4) and improving connections to the activity centers in the Plan Area (Chapter 5).



County of Los Angeles

Willowbrook TOD Specific Plan Area

Metro C Line

Metro A Line

A MLK Medical Center and Associated Facilities

B CDU Campus Area

C Northwest Subarea

D Kenneth Hahn Plaza

E Willowbrook/Rosa Parks Station

F Imperial Highway Corridor

G Residential Neighborhoods

Figure 2-2
Specific Plan Subareas

WILLOWBROOK TOD SPECIFIC PLAN



2.3 Specific Plan Subareas

The Plan Area supports a range of land uses, including residential, retail, office, and other commercial, as well as educational and institutional facilities and services. It can be divided into the following seven subareas as shown in Figure 2-2:

- MLK Medical Center and Associated Facilities
- CDU Campus Area
- Northwest Subarea
- Kenneth Hahn Plaza
- Willowbrook/Rosa Parks Station
- Imperial Highway Corridor
- Residential Neighborhoods

The uses in these subareas are complementary and supportive of each other. These subareas connect through the street network and a system of pedestrian walkways and open space areas. This section describes the existing conditions and the strategies for planned and future development within each subarea.

A. MLK Medical Center and Associated Facilities

Existing Conditions

The MLK Community Hospital reopened in 2015 after being closed in 2007. It is located on approximately 38 acres of land, and the campus is bounded by Wilmington Avenue to the east, East 120th Street to the north, Compton Avenue to the west, and the Specific Plan boundary to the south. The hospital is expected to serve 1.2 million residents in South Los Angeles, including Compton, Inglewood, Watts, Willowbrook, and Lynwood. It is also expected to create more than 1,800 jobs in the area with approximately 900 directly at the hospital.¹



Martin Luther King, Jr. Community Hospital



MLK Outpatient Center

Several other facilities serving the health care needs of the community are located adjacent to the hospital, including the MLK Center for Public Health, which focuses on preventative care, and the MLK Mental Health Center and MLK Outpatient Center, all operated by the County. The MLK Outpatient Center operates more than 70 primary care and specialty care clinics to serve the health care needs of

¹ www.mlkcommunityhospital.org

residents in the surrounding communities. The MLK Center for Public Health includes the MLK Fitness Garden located primarily along the 120th Street frontage that incorporates a walking path and par course fitness equipment for employees and the public.



MLK Center for Public Health



MLK Fitness Garden

The County seeks to establish MLK Medical Center Campus as a center of excellence for health care delivery, urban health promotion and prevention, health workforce development, academic research and teaching, and economic development.² In 2009, the County commissioned the preparation of an EIR that analyzed the potential for significant environmental impacts in association with the proposed MLK Medical Center Campus Redevelopment Project (MLK EIR). The MLK EIR analyzes two tiers: Tier I,³ project-level development, and Tier II,⁴ program-level development. In addition, in 2009, the County Board of Supervisors found that the repurposing of the existing facilities for hospital use was categorically exempt from CEQA.

Following the certification of the MLK EIR, the County approved the *MLK Medical Center Campus Master Plan & The Willowbrook MLK Wellness Community Vision* document in 2012 as a guiding framework for future development. It calls for a coordinated system of pedestrian and bicycle circulation within the campus and the surrounding area.

Tier I development, as described in the MLK EIR, has been completed. As part of Tier II, a 50,000 square-foot medical office building and a 1,400-car parking garage are also in the planning and design stages. Other projects in the pipeline include expansion/construction of two clinics focused on providing services for children. The Los Angeles County Department of Children and Family Services (DCFS)

² *Martin Luther King, Jr. Medical Center Campus Redevelopment Draft EIR, August 2010.*

³ *Tier I analyzed the vacation of the emergency room, storage buildings, cooling towers, and existing Los Angeles County Martin Luther King, Jr. Multi-Service Ambulatory Care Center (MACC), and the development of two new environmentally sustainable buildings: the 132,000 square-foot new MACC and the 24,700 square-foot Ancillary Building. Tier I development consisted of approximately 170,332 square feet of new development. In addition, tenant improvements in existing buildings, site improvements, and potential relocation of the MRI Building was analyzed in Tier I.*

⁴ *Tier II analyzed a campus-wide Master Plan. It is anticipated that the development described in the Master Plan would prepare the proposed project site for future mixed-use campus-related development that would provide the health services necessary to respond to and address the needs of the community. Tier II would have the potential to build out approximately 1,814,696 square feet of development on the proposed project site with mixed uses including medical office, commercial, retail, office space, recreation, and other development in support of the campus. The net new development of the proposed project would be approximately 1,476,010 square feet. Tier II also analyzed the construction of up to 100 residential units, to be developed at a multi-family density consistent with surrounding residential area multi-family development densities. In addition, the Tier II components would entail the reuse or replacement of the existing MACC building, emergency room, storage building, and cooling towers.*

Specific Plan Strategy

The Willowbrook TOD Specific Plan envisions that the MLK Medical Center campus will continue to grow and thrive as a regional destination for medical, health and associated services. The development on the campus will be broadly guided by the *MLK Medical Center Master Plan and the Willowbrook MLK Wellness Community Vision*. Required parking for the medical uses will be provided on campus. The Specific Plan also builds upon the recommendations for enhanced pedestrian connections between the MLK Medical Center campus and the other activity areas in the Plan Area.



B. CDU Campus Subarea

Existing Conditions

Immediately north of the MLK Medical Center campus are CDU, a private university, and King Drew Magnet High School, part of the Los Angeles Unified School District (LAUSD). These institutions are bounded by Holmes Avenue to the east, Compton Avenue to the west, East 120th Street to the south and East 118th Street to the north.

CDU has been in the Willowbrook community since the 1960s and is recognized by the Department of Education under Title III, Part B as a Historically Black Graduate Institution. CDU provides training in

medicine, medical technology, public health, and nursing under the auspices of three schools: the College of Medicine, the College of Science and Health, and the Mervyn M. Dymally School of Nursing.

Hospital-related occupations have been the focus of King Drew Magnet High School's curriculum since its establishment in the early 1980s. The Medicine and Science Careers Program, the centerpiece of the school's offerings, provides an opportunity for students to access experience-based learning in environments including hospitals, clinics, research laboratories, and university facilities. The school is located on the northeast corner of Compton Avenue and East 120th Street.

Other uses located within this area include a multi-family project on CDU-owned land on East 118th Street, several surface parking lots that serve CDU, and other County facilities in the area that are located along East 120th Street.



Charles R. Drew University of Medicine and Science and LAUSD King Drew Magnet High School are anchors in the CDU Campus Subarea.

Specific Plan Strategy

The Willowbrook TOD Specific Plan strategy includes supporting CDU's long range planning efforts. CDU is preparing a master plan that contemplates an increase in its student population from 625 students to 1450 students over a twenty to twenty-five year period. Today, CDU is primarily a commuter campus. The master plan includes new residential facilities (119 units) within the campus. The proposed housing types include dormitories for undergraduate students, shared housing for graduate students, and housing for visiting faculty. The master plan also provides conference facilities, incubator space, research labs, and additional student classrooms and facilities. The growth of the CDU campus will bring the campus community (faculty, students, workers, etc.) buying power to the Willowbrook community.

With MLK Community Hospital, MLK Center for Public Health, and other health-related facilities in the area, CDU and King Drew Magnet High School play an important role in fostering Willowbrook as a "health focused cluster" in South Los Angeles. The Willowbrook TOD Specific Plan reinforces the importance of these two facilities and builds upon the recommendations of CDU's master plan.

CDU's master plan aims to unify the campus into a cohesive setting with the creation of quad spaces and new buildings. The planned buildings are four to six stories in height. CDU's master plan expands the parking structure and parking lot located on East 118th Street to provide all required parking for the campus.

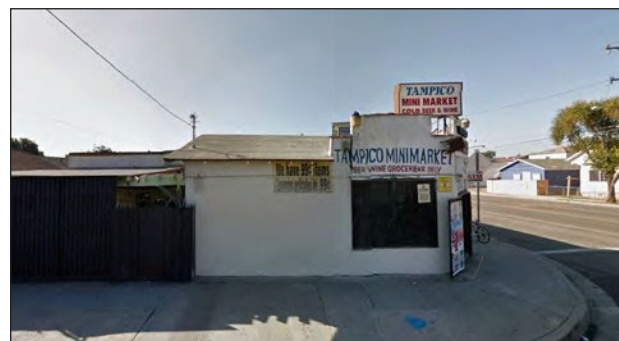
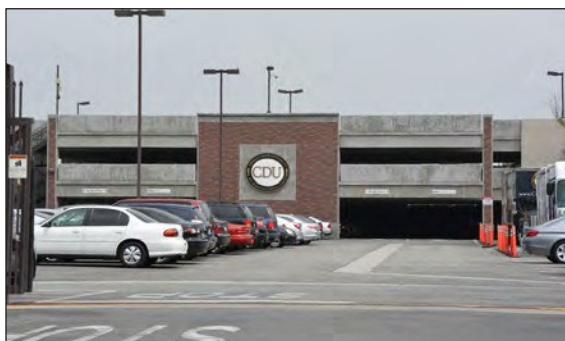
The Willowbrook TOD Specific Plan requires new campus buildings to be set back from the adjoining streets. To be responsive to the scale of the residential uses across Compton Avenue, new buildings along Compton Avenue are limited to four stories in height and are to be set further away from the Compton Avenue property line. Architects and designers for CDU must consider architectural cohesiveness in terms of materials and style and outdoor spaces as new buildings are designed and built.

CDU's master plan also includes a pedestrian, bicycle and shuttle circulation network system that connects with the activity centers in the Specific Plan area. Campus pedestrian and bicycle paths are connected with the public street network and activity centers in the vicinity, including the hospital to the south, and Willowbrook/Rosa Parks Station and Kenneth Hahn Plaza to the east. The CDU shuttle also connects to these activity centers and there is an opportunity to consolidate the various shuttles in the area to improve shuttle routes and service frequency.

C. Northwest Subarea

Existing Conditions

The northwest subarea is an aggregation of a variety of uses – educational, retail, residential and institutional. Several vacant lots, owned by the Los Angeles County Development Authority (LACDA), are located along East 117th Street. A large vacant site on the northeast corner of East 118th Street and Compton Avenue is owned by the Compton Unified School District (Compton Unified). Educational uses include Lincoln-Drew Elementary School (a part of Compton Unified), and Barack Obama Charter Elementary School. Both are located north of East 118th Street. Parking facilities for CDU are located adjacent to the schools. Other uses in this subarea



Two elementary schools, CDU parking and a corner market are found in the Northwest Subarea.

include a water well and reservoir operated by the Liberty Water Company. The residential uses are a mix of single-family homes, duplexes and multi-family structures. The block between Wilmington Avenue and Bandera Street, north of East 118th Street, is the site for The Gateway at Willowbrook, an affordable senior housing project that is currently under construction. The relocated Willowbrook Public Library will be located on the ground floor of this development.

Specific Plan Strategy

The northwest subarea is adjacent to the MLK Medical Center and CDU to the south and Willowbrook/Rosa Parks Station and Kenneth Hahn Plaza to the east. The Specific Plan envisions that over time, this subarea will capitalize on this proximity and become a mixed-use district with employment generating uses and residential infill.

The Work Force Collaborative, among other groups and studies, has suggested that the Willowbrook community become a “health cluster” with MLK Medical Center campus and CDU as the anchors, leading to numerous opportunities for health-related occupations at the two facilities. There is also opportunity for growth in related fields such as medical back office, laboratory facilities, hospital equipment facilities, and medical or university support businesses that would provide job opportunities for local residents. Residential infill housing would increase housing supply for current and future workers, students, and residents in the area. The Specific Plan will facilitate this transition to more intensive uses while the existing single-family residences in the subarea may continue as legally non-conforming.

Lincoln-Drew Elementary School and Barack Obama Charter Elementary School

The two school sites could be consolidated and reconfigured for more efficient use of the land. The California Department of Education Guide to School Site Analysis states that infill school sites can be much smaller than previously recommended. With the available LACDA-owned and Compton Unified-owned properties in the subarea and the partial vacation of East 117th Street, a 15-acre development parcel could be assembled with minimal acquisitions (See Figure 2-3). Five-to six-acres could be reserved for new school construction with the remainder available for development. The proceeds could be used to fund the reconstruction of the elementary and charter schools and any large scale development should provide a public open space for the community.

Commercial users would likely include corporations looking to associate with the new MLK Medical Center campus and/or CDU, and the parcel could be an ideal “back office” administrative location. The parcel is large enough to accommodate a business park, which could attract a variety of healthcare related office uses, or function as a regional administrative facility for a healthcare provider. A portion of this parcel could also be allocated to a healthcare/wellness incubator campus.

The development parcel would also be attractive to several types of residential uses. The location near CDU makes it an attractive location for university housing and its proximity to the Willowbrook/Rosa Parks Station makes it a prime location for work force housing and transit-oriented development.

This subarea will benefit from parking reductions in the Specific Plan and there is an opportunity to explore creating a parking management district. The LACDA-owned properties between East 117th Street and the 105 Freeway can be the location for parking, while at the same time serving the additional purpose of providing a buffer between the freeway and new development.

Examples of employment generating uses and multi-family residential developments from other communities are depicted on the following page.

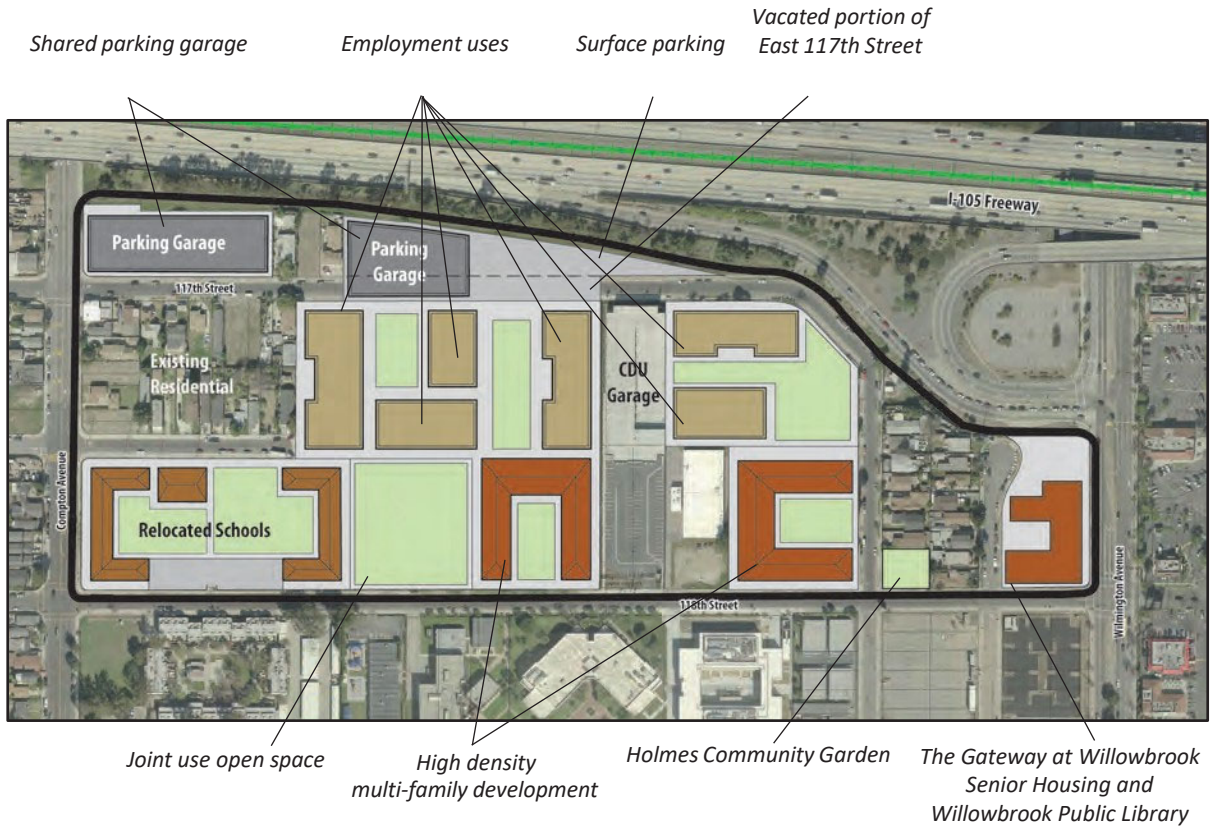


Figure 2-3: Illustrative Site Plan for the Northwest Subarea

This site plan shows one way the Northwest subarea could be developed under the development regulations and standards of the Specific Plan. Through lot consolidation and development of a unified project at higher densities, this subarea offers the potential for an employment-generating district with new residential opportunities. This plan is illustrative only; it is not intended to prescribe this particular site layout or use.



The Northwest subarea is envisioned to accommodate employment-generating uses in office and/or business park settings. Multi-family developments are anticipated to provide workforce, student, and transit-dependent user housing options.

D. Kenneth Hahn Plaza

Existing Conditions

Kenneth Hahn Plaza is a privately-owned shopping center on County-owned land. It is located immediately south of the Willowbrook/Rosa Parks Station, and is bounded by Wilmington Avenue to the west, East 119th Street to the south and Willowbrook Avenue to the east. The major anchor tenant is a Food 4 Less grocery store. Other tenants include Rite-Aid, General Discount, and DaVita Dialysis Center. Food establishments in the center include McDonald's, Taco Bell, Pizza Hut, and Denny's, which is the only sit-down restaurant in the area. The Plaza is also the site of the Los Angeles County Willowbrook Public Library (soon to be relocated) and a Los Angeles County Sheriff substation.

The shopping center has a suburban layout with buildings sited at the rear and parking in front. Most of the fast-food restaurants are located in smaller structures adjacent to Wilmington Avenue. The shopping center presents an uninviting and blank facade towards East 119th Street, which has mostly single-family uses along the south side. The loading areas are located along Willowbrook Avenue. An internal service road between the buildings and rear property line provides access for trucks and other service vehicles. The entire site is surrounded by a six-foot tall wrought iron security fence blocking a pedestrian connection between the Willowbrook/Rosa Parks Station and the shopping center.



Kenneth Hahn Plaza currently has neighborhood serving retail as anchors. It is also isolated from its surroundings by fencing.

The Kenneth Hahn Plaza site is owned by the LACDA. Metro is acquiring approximately 1.5 acres of land on the northern end of the site for the expansion of the Willowbrook/Rosa Parks Station. The rest of the site is being purchased by the shopping center operator, Kimco Realty Corporation.

Specific Plan Strategy

The Kenneth Hahn Plaza site is the primary retail location within the Plan Area. While the site is a prime opportunity for redevelopment, there is also strong need and community desire to retain a grocery store and other neighborhood serving retail in this location.

Long-Term

Over the long-term, the Specific Plan envisions the Kenneth Hahn Plaza will transform to a mixed-use TOD project that takes advantage of the proximity of the Willowbrook/Rosa Parks Station. The MLK Medical Center Campus Master Plan, Urban Land Institute (ULI) Technical Advisory Panel study, and Rosa Park Station Study, all have also envisioned a mixed-use TOD project on the site. The Specific Plan envisions the site providing an integrated, complementary residential and commercial development emphasizing neighborhood serving retail, restaurant and service uses. Development can be integrated horizontally or vertically with residential or office uses above retail uses.



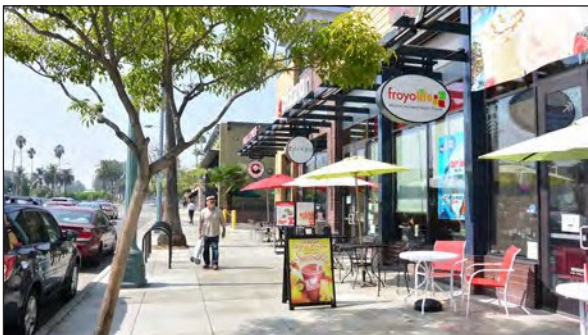
Over the long-term, new mixed-use buildings with neighborhood serving retail on the ground floor could transform Kenneth Hahn Plaza to a vibrant transit-oriented development.

Densities allowed per this Specific Plan are appropriate for both vertical and horizontal mixed-use projects with buildings up to four stories tall. Residential uses must be designed to minimize the noise and vibration impacts of the rail traffic along Willowbrook Avenue. Parking may either be in consolidated lots, under podiums or in structures.

East 119th Street is the primary connection with the residential neighborhoods across the rail tracks within the Plan Area. As such, it is an extremely important connection that needs to be improved in terms of appearance, safety, visibility, and amenities. Infill uses along East 119th Street on the north side could either be storefronts with retail or service uses (child care, senior care, attorney offices etc.) or residential town-home/walk-ups. The massing and scale of these buildings should be lower than the rest of the site as a transition to the single-family residential uses on the south side of East 119th Street. As development occurs along the north side of East 119th Street, a required greater setback will enlarge the public realm along this important pedestrian travel corridor.



Retail or service uses will activate the north side of East 119th Street, the primary east-west connection in the Specific Plan area.



New retail infill along Wilmington Avenue should activate the Wilmington frontage.

As a part of the Kenneth Hahn Plaza redevelopment, a large outdoor plaza or gathering space should be created similar to other successful shopping centers in the region. Such a space should have amenities such as street furniture, landscaped open space, public art, a water feature, and concessions. The site also has the potential for retail infill along Wilmington Avenue that will help create a more pedestrian friendly environment along that street.



The redevelopment of Kenneth Hahn Plaza should include a large outdoor plaza or gathering space.

Short-Term

In the short-term, it is likely that the Kenneth Hahn Plaza will continue as a shopping center that provides neighborhood-serving retail and service uses. A better mix of retail tenants is needed to serve the shopping needs of Willowbrook area residents and employees and visitors to the newly opened MLK Community Hospital and other existing facilities in the Plan Area. Active tenant recruitment will be necessary to improve the tenant mix at the Kenneth Hahn Plaza.

Physical upgrades and improvements that can improve the environment in the Kenneth Hahn Plaza include enhanced landscaping in the parking lot and along the periphery of the shopping center. Landscaping can also help create clearly identifiable paths of travel for pedestrians to navigate around the shopping center and minimize conflicts between the drive-thru lane entrances and pedestrian travel. The main entry drive on Wilmington Avenue should be improved with sidewalks on both sides of the driveway, better landscaping, and marked crosswalks for pedestrian safety.

Removal of the perimeter fence should be considered to better integrate the Kenneth Hall Plaza with surrounding uses. At a minimum, pedestrian access to the Willowbrook/Rosa Parks Station should be restored by removing the fence between the two facilities. Other improvements include better lighting in the parking lot and improved pedestrian linkages with the surrounding activity centers.

E. Willowbrook/Rosa Parks Station

Existing Conditions

The Plan Area has many options for public transportation, including bus service and the Willowbrook/Rosa Parks Station, which is a major transfer point between the Metro A and C light rail lines. The C Line platform is located in the median of the elevated I-105 (Glenn Anderson) Freeway above the A Line platform which is at street level. In addition to the two rail lines, the station is served by six Metro bus lines, and several municipal bus lines and community shuttles. A Metro public parking lot is located north of the I-105 freeway at Wilmington Avenue. Another Metro park-and-ride lot is located across Wilmington Avenue, within the off-ramp loop.

The physical environment at the Willowbrook/Rosa Parks Station needs major improvements. It is poorly lit, difficult to access and navigate, and poorly connected to its surrounding environment. It is blocked off from Kenneth Hahn Plaza by a fence and access to the residential neighborhoods to the east of the tracks is limited. This station also has one of the highest crime rates in the Metro system.



The Willowbrook/Rosa Parks Station environment is inhospitable and unattractive.

Metro is working on the Willowbrook/Rosa Parks Station Improvement Project that is intended to solve many identified issues. The design, still preliminary at the time of writing this Specific Plan, proposes acquiring 1.5 acres from Kenneth Hahn Plaza, relocating the A Line platform to the south; restructuring the entry area and creating better connections for automobiles, buses, bicyclists, and pedestrians. The following improvements are included:

- Extend the A Line platform to reduce crowding;
- Create a new south at-grade pedestrian crossing to the A Line platform;
- Upgrade the elevators, escalators, and stairs;
- Improve the East Bus Depot to create a safer and more comfortable area for patrons;
- Reconfigure the West Bus Depot to relocate the bays from underneath the freeway;
- Reconfigure both Park & Ride lots for optimal circulation and pedestrian safety;
- Improve pedestrian circulation through the Station by creating a new Transit Hall;
- Add a new Sheriff's facility;

- Add a new Mobility Hub, including a Bike Hub;
- Add a new Metro Customer Service Center;
- Add new canopies on the extended A Line platform and at the open-air bus bays; and
- Improve lighting, landscaping, and signage throughout the site.



These images illustrate the reconceived entry plaza for the Willowbrook/Rosa Parks Station and its relationship with the rest of Kenneth Hahn Plaza. (Source: Metro)

Specific Plan Strategy

The Specific Plan supports the improvements planned at the Willowbrook/Rosa Parks Station. Several of the improvements are critical to improving pedestrian access and connectivity in the Plan Area. The at-grade rail right-of-way is a barrier between the residential neighborhoods to the east and institutional uses to the west. Improvements to the at-grade crossing at the north end of the A Line platform will better connect the residential neighborhoods to the east of the tracks and complement the planned crossing at the south end of the platform. Improvements to the intersection of Wilmington Avenue and the I-105 Freeway off-ramps are needed to improve access across Wilmington Avenue to the Metro Park & Ride lot in the on-ramp loop and are described in greater detail in Chapter 4. Lastly, a key improvement will enhance bicycle access to the Station. A Class I bicycle path or cycle track planned along Willowbrook Avenue West between Imperial Highway and East 119th Street will complete a missing connection in the bicycle network. This stretch of Willowbrook Avenue West will also become a dedicated busway.

F. Imperial Highway Corridor

Existing Conditions

Imperial Highway is one of the busiest east-west streets in Los Angeles County. It is generally three lanes in each direction in the Plan Area and is on a viaduct with frontage access roads crossing above Wilmington Avenue. It is wholly uninviting to pedestrians. Uses along Imperial Highway are generally a mix of auto repair, retail, residential, Metro facilities, and underutilized and vacant lots. A school bus parking lot and a Metro maintenance yard are located to the west of Wilmington Avenue. A barber shop and towing yard are located further west near Compton Avenue. A couple of new homes have been built recently in this section as well. Other Metro facilities, including a parking lot, are located to the east of Wilmington Avenue. A vacant site owned by the Housing Authority for the City of Los Angeles (HACLA) is also located along this segment of Imperial Highway. On the north side of Imperial Highway, in the City of Los Angeles, is the Imperial Courts public housing. There is interest in creating a pedestrian crossing to the HACLA site.

Specific Plan Strategy

The parcels along Imperial Highway are sandwiched between the street and the I-105 Freeway. This corridor is suited to less intensive commercial and service uses. Some of the existing uses, such as Metro maintenance yards and parking facilities, fall within this category. Other similar uses, such as self-service public storage facilities may be appropriate. HACLA has tentative plans to build a WorkSource center and some of the area will serve as temporary staging space during Metro's station improvements. Another use under consideration is a communications equipment building for high-speed Internet service for the City of Los Angeles.

If the WorkSource Center or another use that will attract pedestrian traffic is proposed, pedestrian crossings on Imperial Highway, including new crosswalks where feasible, should be considered. Currently, there are no crossings between Mona Boulevard and Willowbrook Avenue. While new residential uses along this corridor are prohibited, the existing residential uses in the subarea may continue as non-conforming uses.



Low-intensity commercial and service uses are appropriate for the Imperial Highway corridor.

G. Residential Neighborhoods

Existing Conditions

Residential uses within the Plan Area are a mix of single-family homes; two-family units; and apartments, condominiums, and townhouses. The residential area south of Kenneth Hahn Plaza, east of Wilmington Avenue and west of Willowbrook Avenue is primarily multi-family, with a mix of mostly two-story apartments and detached townhomes. The area bounded by Mona Boulevard, I-105 Freeway, Willowbrook Avenue, and East 121st Street has mostly one-story, single-family homes except along Willowbrook Avenue East fronting the Metro A Line tracks which is primarily multi-family. The homes are often fenced and have landscaped front yards. Parking vehicles in the front yard setback and on sidewalks as well as general property upkeep are identified challenges. Residential parcel configurations vary dramatically across the Plan Area. In a majority of the residential blocks, parcels are 90 feet wide and more than 200 feet deep. However, some of the parcels are as narrow as 30 feet wide and approximately 100 feet deep. Many of the larger parcels have two or more units, some built without approval or permits.



Single-family homes are the predominant use east of the rail tracks within the Willowbrook TOD Specific Plan area.

Some of the other challenges include illegal conversions/construction, lack of street amenities, and outdated provisions in the Willowbrook Community Standards District.

Specific Plan Strategy

The Willowbrook TOD Specific Plan preserves and enhances existing residential uses in this area. In keeping with the community's desires, the Specific Plan does not increase base residential densities. The Specific Plan lays out the street improvements for the residential neighborhoods including a street tree program, pedestrian-scaled street lighting, and other street amenities. The Specific Plan also improves pedestrian and bicycle connectivity for the residents to the major activity centers in the Plan Area.

Implementing Zones

3.1 Specific Plan Zones

The Willowbrook TOD Specific Plan establishes zoning for parcels within the Specific Plan boundary as identified in Figure 3-1. The zones for the Plan Area are as follows:

- Mixed-Use 1 (MU-1)
- Mixed-Use 2 (MU-2)
- MLK Medical and Overlay
- Drew Educational
- Imperial Commercial
- Willowbrook Residential 1
- Willowbrook Residential 2
- Willowbrook Residential 3
- Open Space (O-S)

Please refer to Title 22, Planning and Zoning, Chapter 22.412 for land use regulations and development standards for each Specific Plan zone. Parking requirements for uses in the Specific Plan are also in Chapter 22.412. Additional design guidelines are contained in Sections 3.2 - 3.6 of this Chapter. Note: Figure 3-1, Specific Plan Zones, is included herein for illustrative purposes only.

A. Mixed Use Zones

The Mixed Use Zones are established to promote walking, bicycling, recreation, transit use, and community reinvestment and are intended to facilitate integrated commercial and residential development through optimal site planning and efficient use of land.

1. Mixed Use 1 (MU-1) Zone. The Mixed Use 1 (MU-1) zone is intended to encourage commercial and residential development, with an emphasis on neighborhood serving retail, restaurant, and service uses. The area is appropriate for a large retail or mixed use center, with a neighborhood plaza or community gathering space as a focal point and strong pedestrian connections to the Willowbrook/Rosa Parks Station, as well as the educational and medical campuses to the west.

2. Mixed Use 2 (MU-2) Zone. The Mixed Use 2 (MU-2) zone is intended to provide commercial and residential development, with an emphasis on employment-generating uses and residential infill development. The area is appropriate for office, business park, or mixed use developments, with open space components and pedestrian connections to the Willowbrook/Rosa Parks Station, and the educational and medical campuses to the south.

B. MLK Medical Zone and MLK Medical Overlay

The MLK Medical Zone and MLK Medical Overlay are established to meet the existing and future needs of the MLK Medical Center campus, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote medical, clinic, medical office, and associated supportive uses such as incidental retail, supportive residential and parking, and expand pedestrian linkages and connectivity between the MLK Medical Center, CDU, Willowbrook/Rosa Parks Station, and the Willowbrook community.

The MLK Medical Overlay, as identified in Figure 3-1, applies to the two blocks bounded by Wilmington Avenue, East 120th Street, Holmes Street and East 118th Street. The properties within this Overlay are suitable for more intensive uses because of their proximity to Willowbrook/Rosa Parks Station. Besides continuing to allow existing medical and public service uses, additional medical and new residential development are permitted on properties within this Overlay.

C. Drew Educational Zone

The Drew Educational Zone is established to meet the existing and future needs of CDU and King Drew Magnet High School, while ensuring compatibility with adjacent land uses. The intent is to create a medical university campus for CDU by maintaining and promoting educational and associated support uses, while maintaining sensitivity to surrounding development. This zone also seeks expanded pedestrian linkages and connectivity between the CDU campus, MLK Medical Center, Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, and the Willowbrook community.

D. Imperial Commercial Zone

The Imperial Commercial Zone is established to meet the commerce and service needs of the resident and business communities, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote commercial uses between Imperial Highway and the I-105 Freeway. The Imperial Commercial Zone provides for the development of a broad range of retail and service uses, as well as freeway-oriented, regional-serving retail, office complexes, and light manufacturing businesses.

E. Willowbrook Residential 1 Zone

The Willowbrook Residential 1 Zone is established to preserve and enhance desirable characteristics of single-family residential areas.

F. Willowbrook Residential 2 Zone

The purpose of the Willowbrook Residential 2 Zone is to preserve and enhance single-family neighborhood characteristics while also providing opportunities for two-family residences. The intent is to promote desirable characteristics of low to medium density neighborhoods.

G. Willowbrook Residential 3 Zone

The Willowbrook Residential 3 Zone is established to provide opportunities for developments containing multiple units, such as apartments or condominiums. The intent is to promote desirable characteristics of medium density neighborhoods and provide a variety of housing options to serve the needs of the Willowbrook community.

H. Open Space (O-S) Zone

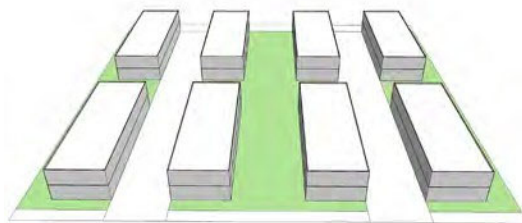
The provisions of Chapter 22.16 apply to all development in the Open Space (O-S) Zone.

3.2 Design Guidelines for Mixed-Use, Commercial and Institutional Uses

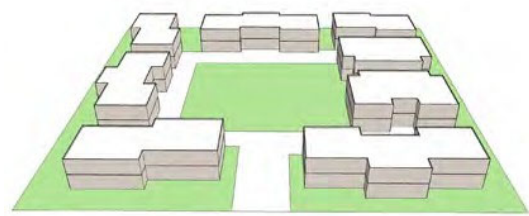
The purpose of this section is to facilitate high quality development for mixed-use, institutional and commercial uses within the Plan Area. These design guidelines address site planning and building design and are in addition to the land use regulations and development standards contained in Chapter 22.412.

1. Building siting and orientation.

- a) New structures should be clustered to create plazas and open space areas and avoid creating “barracks-like” rows. Where clustering is impractical, a visual link between separate structures should be established such as arcade system, trellis or other open structure.



Avoid “barracks-like” layout



Orient buildings to create useable open space

Figure 3-2: Site Layout to Create Usable Space

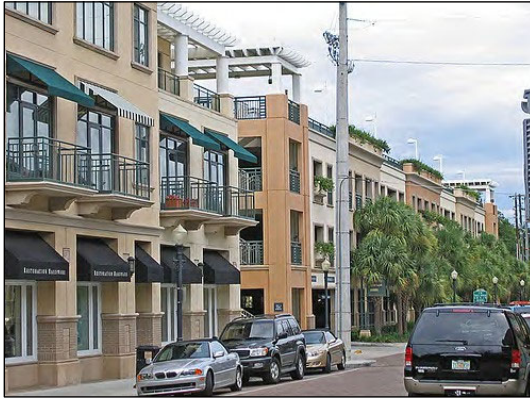


Arcades and trellises provide shade, connections, and define usable space.

- b) Commercial development in mixed-use projects should be oriented to the street, with parking generally located in the rear, side or underground. The perimeter of parking areas and driveways adjacent to streets and sidewalks should be screened with an attractive low wall, berm, fence or landscaping.
- c) Existing residential uses should be buffered from commercial development to mitigate negative impacts such as noise, vibration, shading, light, and glare. Intensified landscaping, increased yard depths and building orientation can improve separation.
- d) Entry and edge design features such as landscaping, architectural signage like monument signs, and/or enhanced paving should be incorporated.

2. Building modulation and detailing.

- a) Building design should avoid large monotonous facades, long straight-line building fronts, plain box shapes, and barren exterior treatments. All building elevations visible from a public right-of-way, including freeways, should be highly articulated and consistently incorporate the chosen design theme.



Varied building forms, volumetric and planar changes, and variations in roof forms and height, contribute to a well-articulated building massing.

- b) Commercial facades of mixed-use projects should be modulated at least every 50 feet by changes in building mass or facade treatment, such as projected entrance windows, roof form or other architectural features.
- c) Building facades should be designed to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; varying color or materials of each individual module within a harmonious palette of colors and materials, etc.
- d) Building form and articulation should accentuate public entrances, de-emphasize service areas, and define and shelter pedestrian walkways and exterior spaces.
- e) Awnings, canopies, and arcades should be used as appropriate to provide visual interest and shade.

Buildings should be treated as a whole and finished appropriately on all sides to provide continuity. The backs of buildings should use similar materials; however, less expensive and more utilitarian substituted materials are acceptable, provided they are compatible with the overall design.

- f) Materials should not change at corners. The same material should continue around corners for a minimum distance of four feet and, if feasible, continue to the next change in the wall plane.

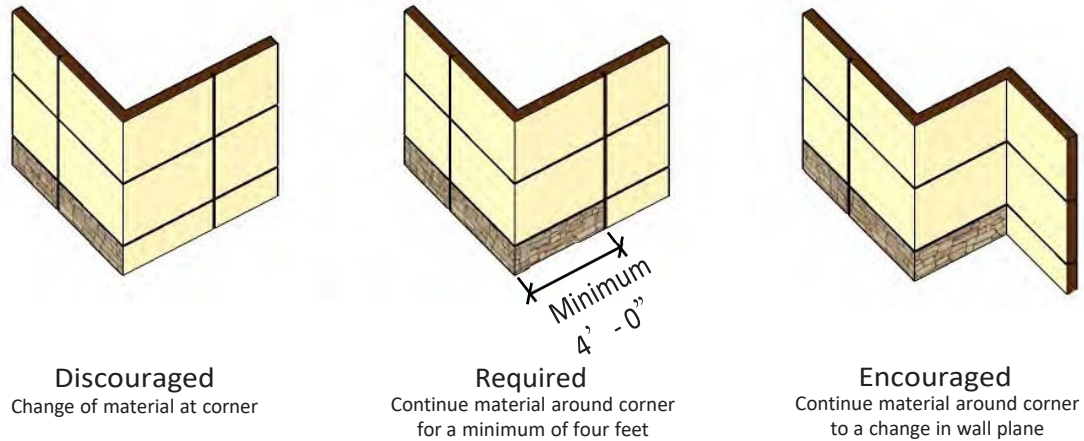


Figure 3-3: Treatments for Material Changes at Corners

3. Building entrances.

- In mixed-use projects, residential and commercial components should have separate entrances.
- The main entrance or entrances should be emphasized and oriented to the street, major plazas, or open space. Main entries to buildings should be clearly demarcated using special materials, color, detailing, or equivalent architectural treatment and accessible from the street and/or pedestrian walkways.
- Secondary entries may be from parking areas.

4. Transparency.

- Placement and orientation of doorways and windows should create a strong, direct relationship with the street. Street-facing façades of all buildings shall incorporate windows and openings providing interior light, create visual interest, and provide “eyes on the street.”
- Windows and openings should provide clear and transparent views into ground floor retail uses which should display merchandise to reinforce a pedestrian scale. Dark tinted, reflective, or opaque glazing should not be applied to any required wall openings of street level retail façades.
- Security doors or gates should not be directly visible when not in use. They should be recessed in the interior walls or ceiling or otherwise built-in to the interior of the space.

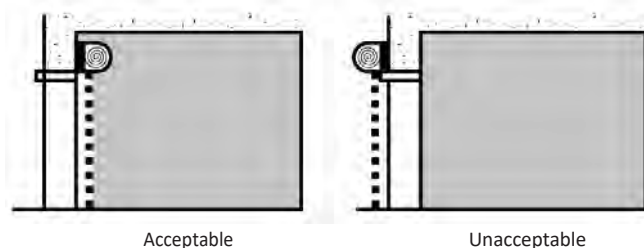


Figure 3-4: Roll-Up Doors

5. Privacy for residential units.

- a) Buildings should be oriented to promote privacy to the greatest extent possible. In mixed use projects, residential windows should face away from loading areas and docks. To the extent residential windows face the windows of an adjacent unit; the windows should be offset to maximize privacy.
- b) Windows, balconies or similar openings should be oriented so as not to have a direct line-of-sight into adjacent units within the development. In addition, units above the first story should be designed so that they do not look directly onto private patios or backyards of adjoining residential property or units.

6.

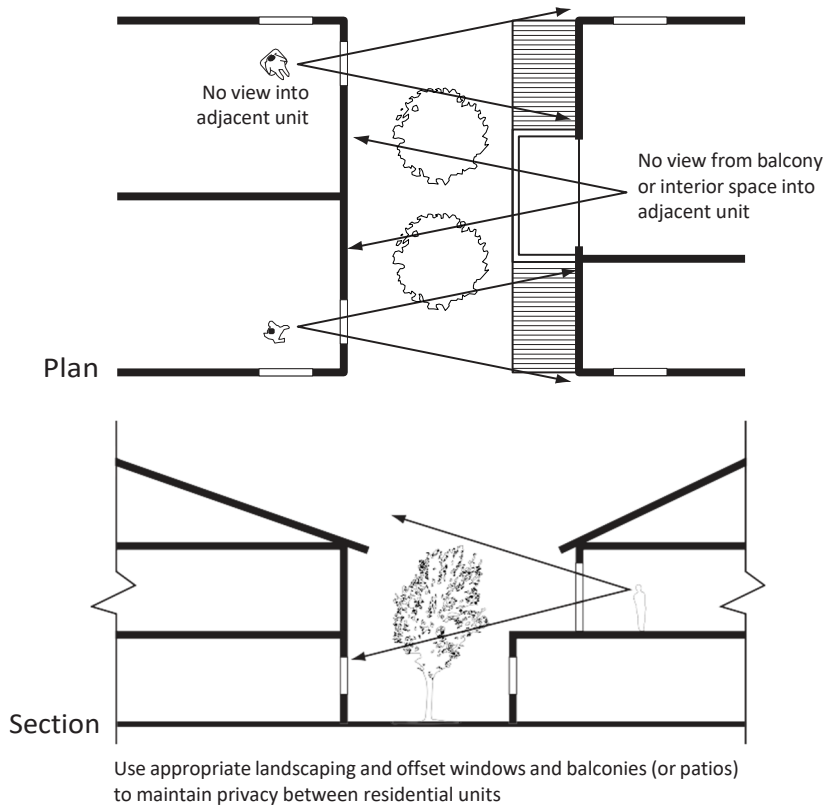


Figure 3-5: Privacy for Residential Units

7. Vehicle circulation and access.

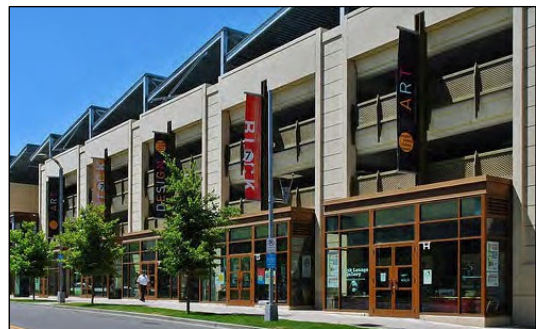
- a) Site access and internal circulation in all developments should be safe, efficient, and convenient. Vehicular traffic should be adequately separated from pedestrian circulation. Vehicular entrances should be clearly identified to minimize pedestrian/vehicle conflict.
- b) The number of site access points or driveway aprons should be minimized for efficiency and to reduce traffic hazards. They should be located as far as possible from street intersections and be coordinated with existing or planned median openings and driveways on the opposite side of the roadway. Common driveways that promote access to more than one site are encouraged.

7. Parking lots.

- a) Parking lots should be designed with a clear hierarchy of circulation: major access drives with no direct access to parking spaces; major circulation drives with little or no parking; and parking aisles for direct access to parking spaces. Loading and service areas should be provided with separate access and circulation whenever possible.
- b) Parking for multi-family developments should be provided on-site in surface or underground structures, parking lots, carports, or attached garages. The visual impact of parking lots should be mitigated with landscaping, walls, or building screening.

8. Parking structures.

- a) The massing, height, and architectural design of a parking structure should be consistent and compatible with the rest of the development.
- a) Vehicular access to parking structures should not obstruct the free flow of traffic on public streets and entrances and exits should be located to reduce traffic conflicts.
- b) Pedestrian access to a parking structure should avoid pedestrian/vehicle conflicts.



Parking structures should be designed to integrate with the area they serve. Well-designed structures, ground-level storefronts, and enhanced landscape treatments can minimize the visual impact of parking structures.

8. Pedestrian circulation.

- a) All developments should promote pedestrian walkability and connectivity both onsite and offsite. Pedestrian walkways should be safe, visually attractive, and adequately separated from vehicular traffic.
- b) Pedestrian walkways should link residential with commercial or institutional uses in a mixed-use or institutional development as well as with common open space, plazas and courtyards, and public sidewalks.
- c) Where possible, multi-family developments should incorporate pedestrian connections to adjoining residential and commercial projects.
- d) Pedestrian walkways that cross parking areas or driveways should have decorative paving or some other method to delineate pedestrian space.
- e) Pedestrian walkways to building entrances should be paved with high quality paving materials like stone pavers, concrete pavers, or brick and well-defined by landscaping.

9. Residential open space.

- a) Common open space areas should be conveniently located, separated from the non-residential component in mixed-use developments and exclusively available for residents.
- b) Common open space areas should contain amenities appropriate to the project's size for example, pools, spas, recreation buildings are encouraged for larger projects while barbeque areas and gazebos may be more appropriate for smaller projects. Children's play areas should be visible from residential units and not visible from the public right-of-way.



Play areas can be incorporated into the common areas.



Pools, spas, and other recreational amenities are encouraged for larger projects.

- c) Rooftop open space accessible to all residents may count toward required common open space or it may count toward required private open space if it is directly accessible from the unit(s).

10. Fences, walls, gates, and hedges.

Walls and fences and other boundary elements should be designed as integral parts of projects for screening and security. Decorative fencing adjacent to public rights of way, such as wrought iron, are encouraged.

11. Exterior lighting.

- a) Exterior lighting should provide security and safety and visibility of areas such as building entrances, parking, loading and service areas, walkways, driveways, and open space areas.
- b) The design of light fixtures and their structural support should be architecturally compatible.
- c) Exterior lighting should not impact neighboring properties. All lighting should be shielded to confine light spread on-site.
- d) Exterior lighting in mixed-use projects should not adversely impact the residential portion of the development.

12. Outdoor Storage, Loading and Service areas.

- a) Areas for service entrances, loading docks and bays, or trash and recycling collection facilities should be clearly defined. They should not be visible from public rights-of-way and should be screened from view.
- b) Loading and service areas should not interfere with vehicular or pedestrian circulation and parking.
- c) Loading areas should be away from main entrances and the street and preferably located toward the rear of the property to minimize odors and noise affecting residential units or adjacent residential uses.
- d) All outdoor storage, loading, and service areas should be located to minimize odors and noise affecting residential units or adjacent residential uses.
- e) Outdoor storage, loading and service areas should not be visible from public rights-of-way and should be screened from public view.

3.3 Design Guidelines for Multi-Family Residential Uses

The purpose of this section is to facilitate high quality multi-family residential development within the Plan Area. These design guidelines address site planning and building design and are in addition to the above land use regulations and development standards for multi-family uses.

1. Scale and mass.

- a) Multi-family residential projects should avoid having a massive "big box" appearance. Multi-family units in large projects should be clustered or broken up into groups of structures. Units facing the street should have windows to break up massing and to contribute to "eyes on the street" for neighborhood security.

2. Building orientation and entrances.

- a) Multi-family developments should have a strong street presence with public entrances and unit entrances oriented towards the street.
- b) Multi-family developments should have transitional spaces in the form of stoops, overhangs and porches between public areas and entrances to the units.

3. Building articulation and detailing.

- a) Multi-family building design should avoid large monotonous façades, long straight line building fronts, plain box shapes, and barren exterior treatment. All building elevations visible from a public right-of-way should be articulated and incorporate the chosen design theme in a consistent manner.
- b) Multi-family building facades should be designed so as to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; and varying color or materials of each individual module within a harmonious palette of colors and materials.



Articulate buildings with architectural detailing, staggered building planes, material and color changes, and varied roof lines.

4. Privacy for residences.

- a) Multi-family buildings should be oriented to promote privacy to the greatest extent possible. Openings above the first story such as windows and balconies should be oriented to not have a direct line-of-sight into adjacent units within the development.
- b) Multi-family units above the first story should not look directly onto private patios or backyards of adjoining residential property or units. Landscaping should be used to aid in privacy screening in ground floor units.

5. Vehicle circulation and access.

- a) Site access and internal circulation in multi-family developments should be safe, efficient, and convenient. Vehicular traffic should be adequately separated from pedestrian circulation. Vehicular entrances should be clearly identified to minimize conflict.
- b) The number of site access points or driveway aprons should be minimized to eliminate traffic hazards. They should be located as far as possible from street intersections and be coordinated with existing or planned median openings and driveways on the opposite side of the roadway.

6. Parking.

Parking for multi-family developments should be provided on-site in on-grade or underground structures, parking courts, carports, or attached garages. Adverse visual impacts should be minimized through proper siting and design.

7. Pedestrian circulation.

- a) Pedestrian circulation should be adequately separated from vehicular traffic. Pedestrian entrances and walkways should be clearly identified and easily accessible to minimize conflict.
- b) Pedestrian walkways should link dwelling units with common open space and recreation areas, parking areas, and the street at the project entries. Where possible, multi-family developments should connect to adjoining residential and commercial projects.
- c) Pedestrian walkways should be safe, visually attractive, and well defined by landscaping and lights.

8. Exterior lighting.

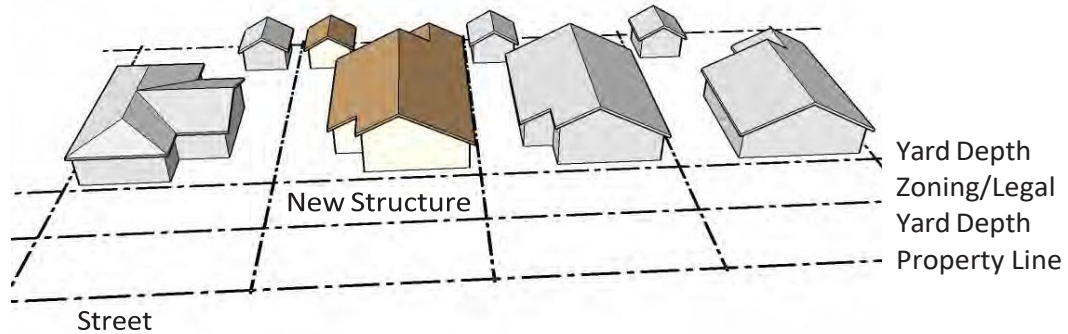
- a) Every multi-family project should have adequate lighting for security and visibility, particularly along walkways and driveways, at entrances to parking areas, and within open space areas.
- b) Site lighting should not impact neighboring properties and all lighting fixtures should be shielded to confine light spread on-site.
- c) The design of all exterior light fixtures should be compatible with the building's architecture.

3.4 Design Guidelines for Single-Family Residential Uses

The purpose of this section is to facilitate high quality single-family residential development within the Willowbrook Residential 1 and Willowbrook Residential 2 Zones. These design guidelines address site planning and building design and are in addition to the land use regulations and development standards in Chapter 22.412.

1. Yard areas.

A new dwelling's front and side yard depths should approximate that of adjacent residences.



New structure respects the yard depths established along a street

Figure 3-6: Yard Depths

2. Scale and mass.

The scale and mass of a new dwelling should be similar to that of neighboring houses

3. Additions and accessory buildings.

Additions and accessory structures should maintain and complement the look and appearance of the existing primary structure and maintain the same floor-to-floor height of the original structure.

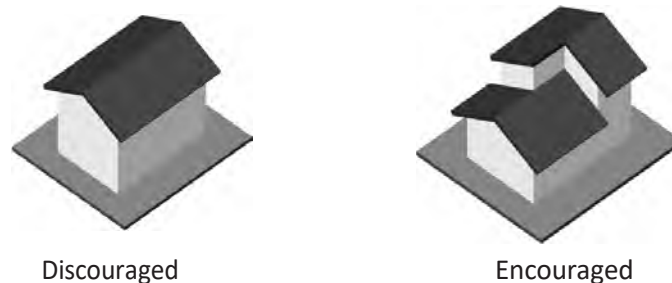


Figure 3-7: Massing

4. Orientation.

Dwellings should be oriented to the street with a clearly identifiable front door and windows to put “eyes on the street.”



Discouraged



Encouraged

Orient the front of the house and entry to face the street

Figure 3-8: Orientation

5. Privacy for residences.

New houses and/or accessory buildings should maintain privacy by staggering the placement of new windows to the opposing neighbor's windows and avoid locating balconies that directly overlook a neighboring patio or yard.

6. Exterior lighting.

Exterior lighting includes all lighting fixtures on front façades, all security lighting, and all landscape lighting. Adequate exterior lighting should be provided on the front of the house to contribute toward neighborhood safety and security at night. Recessed porches should be lit. Exterior lighting should be shielded and positioned to confine light spread on-site and not into neighboring properties.

3.5 Sustainable Design Guidelines

Building green means reducing the use of resources, minimizing harmful impacts to the environment, and creating healthier environments for people. Green buildings can incorporate both passive, low-tech design, such as daylighting; and active, high-tech strategies and systems, such as photovoltaic panels. To be most effective, green building strategies should be incorporated into all phases of a project.

1. Site Design and Passive Solar Design

- Buildings should be oriented and designed to maximize the use of sunlight and shade for energy savings without compromising the solar access of neighbors.
- Buildings should be clustered for shade, and incorporate features like protected courtyards and recessed windows and doors.
- East and west walls of buildings should be shaded with evergreen trees to reduce summer heat gain. South walls should be shaded with deciduous trees to increase winter heat gain.

2. Water Efficiency

- Walkways and plazas should be designed to collect stormwater so it can be retained on-site.
- Plant materials should be drought tolerant and low-maintenance.



Landscape materials should be drought-tolerant and low-maintenance. Pervious, non-vegetative ground cover interspersed with native plantings can provide a sustainable, attractive landscaped area.

3. Building Design

- Green roofs should be considered to reduce solar gain and retain stormwater on-site.
- Solar panels on roofs should be considered to generate power on-site and provide energy savings.
- Arcades, covered walkways, and trellises should be incorporated to provide pedestrian shade and reduce building energy use.



Green roofs serve several purposes for a building, such as absorbing rainwater, providing insulation, and helping to lower urban air temperatures and mitigate the heat island effect. Parking structures and other buildings offer potential for solar roofs, which capture sunlight and transform it into energy.

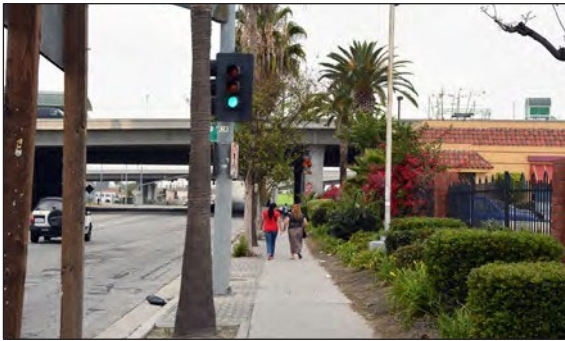
3.6 Street and Sidewalk Guidelines

The Plan Area has a mix of commercial, institutional, and residential uses. While both paved sidewalks and sidewalks with parkways can be found within the area, this Specific Plan differentiates the sidewalk treatment between residential and non-residential frontages as follows:

- All existing parkways fronting residential uses should be retained.
- All other street segments (with institutional, commercial or mixed-use zone frontages) should have paved sidewalks instead of parkways.



Existing parkways along residential frontages include turf and drought tolerant landscaping.



Existing sidewalks along non-residential parkways have a mix of paving materials.

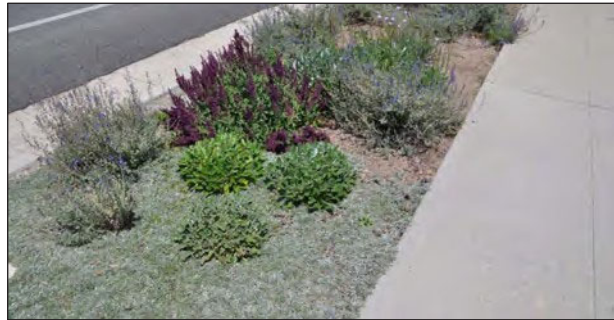
Each property owner is responsible for the maintenance of the parkway that fronts their lot. Low-maintenance drought-tolerant landscaping is encouraged in residential parkways.



Sidewalks along non-residential frontages should have attractive paving materials and patterns. Tree wells can also become a part of a Low Impact Development strategy.

As new development occurs in the Mixed-Use Zones, the adjoining property owner/developer should apply the following guidelines for upgrading the public right of way. Property owners in other areas are also encouraged to follow these standards and guidelines.

- New sidewalks should be graded to allow stormwater run-off into adjacent unpaved areas, such as planter strips and parkways.
- Sidewalks should be designed so that street furniture, signage, lighting, landscaping, etc. do not interfere with a continuous pedestrian path.
- Decorative non-vegetative groundcover, such as river cobble, pebbles, and crushed stone, that allow proper drainage are encouraged. Any material, such as crushed tires, that may leach harmful chemicals into the soil should be avoided.



Drought-tolerant landscaping in the parkways is an attractive replacement for turf.

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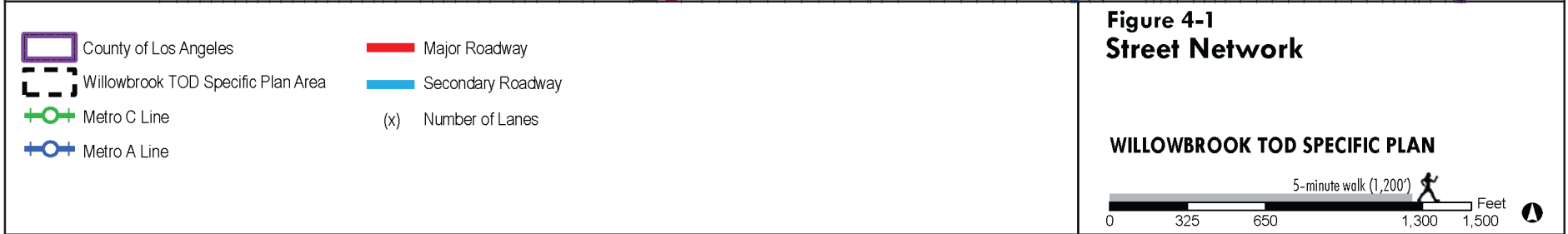
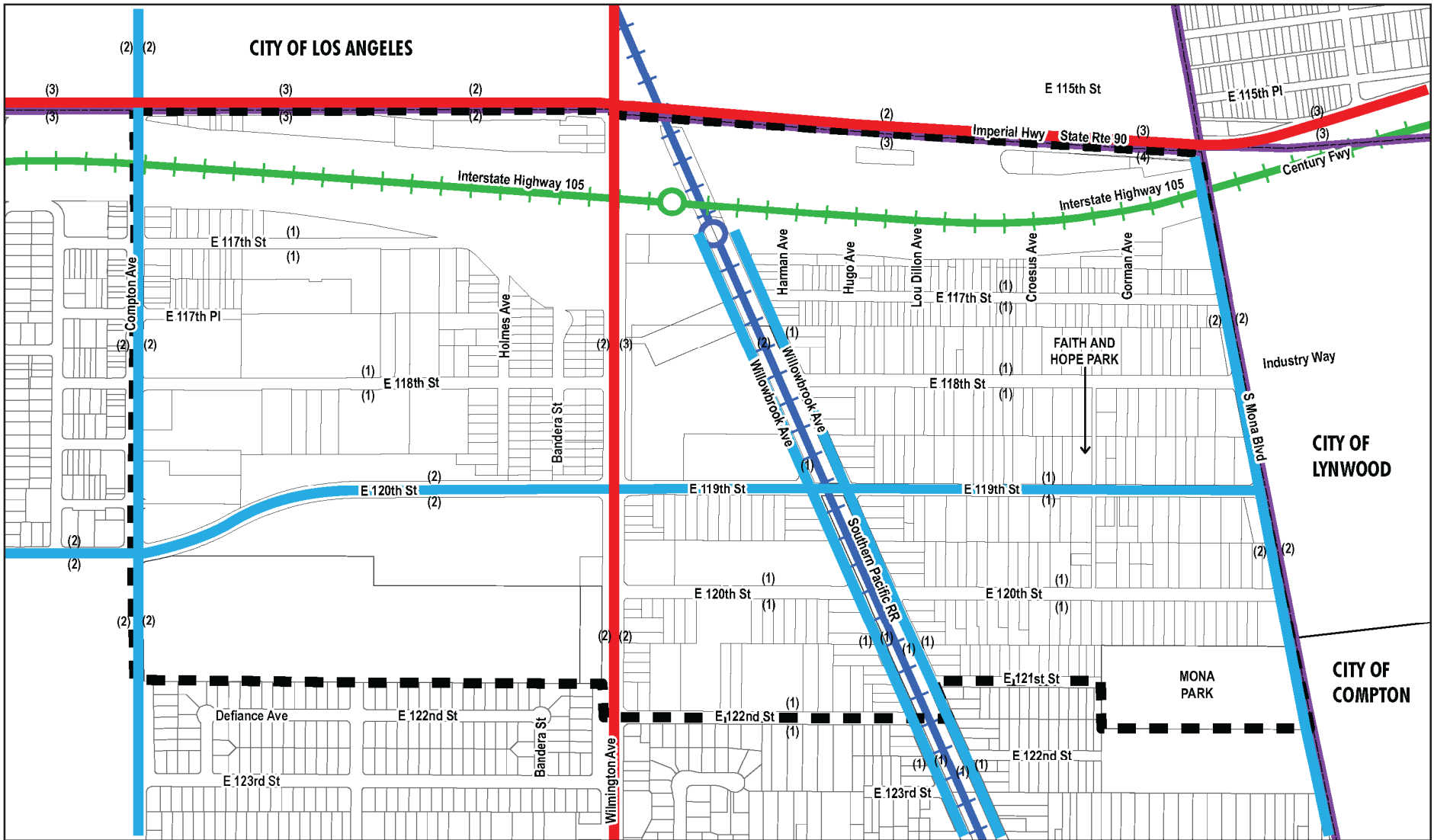
4.1 Introduction

This Chapter describes the Mobility Strategy for the Willowbrook TOD Specific Plan. It includes recommendations for the roadway network as well for improving bicycle, pedestrian, and transit circulation. The overall goal is to enhance connectivity and encourage active transportation.

4.2 Background

The Plan Area is centered on the Willowbrook/Rosa Parks Station, a transfer station for both the Metro A Line and the Metro C Line. The existing circulation system in the area is automobile-oriented and dominated. Even with numerous shuttles and bus routes serving the Plan Area and connecting to the Metro Station, the Metro station is generally inhospitable and difficult to access for pedestrians.

Community members expressed safety concerns over high speed traffic on key roadways and the poor pedestrian environment at most intersections and on many of the sidewalks. There are few locations to safely cross streets and many intersections are without crosswalks. The east side of Wilmington Avenue near the Willowbrook/Rosa Parks Station is particularly inhospitable. Some areas have unpaved pathways along streets and most areas have inadequate lighting. Furthermore, the lack of bicycle infrastructure including bike lanes and bike parking make riding bicycles inconvenient and less safe.



4.3 Roadway Network

The roadway system, shown on Figure 4-1, is the basis for all modes of transportation. While historically oriented to automobiles, "Complete Streets" also accommodate the safe and efficient circulation of transit, bicycles, and pedestrians. The main streets in the Plan Area are Wilmington Avenue and Imperial Highway. Secondary streets are Compton Avenue, Willowbrook Avenue and Mona Boulevard in the north-south direction, and 120th Street/119th Street in the east-west direction. The existing street system will be largely maintained with some new safety, access, circulation, and walkability improvements shown in Figure 4-2 and described in more detail in Sections 4.4 and 4.5.

Road Diet and Bicycle Lanes on 120th Street

The section of 120th Street between Compton Avenue and Wilmington Avenue will be reduced from four traffic lanes to three lanes and on-street parking removed to accommodate a bicycle lane in each direction as part of the Willowbrook Area Access Improvement Project.

Willowbrook Avenue West

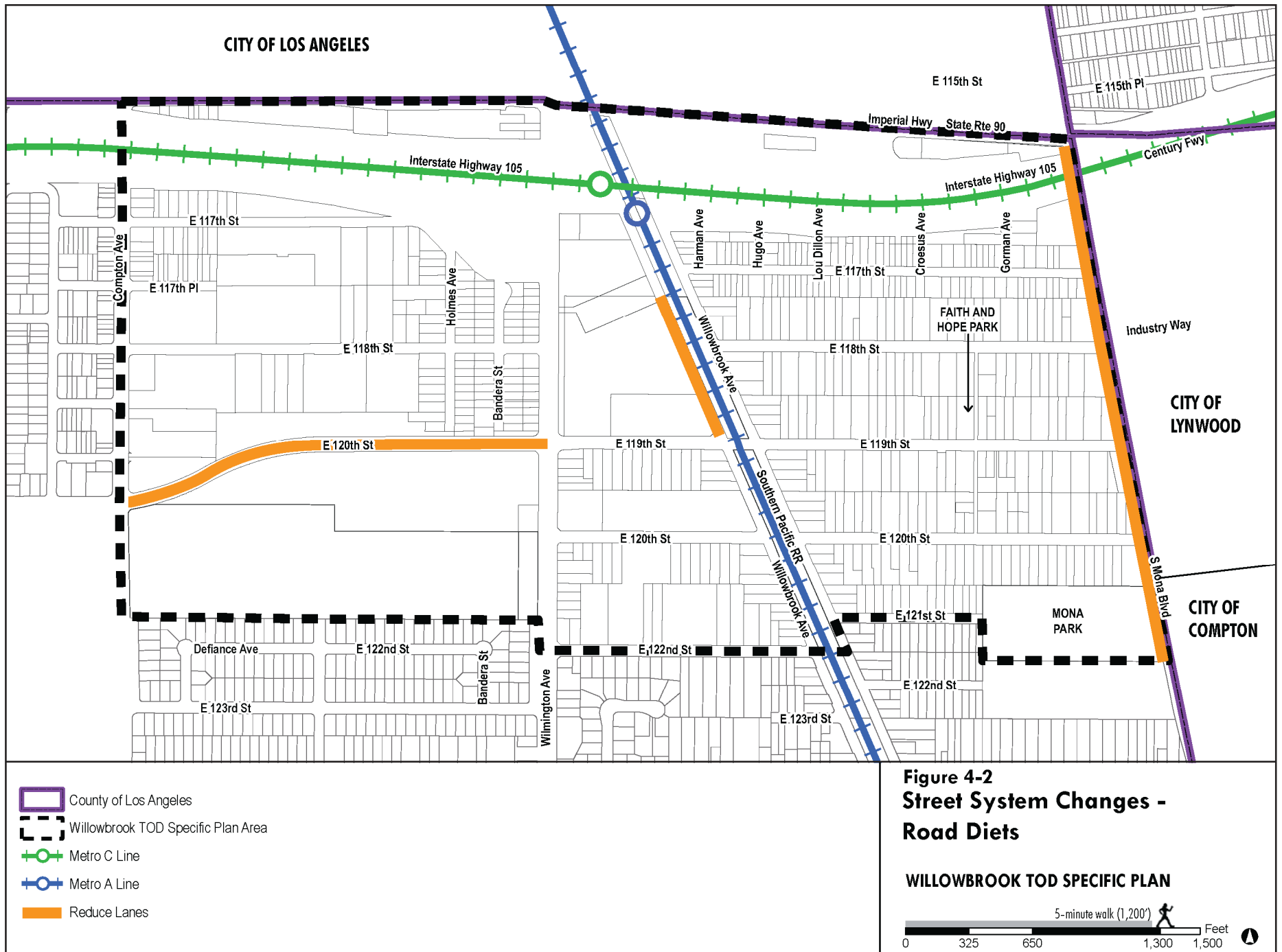
The southbound section of Willowbrook Avenue between the Willowbrook/Rosa Parks Station and 119th Street will be reduced from two lanes to a bicycle path installed on the west side of the street and a one lane dedicated busway.

Road Diet and Bicycle/Pedestrian Trail on Mona Boulevard

Mona Boulevard from the I-105 Freeway to 124th Street will be reduced from four lanes to three to accommodate a paved, Class I pedestrian/bicycle trail on the west side of the street. On-street parking will remain. Most properties on the west side of Mona Boulevard take their access from the east-west streets. There are twelve existing driveways and curb cuts that the trail will have to safely accommodate. Figure 4-3 illustrates a typical cross section of reconfigured Mona Boulevard.

Rename 120th Street between Compton and Wilmington Avenues

Currently, the segment of 120th Street between Compton and Wilmington Avenues confusingly aligns with 119th Street east of Wilmington Avenue. This segment fronting the MLK Medical Center should be renamed via a community contest similar to the one that named "Healthy Way" on the MLK Medical Center campus.



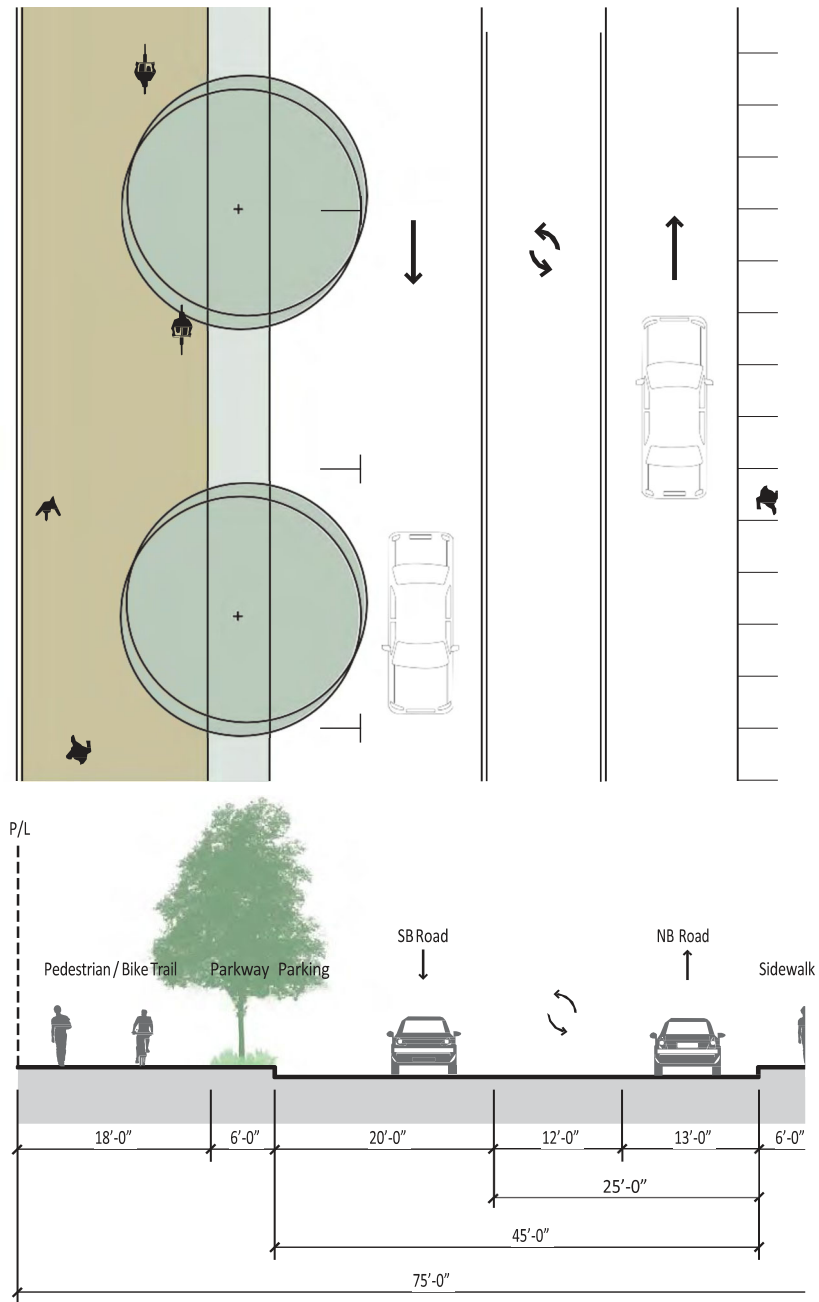
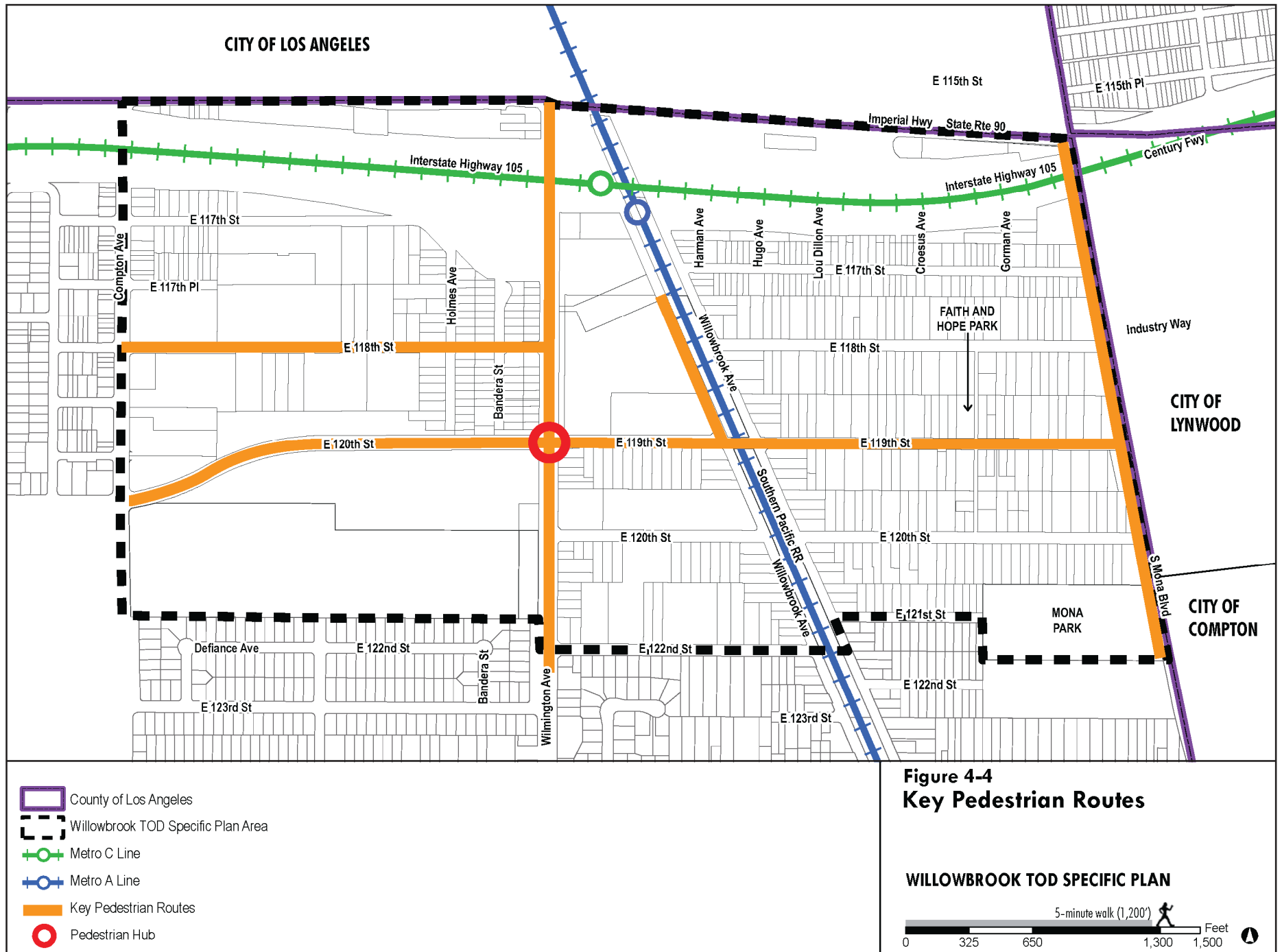


Figure 4-3: Proposed Mona Boulevard Section



Examples of a Class I bicycle path and pedestrian trail



4.4 Pedestrian Circulation

The key pedestrian routes in the Plan Area are Wilmington Avenue in the north-south direction and 120th/119th Street in the east-west direction shown in Figure 4-4. These corridors connect activity centers of the Willowbrook/Rosa Parks Station, the Kenneth Hahn Plaza, and the MLK Medical Center. Their intersection is the functional pedestrian hub of the Plan Area. Section 5.2C in Chapter 5 discusses and illustrates concepts for special treatment for this intersection.

There are four additional key routes. East-west 118th Street between Compton Avenue and Wilmington Avenue connects the CDU campus to activity centers. Willowbrook Avenue West between 119th Street and the Willowbrook/Rosa Parks Station connects residential areas to the Metro station. East-west 119th Street between Willowbrook Avenue and Mona Boulevard connects residential areas to activity centers. And finally, Mona Boulevard connects to Mona Park, MLK Elementary School, and Dr. Ralph Bunche Middle School on the east side of the plan area.

Pedestrian Sidewalk Improvements

Sidewalks currently exist on most streets although some are narrow or substandard. Sidewalks will be improved as new development occurs. Specific recommendations include:

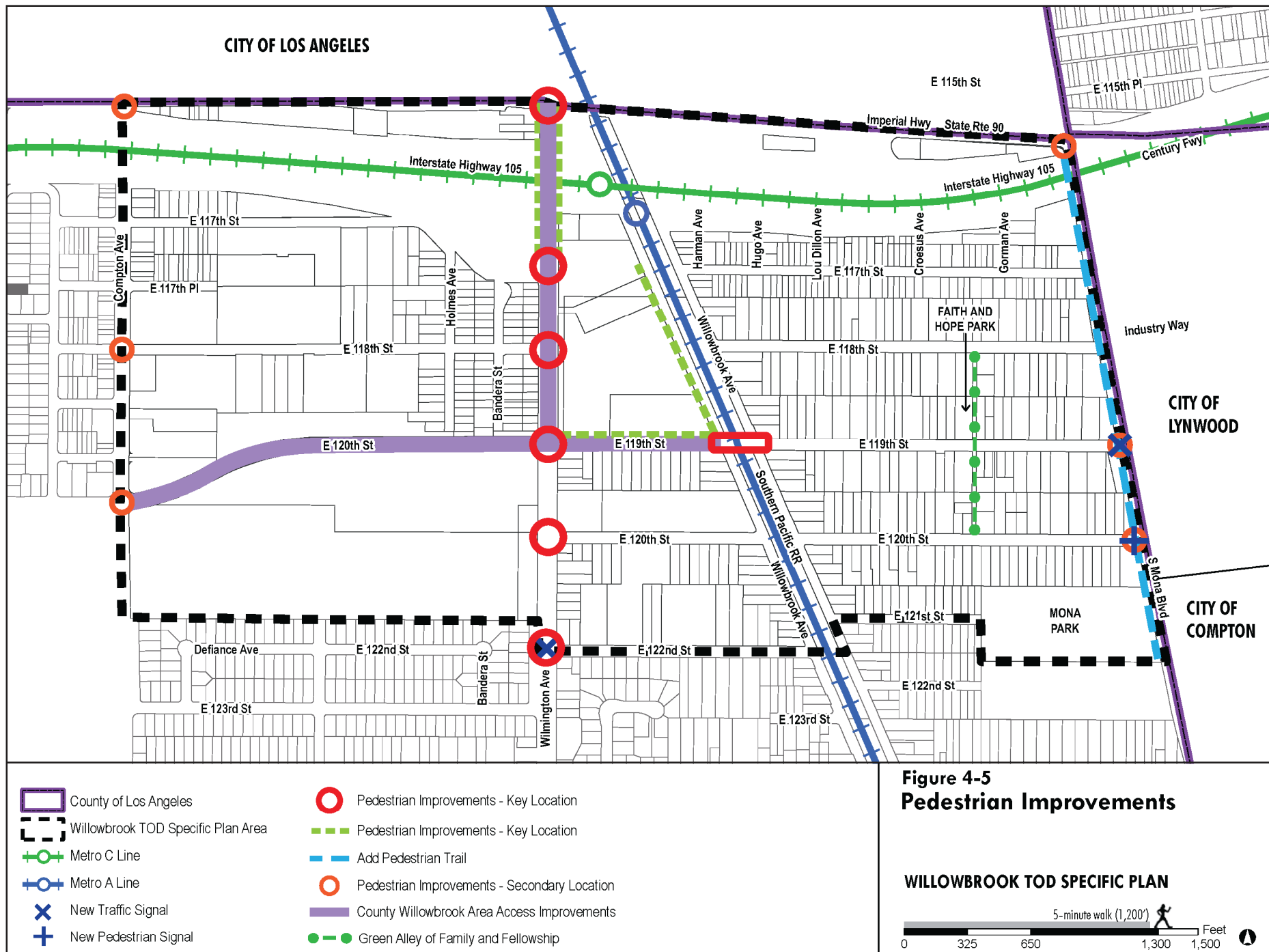
- Adding a sidewalk to the currently unpaved west side of Willowbrook Avenue West between the Willowbrook/Rosa Parks Station and 119th Street.
- Improving the sidewalks on Wilmington Avenue between the I-105 Freeway Off-Ramps and Imperial Highway through widening and streetscape improvements including pedestrian-scaled street lighting.
- Widening sidewalks where feasible as new development occurs for example, along the north side of 119th Street between Wilmington and Willowbrook Avenues.

Pedestrian Oriented Intersection Improvements

A number of pedestrian-oriented and traffic calming intersection improvements will be implemented throughout the Plan Area based on a menu that includes the following:

- Adding high visibility crosswalks at both existing and new intersections recommended in this Specific Plan.¹
- Adding passive pedestrian detection and pedestrian push buttons for crosswalks at traffic signals at intersections.
- Adding pedestrian audio and countdown signals to crosswalks at intersections.
- Adding advance stop lines to signalized intersection approaches.
- Adding sidewalk bulb-outs and extensions on intersection corners where feasible.

¹ Design of any high visibility crosswalks should be reviewed on a case-by-case basis, and consider ADA access, maintenance cost, and feasibility. Pavers or textured surfaces should not be considered in the pedestrian walking path of the crosswalks.



Bulb outs and curb extensions provide additional sidewalk space and reduce the width of the roadway pedestrians have to cross while making pedestrian crossings more visible. Recommended locations are shown in Figure 4-5 and a typical diagram is illustrated in Figure 4-6.

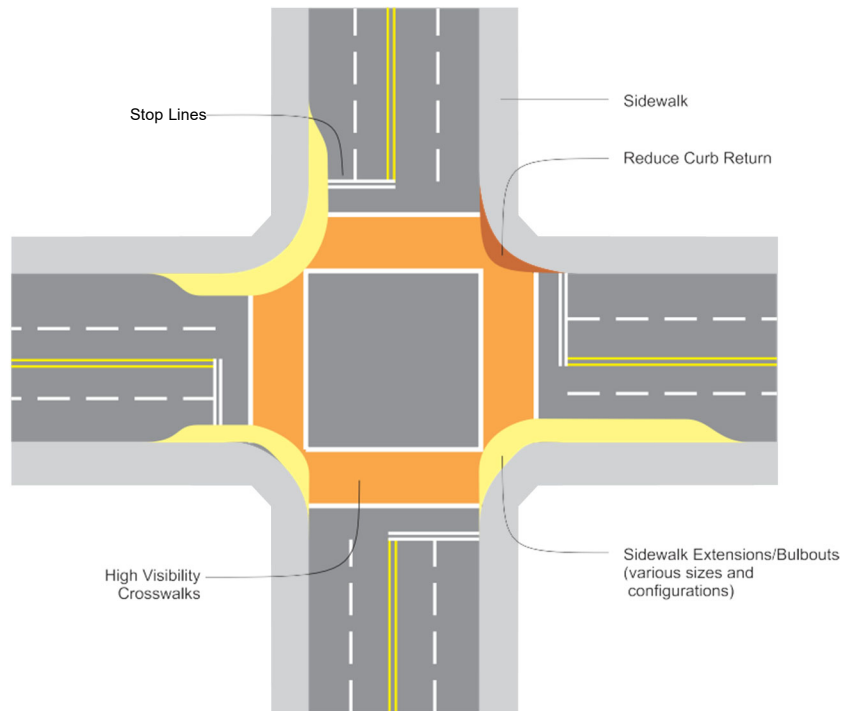
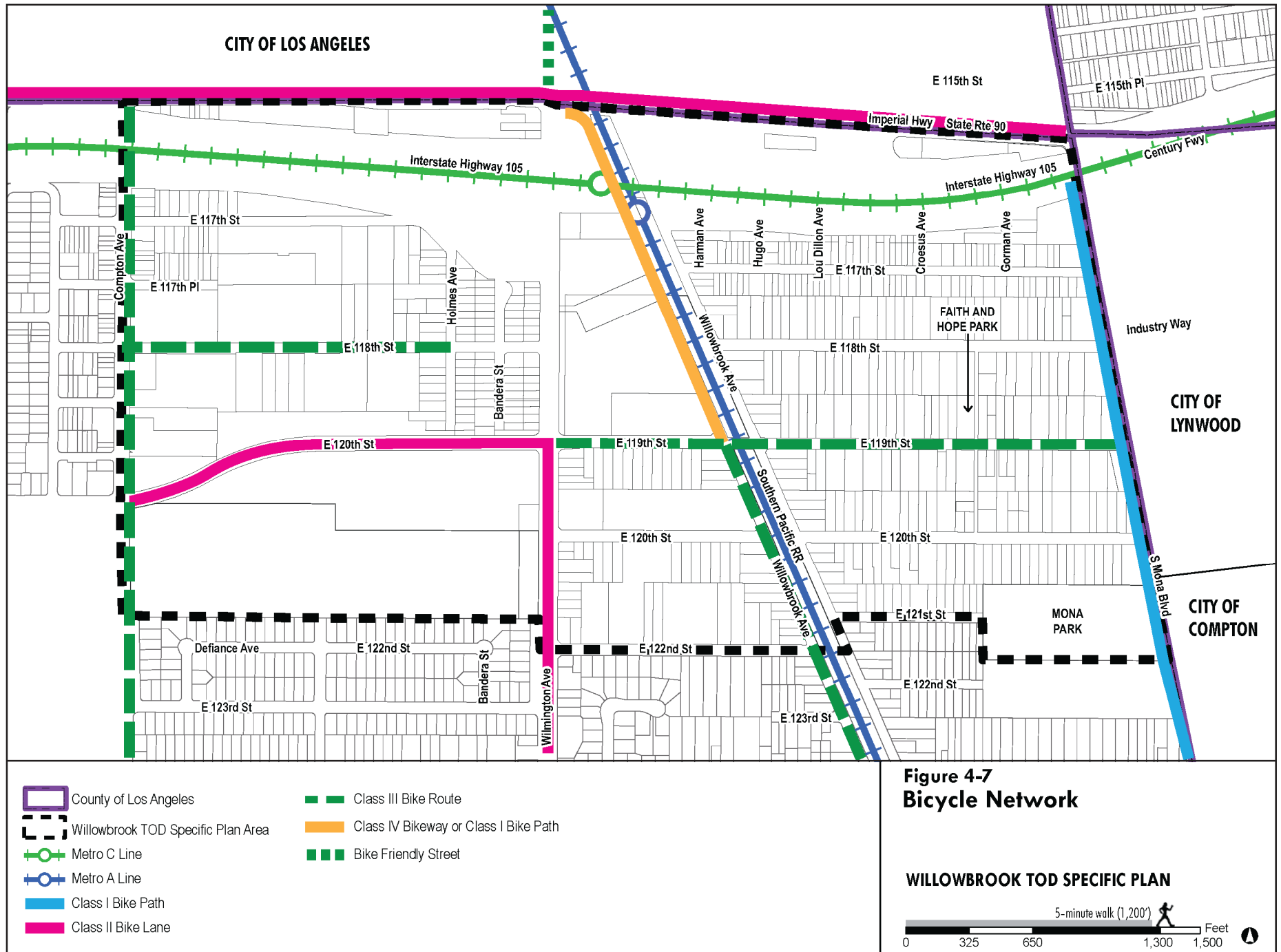


Figure 4-6: Pedestrian-oriented intersection improvements

The type of improvements will follow the concepts identified in the “Los Angeles County Transit-Oriented Districts Access Study, SCAG and County of Los Angeles.” The specific improvements to be implemented at each location will be determined following detailed design studies to determine applicability and feasibility and the ultimate configuration.

The improvements at Wilmington Avenue and I-105 eastbound ramp will add a crosswalk across Wilmington Avenue for access to the Willowbrook/Rosa Parks Station. Other improvements being considered by DPW include:

- New traffic signals at Wilmington Avenue and 122nd Street and at Mona Boulevard and 119th Street.
- A signalized pedestrian crosswalk on Mona Boulevard and 120th Street for pedestrians crossing to Dr. Ralph Bunche Middle School.



4.5 Bicycle Circulation

The Specific Plan Bicycle Network, shown in Figure 4-7, includes a combination of Class I, Class II, and Class III bicycle facilities that connect activity centers and neighborhoods to the Willowbrook/Rosa Parks Station, connect to adjacent communities, and provide a dedicated network for bicyclists to use safely and efficiently. The Bicycle Circulation System builds on the County's Bicycle Master Plan.

Bicycle Facilities

- A Class I Bicycle Path consists of an exclusive bicycle facility separated from roadways and traffic. A cycle track provides a right-of-way designated exclusively for bicycle travel within or adjacent to a roadway and protected from vehicular traffic. Some types of separation include grade separation, flexible posts, inflexible physical barriers, or on-street parking.
- A Class II Bicycle Lane consists of a designated striped lane along the curb lane of a street. It provides for one-way travel and is generally delineated with special striping and signage.
- A Class III Bicycle Route is for shared use with automobile traffic. Signs are posted, which indicate the road also serves as a bicycle route, but no special bicycle lane is striped. Symbols known as "sharrows" may also be painted on the roadway surface to indicate that bicycles also use the street.

Recommendations include:

- Class I Bicycle Paths or potentially a cycle track, on Willowbrook Avenue West between 119th Street and Imperial Highway and on Mona Boulevard (west side) between Imperial Highway and 124th Street.
- Class II Bicycle Lanes on 120th Street between Compton Avenue and Wilmington Avenue, on Wilmington Avenue between 124th Street and 120th Street, and on Imperial Highway between Compton Avenue and Mona Boulevard.
- Class III Bicycle Routes on Compton Avenue, Willowbrook Avenue West south of 119th Street, and 119th Street between Wilmington Avenue and Mona Boulevard

Bicycle Amenities

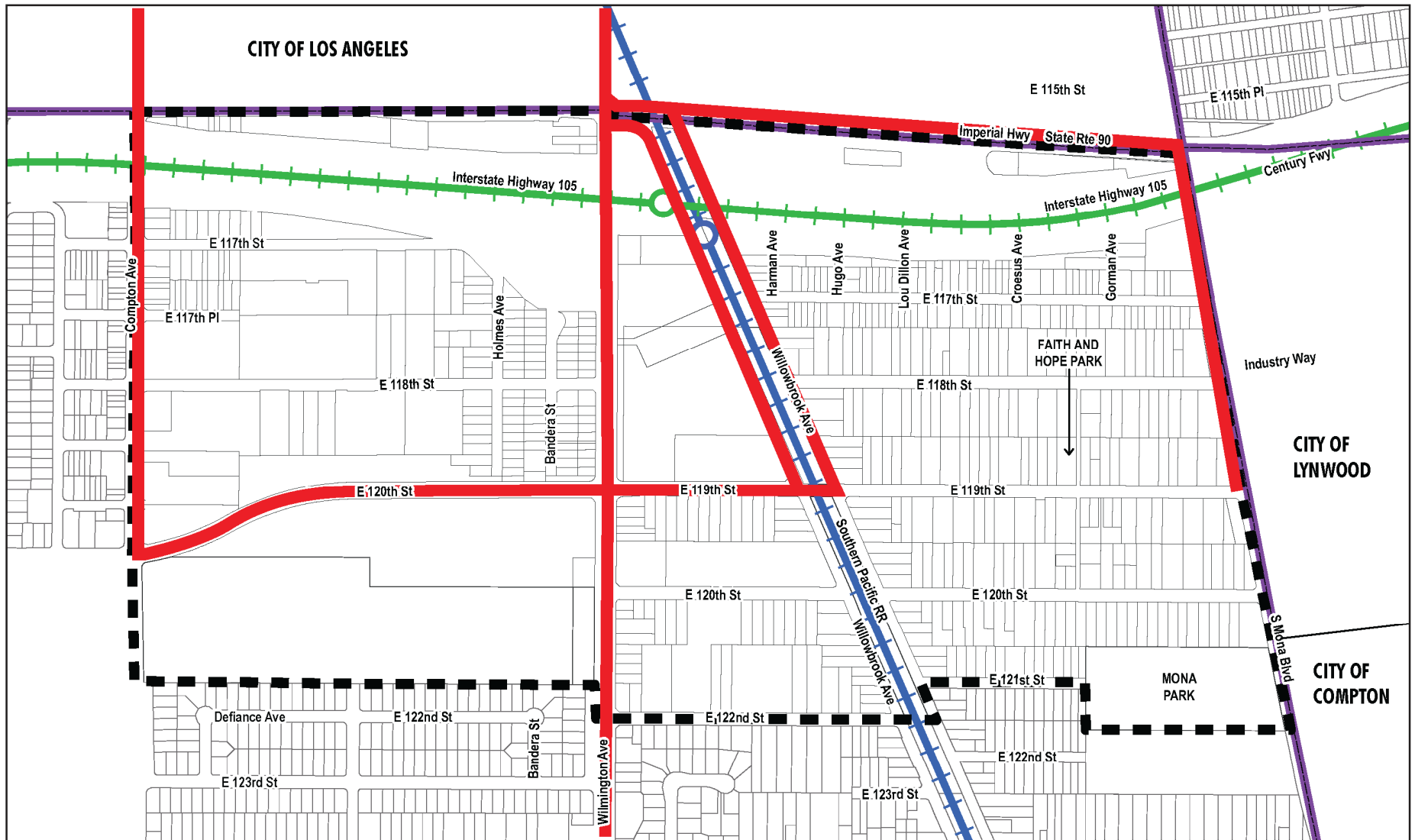
Bicycle parking can include bicycle racks and lockers, bicycle corrals (larger areas dedicated to bicycle parking), and bicycle stations. Bicycle stations can include repair facilities and a small bicycle shop as well as secure bicycle parking. Bike share programs provide bicycles that can be rented for a period of time and picked up and dropped off at any bicycle share stop in an area.

Bicycle Parking and Stations

Bicycle parking should be provided according to Title 22 for all new developments. Additional bicycle parking should be provided at Kenneth Hahn Plaza and the Willowbrook/Rosa Parks Station. Metro is planning to provide a bicycle station at the Metro station called a Metro Bike Hub. Bicycle stations and additional bike parking may also be appropriate on the MLK Medical Center Campus, the CDU Campus, and in the Northwest Subarea and should be provided by those facilities and uses.

Bike Share Program

A bike share program should be implemented in the Plan Area to encourage bicycling and improve the first/last mile connections to the Willowbrook/Rosa Parks Station. A program could be coordinated with neighboring jurisdictions.



**Figure 4-8
Key Transit Streets**

WILLOWBROOK TOD SPECIFIC PLAN



-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro C Line
-  Metro A Line
-  Transit Streets (Multiple Routes)

4.6 Transit Circulation

Station Access

Metro is currently working on improvement plans for the Willowbrook/Rosa Parks Station. These are expected to include enhancements to the station facilities and platforms, a shuttle stop and bicycle station, and access improvements including a pedestrian railroad track crossing to neighborhoods to the east.

Transit Service

Key transit streets that offer multiple bus routes in the Plan Area are shown in Figure 4-8. Current bus routes are expected to continue to serve the Plan Area focusing on the Willowbrook/Rosa Parks Station. To encourage transit use, bus stops with benches, shade, landscaping and transit service information should be located at quarter mile spacing.

Shuttle Routes

Existing shuttle routes that are operated by the County and CDU should continue to provide critical access for those without a car. Shuttle providers should also extend service to accommodate second-shift hospital workers.

Additional shuttle routes should be added in the Northwest Subarea and connect to the Willowbrook/Rosa Parks Station. New shuttle services could be provided by the private sector as part of a comprehensive Transportation Demand Management Program.

4.7 Transportation Demand Management Program

A Transportation Demand Management (TDM) Program should be included in project development to implement strategies to take advantage of the high level of transit service and to reduce both vehicle trips and demand for vehicle parking. Strategies may include subsidizing transit passes, parking cash out programs, preferential parking for carpools, and facilitating carpools and vanpools. Such programs encourage the use of transit, bicycling, walking, and ride sharing and are generally most suitable for large employers and institutional uses.

TDM strategies could be particularly attractive to employers in new office type land uses in the Northwest Subarea by reducing capital costs associated to building parking. They could also appeal to CDU and the MLK Medical Center. A Transportation Management Organization (TMO) could also be established to facilitate these programs at an area-wide level and support individual participating employers and/or buildings.

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Streetscape Improvements

5.1 Introduction

This chapter describes recommended streetscape improvements to improve the quality of life for residents. Streetscape improvements enhance and unify the visual and spatial experience of bicyclists, pedestrians, and drivers and help provide key linkages between the activity centers and neighborhoods. Some recommendations are conceptual and should be evaluated on a case-by-case basis.

5.2 Streetscape Improvements

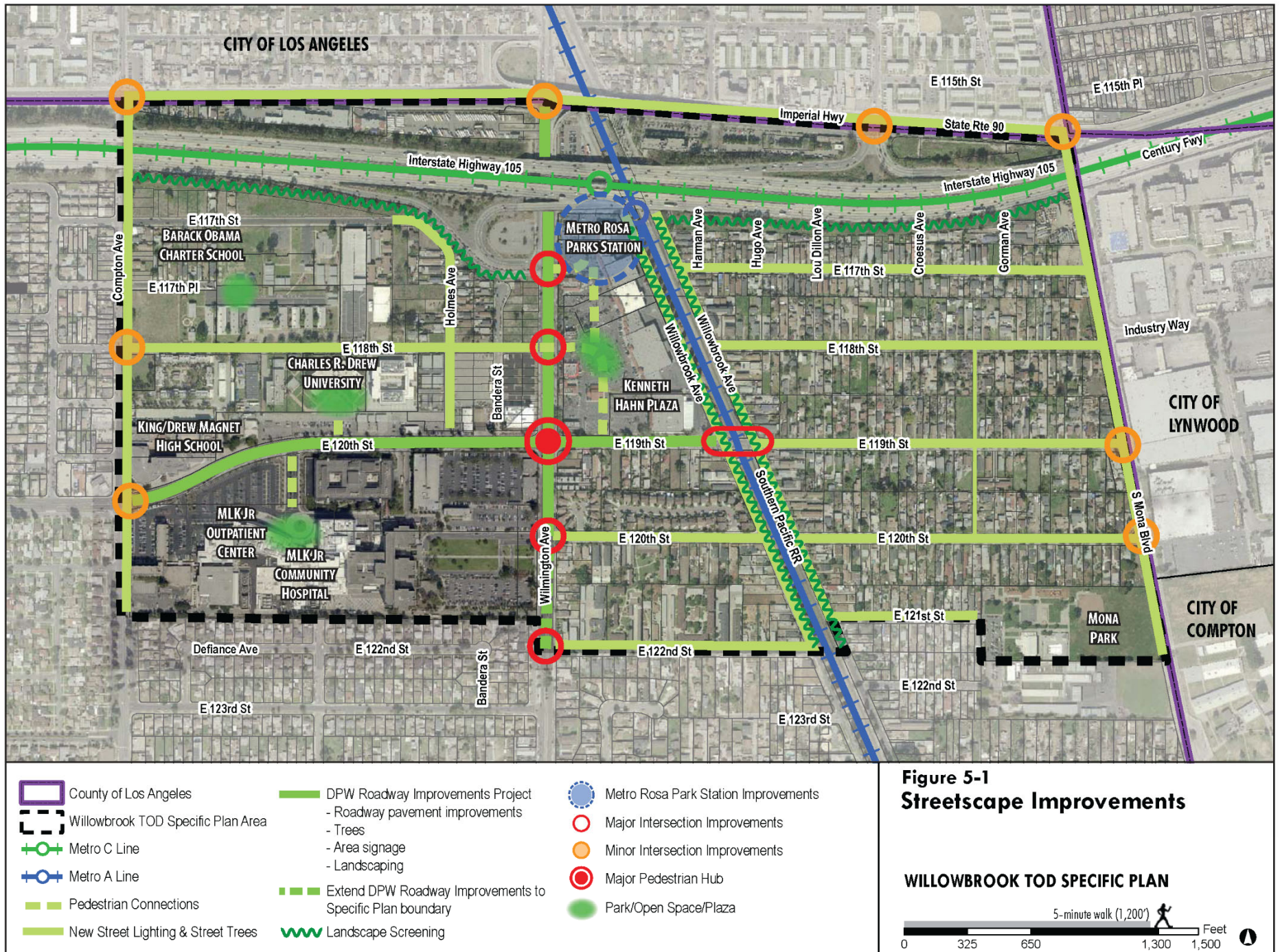
Recommended streetscape improvements consist of an interrelated palette of street trees, street furniture, street lighting, wayfinding signage, public art, and landscaping. (See Figure 5-1).

A. Willowbrook Area Access Improvements Project

DPW has streetscape improvements planned for Wilmington Avenue (Imperial to East 120th Street) and East 120th/East 119th Street (Compton to Willowbrook) as a part of the Willowbrook Area Access Improvements (WAAI) Project. The streetscape design includes new sidewalks, Class II bicycle lanes, solar pedestrian lights, street trees, street furnishings, bus shelters, and Low Impact Development features such as permeable paving and storm capture underground storage facilities. The WAAI Project also includes signage and wayfinding services with the theme of “willow and brook.” The Specific Plan carries the design themes of the WAAI Project to the rest of the Plan Area.

B. Green Alley Project

DPW is installing a green alley between East 118th Street and East 120th Street called “The Green Alley of Family and Fellowship.” The project connects East 118th Street to Faith and Hope Park at East 119th Street, and then continues to East 120th Street near the access way to Mona Park. It retains residents’ vehicular access to their homes while making the alley more pedestrian-friendly through the use of colored concrete pavement; curb and driveway reconstruction at the alley entrances; crosswalk enhancements; pedestrian-scale lighting; monument and wayfinding signs; and decorative planter walls distributed along its length that neighbors can use to grow vegetables, berries, and flowers.



C. Streetscape Improvements at the Intersection of Wilmington Avenue and East 120th/East 119th Street

The two major pedestrian streets are Wilmington Avenue in the north-south direction and East 120th/East 119th Street in the east-west direction. These two streets connect the major activity areas and across the rail tracks to the residential neighborhoods to the east. The intersection forms the pedestrian core of the Plan Area and should have special treatments that build on the WAAI Project (see Figure 5-2). These include a bulb-out on the southeast corner and special paving designs. A gateway structure across Wilmington Avenue could also be appropriate. Examples of gateway structures from other communities are shown below.

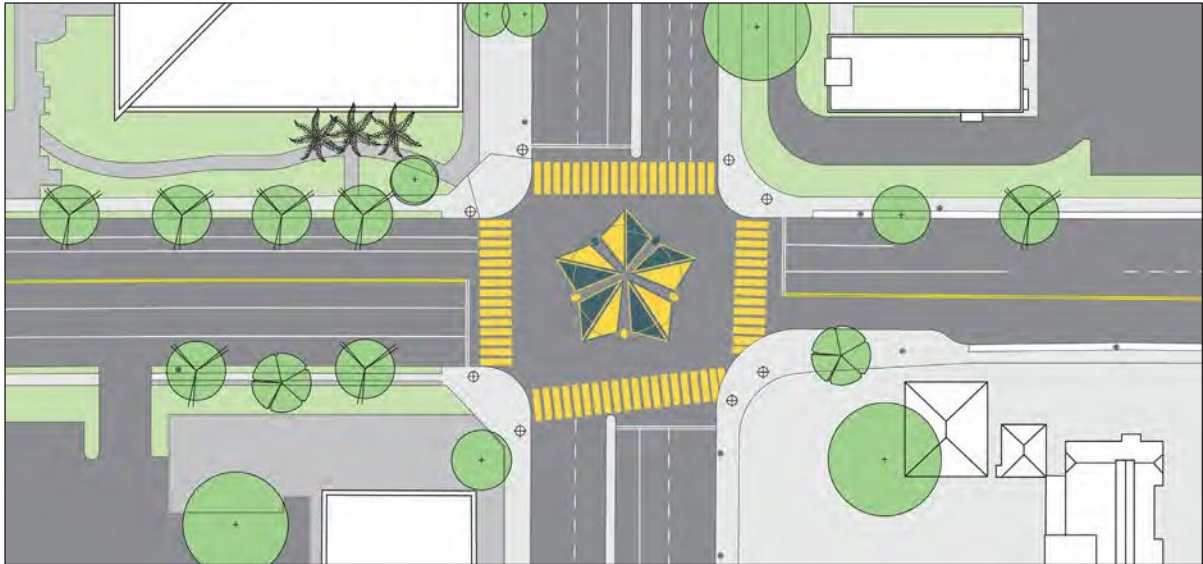
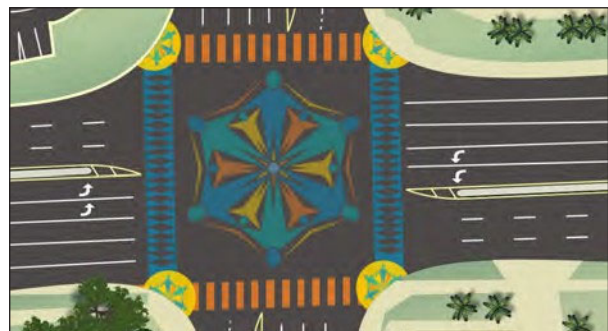


Figure 5-2: Wilmington Avenue and East 120th/East 119th Street intersection



Examples of paving designs that highlight special intersections.



Gateway structures that mark entries can also be used to celebrate the heart of a community.

D. Pedestrian Amenities

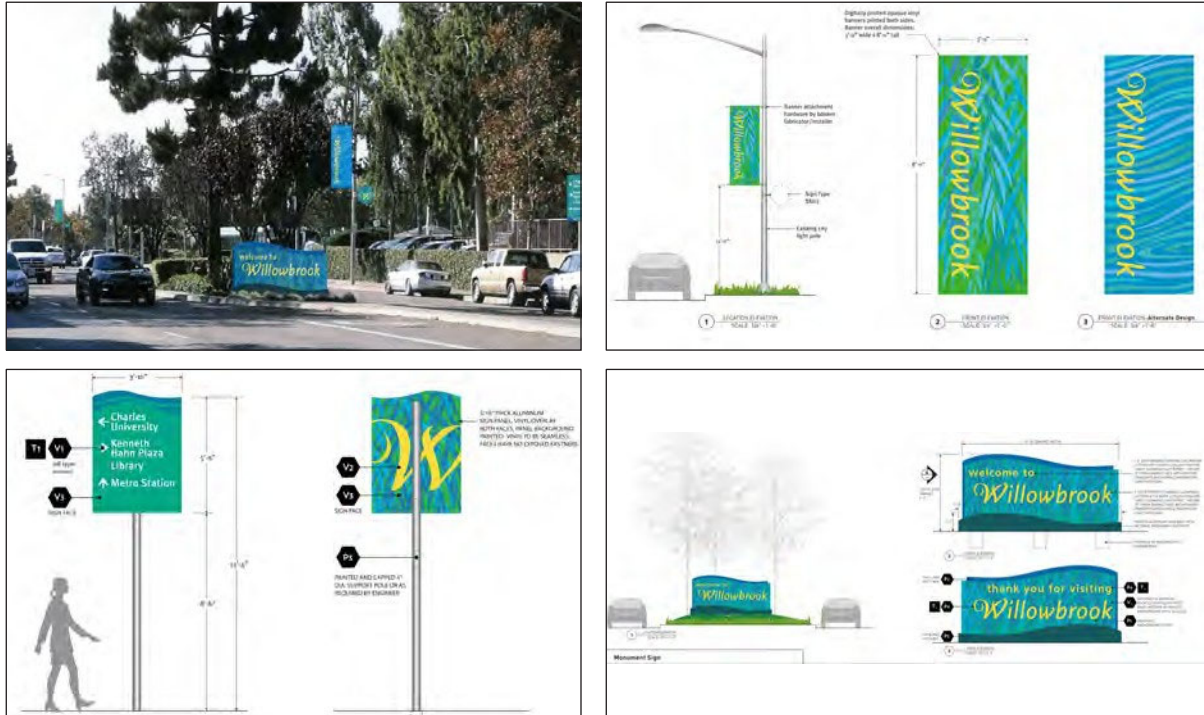
Engaging, pedestrian-oriented streetscapes include street trees (described in Section 5.2E), streetlights, street furniture, wayfinding, and landscaped open space. Public art and water features can also be appropriate. Pedestrian amenities should be focused along major streets and locations that have the most pedestrian activity.

- a. **Street Furniture.** A consistent palette of street furniture will help unify the area and provide essential seating, trash receptacles and bicycle racks. As part of the WAAI Project, fixtures will be installed to match the selected design theme and these should be continued throughout the Plan Area. Compatible street furniture is also encouraged in setback areas, plazas, courtyards and other publicly accessible spaces as appropriate and where access will not be impeded.



The selected street furniture palette in the Willowbrook Area Access Improvements project includes Landscape Forms' Presidio bench and trash receptacle. (Source: County of Los Angeles Department of Public Works)

2. **Wayfinding.** Wayfinding helps people orient themselves in physical space and navigate from place to place. A system of wayfinding signs, markers, and monument signs with a consistent and coordinated design is part of the WAAI Project. This wayfinding should be expanded to the entire Plan Area and particularly at the Willowbrook/Rosa Parks Station.



Banners, directional signs and monument signs are a part of the Willowbrook Area Access Improvements Project. (Source: County of Los Angeles Department of Public Works)

3. **Street Lighting.** Lighting promotes safety and provides spatial definition. The WAAI Project recommends lighting on key streets and that lighting should be extended to all streets in the Plan Area. Light fixtures should adhere to Dark Sky Association guidelines and Southern California Edison (SCE) standards for maintenance.
4. **Public Art.** Public art should be incorporated at the Willowbrook/Rosa Parks Station and early in project design particularly along major streets and at activity centers.

E. Street Trees

A mix of street tree species can be found in the Plan Area. A complimentary palette of carefully selected street trees will provide shade and a pleasant sidewalk experience, add seasonal color, and expand the urban forest. Table 5-1 and Figure 5-3 illustrate the street tree designations for the streets within the Specific Plan area. Figure 5-4 illustrates the Street Tree Palette from DPW Urban Forestry Unit's Maintenance District Street Tree List and street trees planned in the WAAI Project are incorporated. Tree spacing recommendations for each street and tree type noted in Table 5-1 are based on the canopy size of a mature tree of each species, the land uses along the streets, presence or absence of overhead power lines, and the available planting area. Final placement will be determined by DPW following all street tree and planting guidelines.

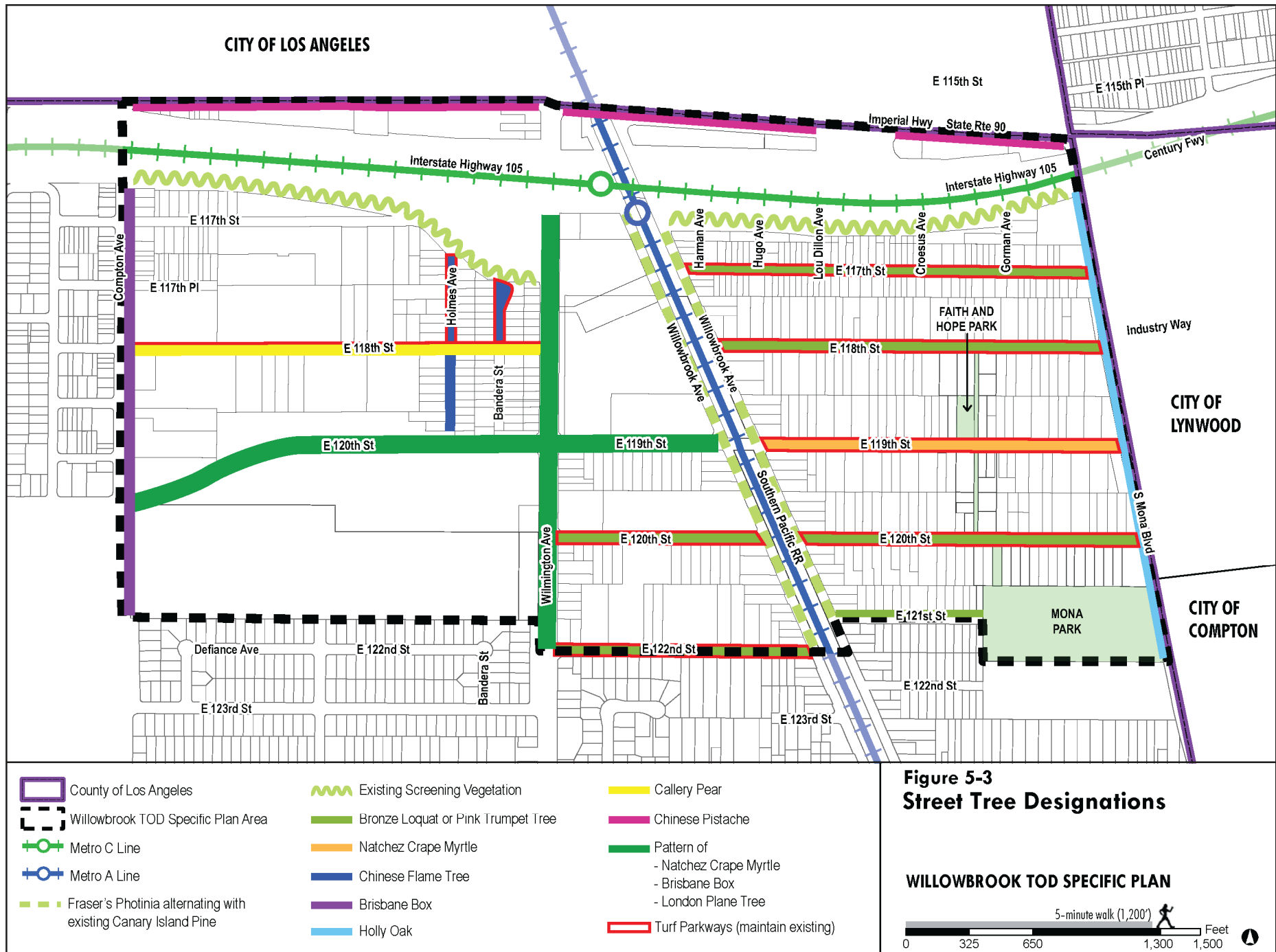


Figure 5-4: Street Tree Palette

**Pink Trumpet Tree***Handroanthus impetiginosus*

Deciduous

Height: 30-50 feet

Canopy: 30 feet

Bloom: Spring

Water: low to moderate

Powerline Friendly: Yes

**Bronze Loquat***Eriobotrya deflexa*

Evergreen

Height: 25-30 feet

Canopy: 20 feet

Bloom: Winter

Water: low to moderate

Powerline Friendly: Yes

**Chinese Flame Tree***Koelreuteria bipinnata*

Deciduous

Height: 25-45 feet

Canopy: 40 feet

Bloom: Summer

Water: moderate

Powerline Friendly: Yes

**'Natchez' Crape Myrtle***Lagerstroemia indica x fauriei*

'Natchez'

Deciduous

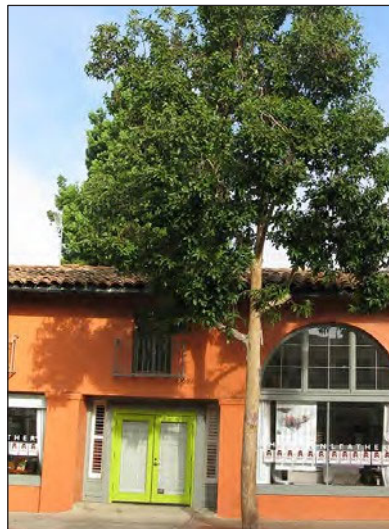
Height: 15-25 feet

Canopy: 25 feet

Bloom: Summer

Water: low to moderate

Powerline Friendly: Yes

**Brisbane Box***Lophostemon confertus*

Evergreen

Height: 30-60 feet

Canopy: 40 feet

Bloom: Summer

Water: low to moderate

Powerline Friendly: No

**Holly Oak***Quercus ilex*

Evergreen

Height: 30-65 feet

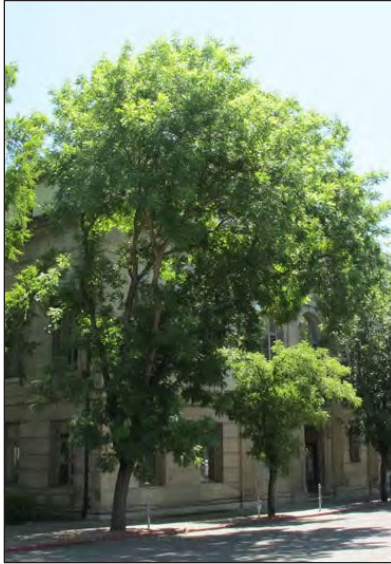
Canopy: 50 feet

Bloom: Spring, Summer

Water: low to moderate

Powerline Friendly: No

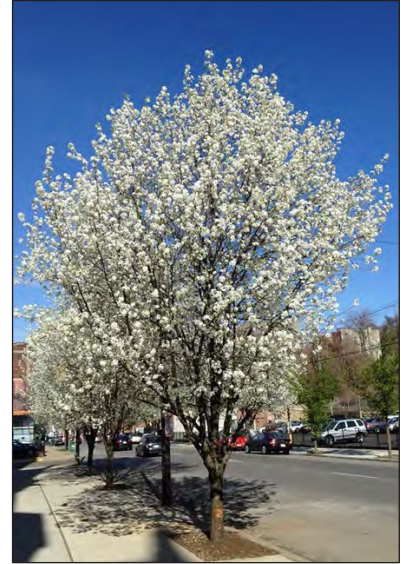
Figure 5-4 (continued)



Chinese Pistache
Pistacia chinensis
 Deciduous
 Height: 60 feet
 Canopy: 50 feet
 Bloom: Winter
 Water: low to moderate
 Powerline Friendly: No



London Plane Tree
Platanus x acerifolia
 'Bloodgood'
 Deciduous
 Height: 40-80 feet
 Canopy: 70 feet
 Bloom: Spring
 Water: moderate
 Powerline Friendly: No



Callery Pear 'Aristocrat'
Pyrus calleryana
 Deciduous
 Height: 35-45 feet
 Canopy: 35 feet
 Bloom: Spring
 Water: moderate
 Powerline Friendly: Yes



Canary Island Pine
Pinus canariensis
 Evergreen
 Height: 65-80 feet
 Canopy: 40 feet
 Bloom: None
 Water: low to moderate
 Powerline Friendly: No



Fraser Photinia
Photinia x fraseri
 Evergreen
 Height: 10-15 feet
 Canopy: 20 feet
 Bloom: Spring
 Water: moderate
 Powerline Friendly: Yes

Table 5-1: Street Tree Designations in the Specific Plan Area

Street Names	Existing	Specific Plan (2015)	Recommended Spacing*	Tree Well Dimensions**
North-South				
Compton Avenue (East side only)	Victorian Box <i>Pittosporum undulatum</i> Lemon Bottlebrush <i>Callistemon citrinus</i>	Brisbane Box <i>Lophostemon confertus</i>	30'-40'	5 sf
Holmes Avenue	Mixed	Chinese Flame Tree <i>Koelreuteria bipinnata</i>	30'-40'	5 sf
Wilmington Avenue	Mixed	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i> London Plane Tree (Maintain Existing) <i>Platanus x acerifolia</i>	-- -- --	
Willowbrook Avenue	Canary Island Pine <i>Pinus canariensis</i>	Fraser Photinia <i>Photinia x fraseri</i>	Alternating between existing	
Mona Boulevard (West side only)	None	Holly Oak <i>Quercus ilex</i>	40'-50'	
Bandera Street	None	Chinese Flame Tree <i>Koelreuteria bipinnata</i>	30'-40'	5 sf
East-West				
Imperial Highway (South side only)	Mixed	Chinese Pistache <i>Pistacia chinensis</i>	30'-40'	5 sf
117th Street	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf
118th Street (West of Wilmington Avenue) (North side only)	Mixed	Callery Pear <i>Pyrus calleryana</i>	30'-40'	5 sf
118th Street (East of Wilmington Avenue)	London Plane Tree <i>Platanus x acerifolia</i>	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf
119th Street (East of Willowbrook Avenue)	Mixed	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i>	20'-30'	4 sf
120th Street (East of Wilmington Avenue)	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf
120th Street (West of Wilmington Avenue)	Canary Island Pine <i>Pinus canariensis</i>	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i>	-- --	
121st Street (North side only)	None	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf
122nd Street (North side only)	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/Turf

*Recommended spacing considers the size of mature canopy, available planting areas, and adjacent land uses.

**Optimal sizes provided.

F. Parks and Open Space

Existing open space resources in the Plan Area include Mona Park, Faith and Hope Park, and the MLK Fitness Garden. Mona Park is a 8.4-acre recreational facility that includes a softball and multi-purpose fields, a gymnasium, and a swimming pool. It features year-round recreational programming including youth sports, the summer lunch program, after-school snacks, and after-school day camp programs. Faith and Hope Park opened in 2016 and will be connected north to 118th Street and south to Mona Park via the planned Green Alley of Family and Fellowship. Other important open space resources are the MLK Fitness Garden at the MLK Center for Public Health that incorporates a public walking path with fitness equipment and the planned Fellowship Garden of Love at Holmes Community Garden.



Faith and Hope Park opened in 2016 along East 119th Street.



MLK Fitness Garden is located along 120th Street.



Mona Park connects to East 120th Street along a landscaped paseo (shown on the right).

The recommended strategies for parks and open space build upon the existing and planned park spaces in the Plan Area. These strategies include:

- Exploring joint-use opportunities with the schools.
- Encouraging new development to provide public open space as a community benefit. Potential locations are shown in Figure 5-1.
- Considering building pocket parks and community gardens on County-owned vacant lots.
- Improving bicycle and pedestrian access to existing and future parks and open spaces.

G. Mona Boulevard Bicycle/Pedestrian Path

Mona Boulevard provides north-south pedestrian access on the east side of the Plan Area including access to Mona Park, the MLK Elementary School and the Dr. Ralph Bunche Middle School. Mona Boulevard between the I-105 Freeway to 124th Street is proposed to be converted from a four-lane street to a three-lane street with a pedestrian/bicycle trail along the west side of the street that meets Class I design guidelines. A typical cross section is shown in Figure 4-3 in Chapter 4.



The Mona Boulevard Bicycle/Pedestrian Path (a Class I Bicycle Path) will connect residential neighborhoods with Mona Park, MLK Elementary School and Dr. Bunche Middle School.

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6.1 Introduction

The purpose of this chapter is to provide an overview of existing and planned infrastructure facilities, and to outline recommended infrastructure upgrades for the Plan Area. The recommended upgrades are based on analyses of the capabilities and capacities of existing facilities and projected infrastructure needs based on build-out of the Willowbrook TOD Specific Plan. Figure 6-1 shows how the Plan Area has been divided into “groups” for the purposes of the analyses.

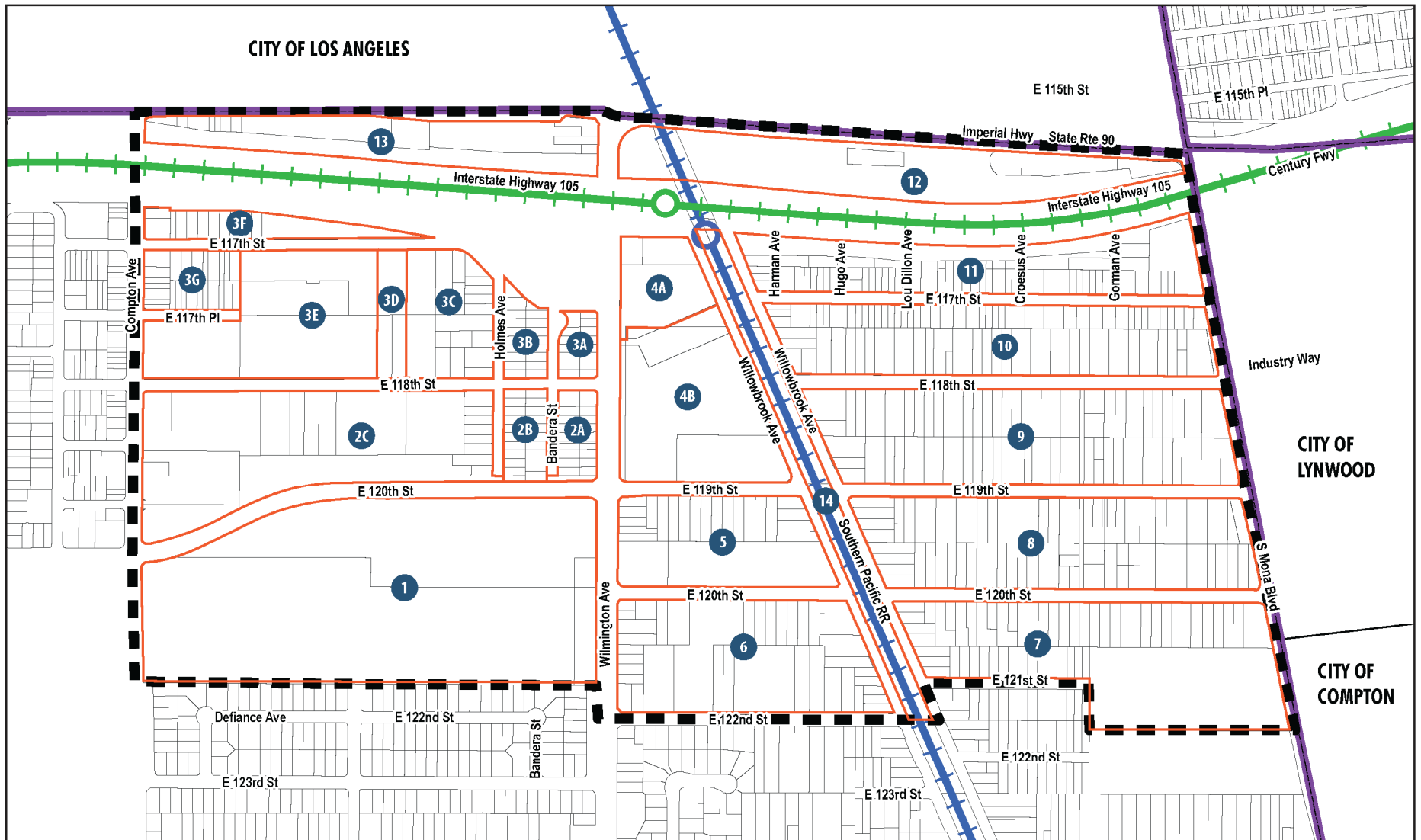
6.2 Water System

Three water purveyors serve the Plan Area: Liberty Utilities (formerly Park Water Company), Golden State Water Company and Los Angeles Department of Water and Power (LADWP). Park Water Company provides water service to the area west of Wilmington Avenue, and Golden State Water Company provides water service to the area east of Wilmington Avenue. There are potable water lines under almost all streets and alleys within the Plan Area. The sizes of the potable water lines vary from 4 to 12 inches. The existing pipes that distribute water services in the Plan Area include steel pipe (ST), asbestos cement pipes (AC), ductile iron pipes (DI), and cast iron pipes (CI). Figure 6-2 shows the layout of the existing water lines. This figure also illustrates the upgrades necessary to meet the projected demand.

As shown in Figure 6-2, except for Group 14 (which remains unchanged), almost all development capacity groups under total build-out of the Specific Plan have some increase in land use density, which in turn results in an increase in water demand. Most of the existing water lines in the Plan Area are 8 inches in diameter and above. The 8 inch pipes generally have the capacity to handle the increase in water demand/load under total build-out of the Plan Area. However, to meet the demand of water consumption, including water services for fire prevention purposes, such as demand of new fire hydrants and fire sprinkler systems in the new developments in the post total “build-out” condition, the following upgrades in water line pipe sizes are recommended:

Group 2A, 2B:

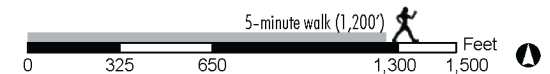
Replace existing water line of 4 inch CI on 118th Street north of this area with 8 inch DI (or PVC C-900).

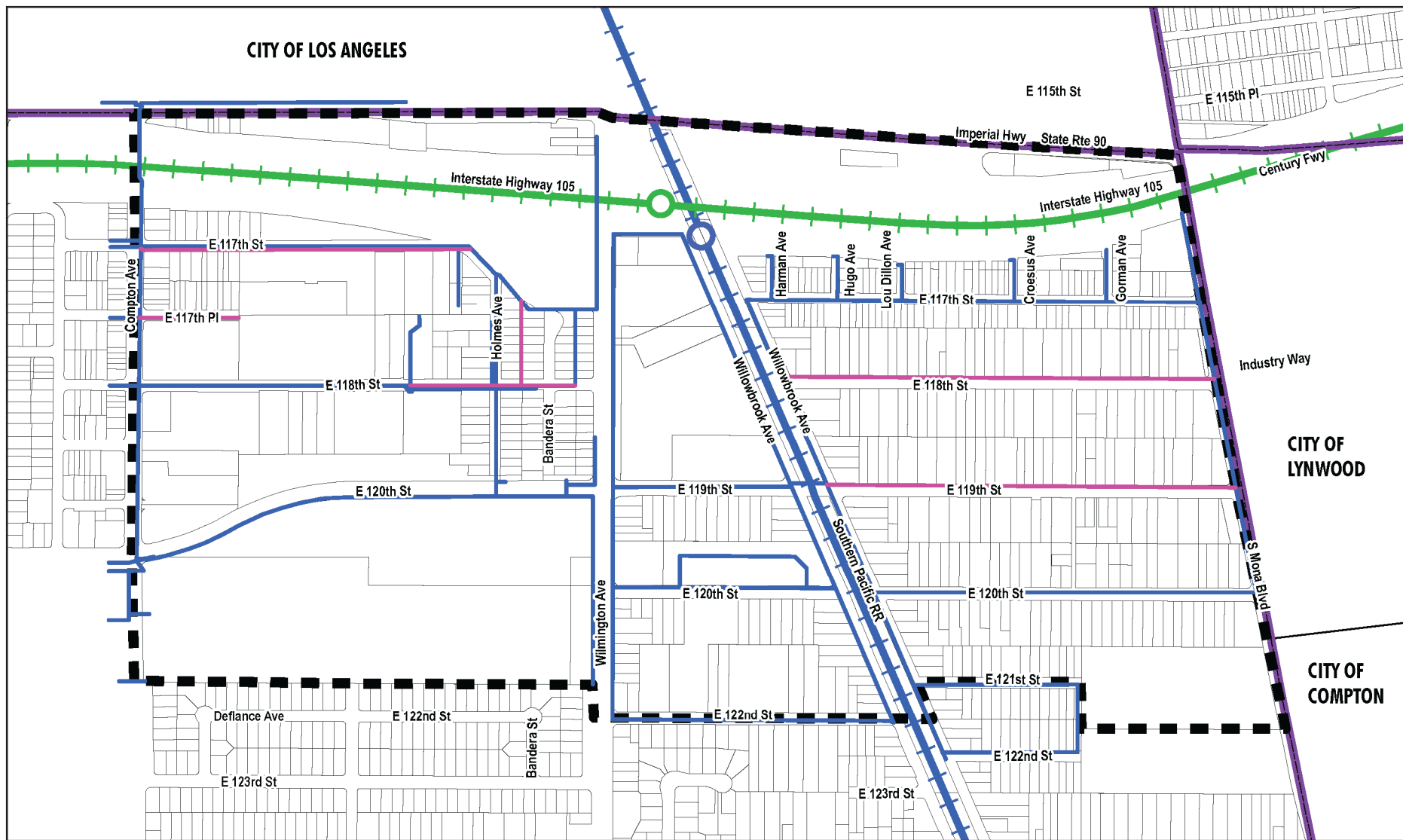


-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro C Line
-  Metro A Line

Figure 6-1
Key Map by Block for Calculating
Infrastructure Demand

WILLOWBROOK TOD SPECIFIC PLAN

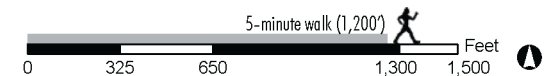


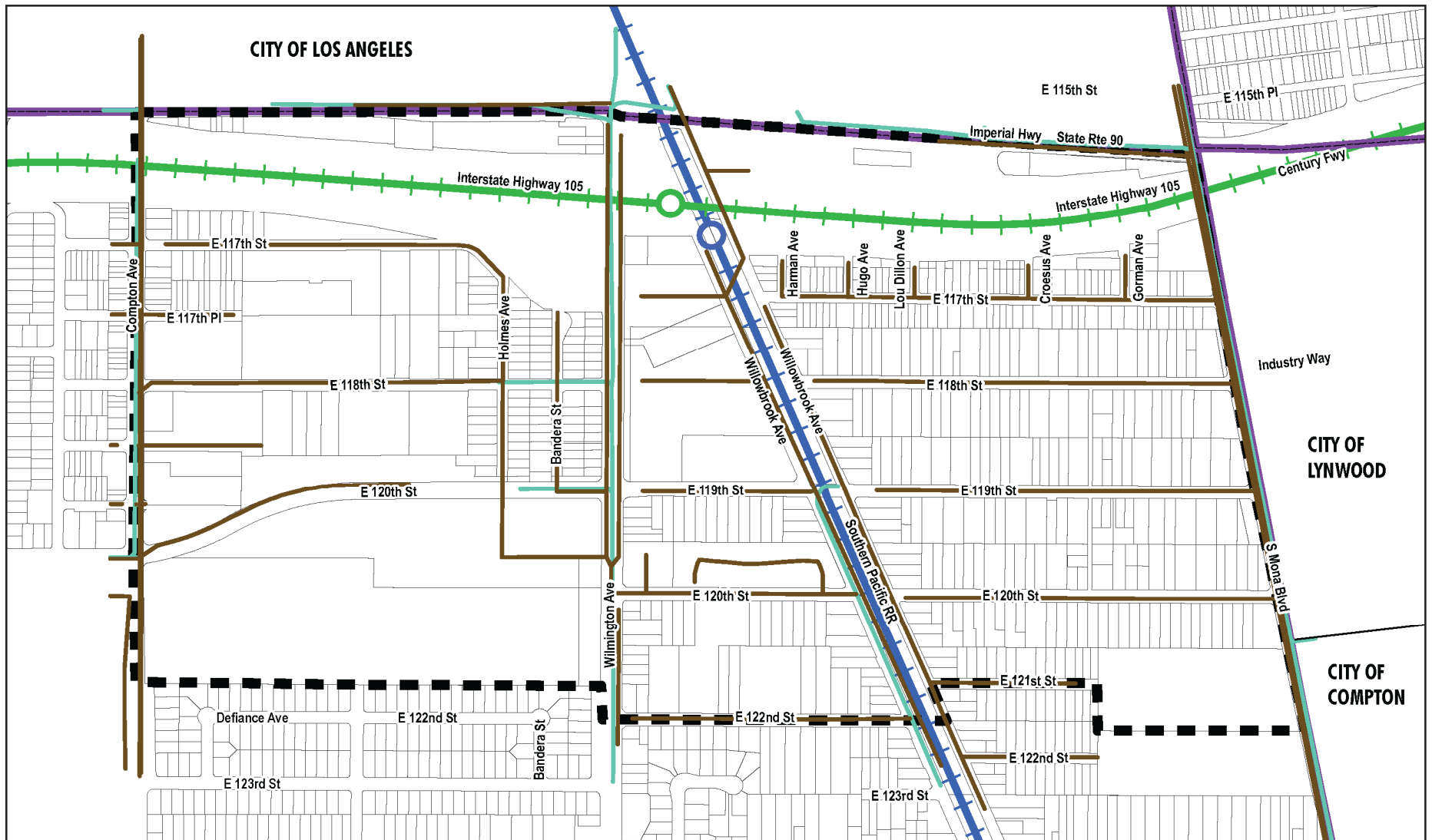


- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro C Line
- Metro A Line
- Existing Water Sytem
- Upgrade 4 or 6" (E) Water to 8" DI Water

Figure 6-2
Existing and Planned
Water System

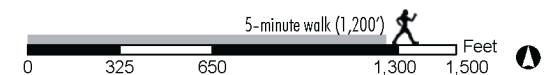
WILLOWBROOK TOD SPECIFIC PLAN





**Figure 6-3
Existing Sewer and
Storm Drain System**

WILLOWBROOK TOD SPECIFIC PLAN



- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro C Line
- Metro A Line
- Existing Sewer System
- Existing Storm Drain System

Group 3B:

Replace existing water line of 6 inch CI in the alley between Holmes Avenue and Bandera Street with 8 inch DI (or PVC C-900).

Group 3C, 3D, 3E, 3F, 3G:

Upgrade existing water line of 4 inch CI on 117th Street from Compton Avenue to Holmes Avenue and the 4 inch CI in 117th Place with 8 inch DI (or PVC C-900).

Group 8, 9:

Upgrade existing water line of 6 inch CI and 4 inch CI in 119th Street to 8 inch DI (or PVC C-900) from Willowbrook Avenue to Mona Boulevard.

Group 9, 10:

Replace existing water line of 6 inch CI and 4 inch CI in 118th Street with 8 inch CI from Willowbrook Avenue to Mona Boulevard.

All three water purveyors will evaluate future development within their service areas and conduct their own detailed analysis, and will ultimately decide whether the water distribution facilities are required to be upgraded.

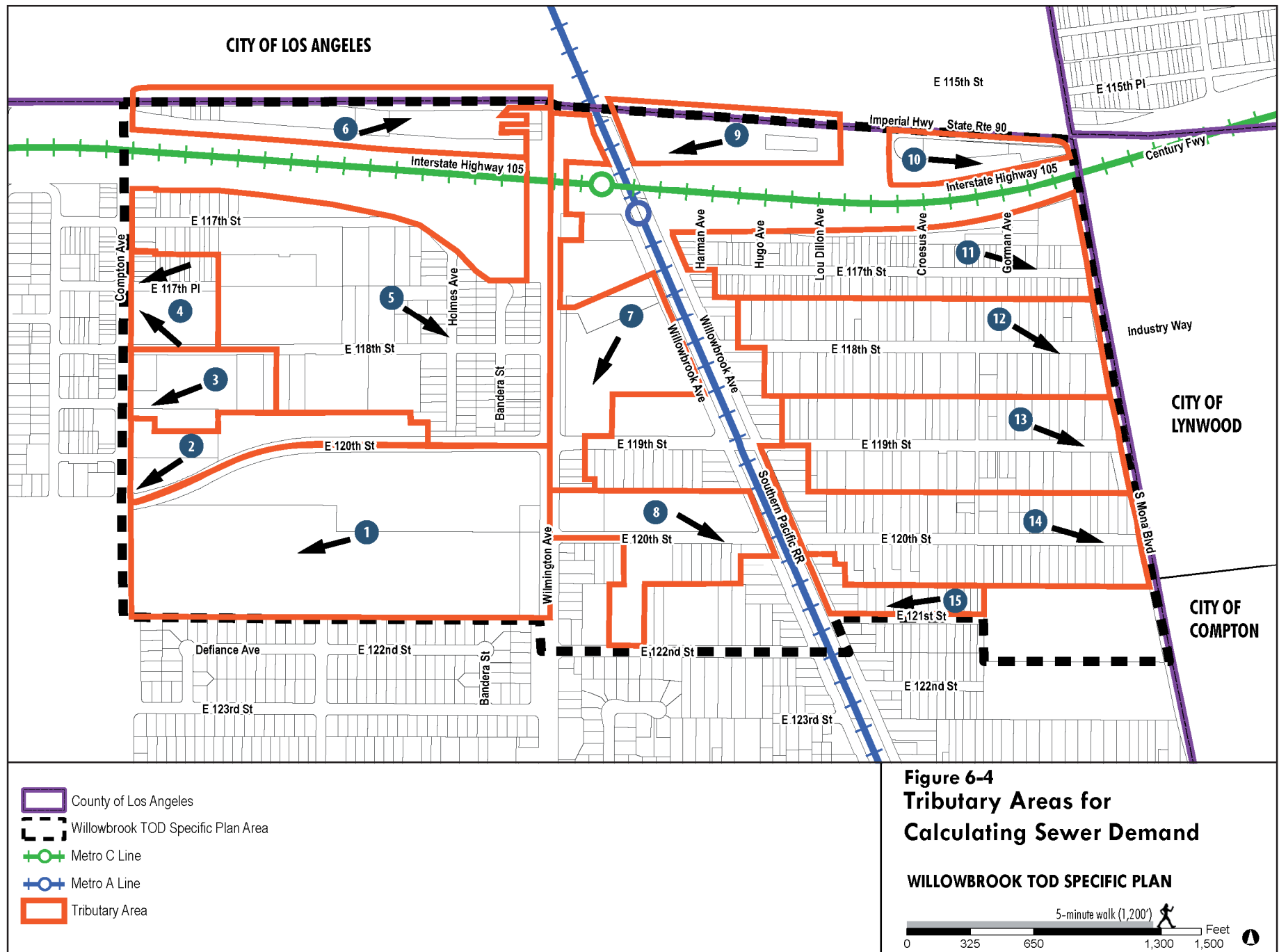
6.3 Sewer System

The sewer lines serving the Plan Area are owned and maintained by DPW and the Sanitation Districts of Los Angeles County (LACSD). The sewer lines within the area are mainly Vitrified Clay Pipes (VCP). The sewer mains are all under the major streets with smaller lateral lines connecting directly to residential buildings or businesses (Figure 6-3).

As mentioned earlier in the water system section, except for Group 14 (which remains unchanged), almost all development capacity groups under total build-out of the Specific Plan have some increase in land use density, which in turn results in an increase in sewage effluent throughout the Plan Area. The cumulative sewage effluent by each Tributary Area (shown in Figure 6-4 and Table 6-1) has been calculated using the Zoning Coefficient for runoff provided for a typical sewer area study by the DPW Land Development Division. All existing sewer mains in the Plan Area are 8" or above in diameter. Using the minimum allowable slope of 0.24 percent, the design capacity for the existing sewer mains are at least 0.27 cfs (some pipes are larger than 8 inch). There are five (5) existing sewer collection points that may not be adequate to meet the future cumulative sewage effluent.

One of the purposes of this study is to identify the deficiencies, if any, of the existing utilities when compared to the future developed condition. This study has identified that the developments in Tributary Areas 5, 7, 8, 11, and 16 have the potential to exceed the existing sewage capacity of their connecting sewer mains. The rest of the groups should remain below the sewage capacity.

Since the design capacity refers to future full build-out conditions there is no real justification to upgrade any of these sewer mains at this time. Regardless, DPW will require a detailed sewer area study of any new development within Tributary Area 5. The sewer area study will determine whether any upgrade will be required for sewer system network at the time of that specific development. The LACSD will also review all new sewer infrastructure upgrades as DPW will send improvement plans for the district to review.



**Table 6-1:
Sewer Area Study Calculations by Tributary Area**

Street Name	Pipe		Area (Acres)	Calculated Flow (cfs)	Cumulative Calculated Flow* (cfs)	Cumulative Depth (inches)	Comment
	Size (inches)	Slope (%)					
Tributary Area 1 (Compton Ave)	8	0.24					<i>Not Part of Calculations</i>
Medical Buildings & Residential (R-3)			N/A				<i>Private Sewer Line</i>
Tributary Area 2 (Compton Ave)	8	0.24			0.1836	3.2	
Commercial			3.60	0.0540			
High School (1,675 people x 20 gal/day x 2.5)			N/A	0.1296			1,600 students & 75 staff
Tributary Area 3 (Compton Ave)	8	0.24			0.1350	2.8	
Residential (R-3)			4.50	0.1350			
Tributary Area 4 (Compton Ave)	8	0.24			0.1041	2.4	
Lincoln ES 2.5 x ((280 students x 15 gal/day) + (50 staff x 20 gal/day))			N/A	0.0201			280 students & 50 staff
Mixed-Use 2 (60 du/acre)			1.40	0.0840			
Tributary Area 5 (Wilmington Ave)	8	0.24			1.9023	6.4	
Lincoln ES 2.5 x ((280 students x 15 gal/day) + (50 staff x 20 gal/day))			N/A	0.0201			280 students & 50 staff
Charter ES 2.5 x ((336 students x 15 gal/day) + (35 staff x 20 gal/day))			N/A	0.0222			336 students & 35 staff
Mixed-Use 2 (60 du/acre)			31.00	1.8600			
Tributary Area 6 (Imperial Hwy)	8	0.24			0.1061	2.5	
Commercial			7.07	0.1061			
Tributary Area 7 (Wilmington Ave)	8	0.24			0.4437	5.4	
Mixed-Use 1 (30 du/acre)			14.79	0.4437			
Tributary Area 8 (Willowbrook Ave)	15	0.12			2.7945	12.1	
Residential (R-3)			11.69	0.3507			
Residential (R-2)			3.60	0.0720			
Residential (R-1)			2.86	0.0257			
Tributary Area 9 (Willowbrook Ave)	8	0.24			0.0356	1.4	
Commercial			2.37	0.0356			
Tributary Area 10 (Mona Blvd)	8	0.24			0.0323	1.4	
Commercial			2.15	0.0323			
Tributary Area 11 (Mona Blvd)	8	0.24			0.2887	4.2	
Residential (R-3)			0.99	0.0297			
Residential (R-2)			12.95	0.2590			
Tributary Area 12 (Mona Blvd)	8	0.24			0.1654	3.1	
Residential (R-1)			18.38	0.1654			
Tributary Area 13 (Mona Blvd)	8	0.24			0.1516	3.0	
Residential (R-1)			16.84	0.1516			
Tributary Area 14 (Mona Blvd)	8	0.24			0.2006	3.4	
Residential (R-3)			2.17	0.0651			
Residential (R-1)			15.05	0.1355			
Tributary Area 15 (Willowbrook Ave)	8	0.24			0.1310	2.8	Manhole 48
Residential (R-3)			3.70	0.1110			Includes 117th to 119th
Residential (R-1)			2.22	0.0200			
Tributary Area 16	12	0.24			1.0468	7.1	Trunk Line
Residential (R-3)			19.00	0.5700			at Oris and Willowbrook
Residential (R-2)			16.64	0.3328			
Residential (R-1)			16.00	0.1440			

* Based on current land use and coefficients per DPW.

Sewer laterals are sewer pipes which connect with buildings with sewer mains that run in the street. In general, new or upgraded sewer laterals are required for new buildings, with the cost of the installation borne by the developers.

6.4 Storm Drain System

The storm drain system serving the Plan Area is maintained by the Los Angeles County Flood Control District (LACFCD). The storm drain system within the Plan Area consists of mostly underground Reinforced Concrete Pipes (RCP) ranging from 24 to 54 inches (Figure 6-3).

Build-out of the Plan Area will generate little or no increase in runoff to the existing drainage system, since the area is completely developed. Approximately 80-90 percent of the existing Plan Area is impervious. It is predicted that the new development will not directly trigger any need for upgrades to the County's existing storm drain major backbone facilities, mainly due to the Low Impact Development (LID) Ordinance requirements for percolation and on-site detention for new development, which will stabilize and/or even reduce runoff in the Plan Area.

There are no recommendations to upgrade the existing storm drain system within the Plan Area.

LID Practices and Project Requirements

In October 2008, the County adopted the LID Ordinance as part of Los Angeles County Code Title 12, Chapter 84 to require the use of LID principles in all development projects except road and flood infrastructure projects. Unlike traditional stormwater management, which collects and conveys stormwater runoff through storm drains, pipes, or other conveyances to a centralized stormwater facility, LID uses site design and stormwater management to maintain the site's pre-development runoff rates and volumes. The goal of LID is to mimic a site's pre-development hydrology by using design techniques that infiltrate, filter, store, evaporate, and detain runoff close to the source of rainfall.

LID practices or stormwater quality control measures can be categorized into the following types:

- *Retention-based stormwater quality control measures*
Examples: bio-retention, infiltration basin, dry well, permeable pavement.
- *Bio-filtration*
Example: bio-filtration area.
- *Vegetation-based stormwater quality control measures*
Example: stormwater planter (or planter box), vegetated swale, green roof.
- *Treatment-based stormwater quality control measures*
Examples: sand filter, constructed wetland, proprietary treatment control measures.

All new development that requires new grading in the Plan Area will require preparation of a hydrology study to demonstrate that building sites are free from flooding hazard. New development will be required to mimic the site's pre-development runoff by choosing the appropriate LID practice most suitable for the site. A proposed project must demonstrate that any proposed improvement, including filling, does not raise the flood level upstream or downstream of the project. As required by the County, developers shall prepare National Pollution Discharge Elimination System (NPDES) reports, such as the Water Quality Management Plan (WQMP) and Stormwater Pollution Prevention Plan (SWPPP), to ensure the quality of water is preserved and adverse environmental impacts are minimized.

6.5 Electrical System

Southern California Edison (SCE), which is a private utility company, provides electrical power service to the Plan Area. SCE sets its own service standards, with oversight from the California Public Utilities Commission (CPUC), and facility improvement strategies. Currently, there is a network of aerial electrical facilities creating a power grid that supplies sufficient electrical service to the Plan Area. There is no major deficiency or functional problem in the power supply facilities within the Plan Area. The specific locations of the existing underground and overhead electrical lines are illustrated in Figure 6-5.

The decision to upgrade the power supply facilities and the number of upgrades to meet the demand of future development will be determined by SCE in coordination with the County after developers have submitted their building plans. Demand for services and the ability to serve new developments are generally determined on a case-by-case basis.

Most of the electrical distribution facilities in the Plan Area are aerial facilities. The Specific Plan recommends that existing aerial electrical facilities be placed underground whenever funding is available. Underground electricity provides higher reliability, is safer in general, and is also less unsightly. One of the potential funding mechanisms is CPUC Rule 20. CPUC Rule 20 is a set of policies and procedures established by the CPUC to regulate the conversion of overhead electric equipment to underground facilities, a process often referred to as “undergrounding.” Rule 20 determines the level of ratepayer funding for different undergrounding arrangements.

CPUC Rule 20

Under Rule 20, undergrounding projects are financed by utility rate money, combined rate funds and local tax proceeds, or private funds, depending on whether Rule 20A, Rule 20B or Rule 20C provisions apply.

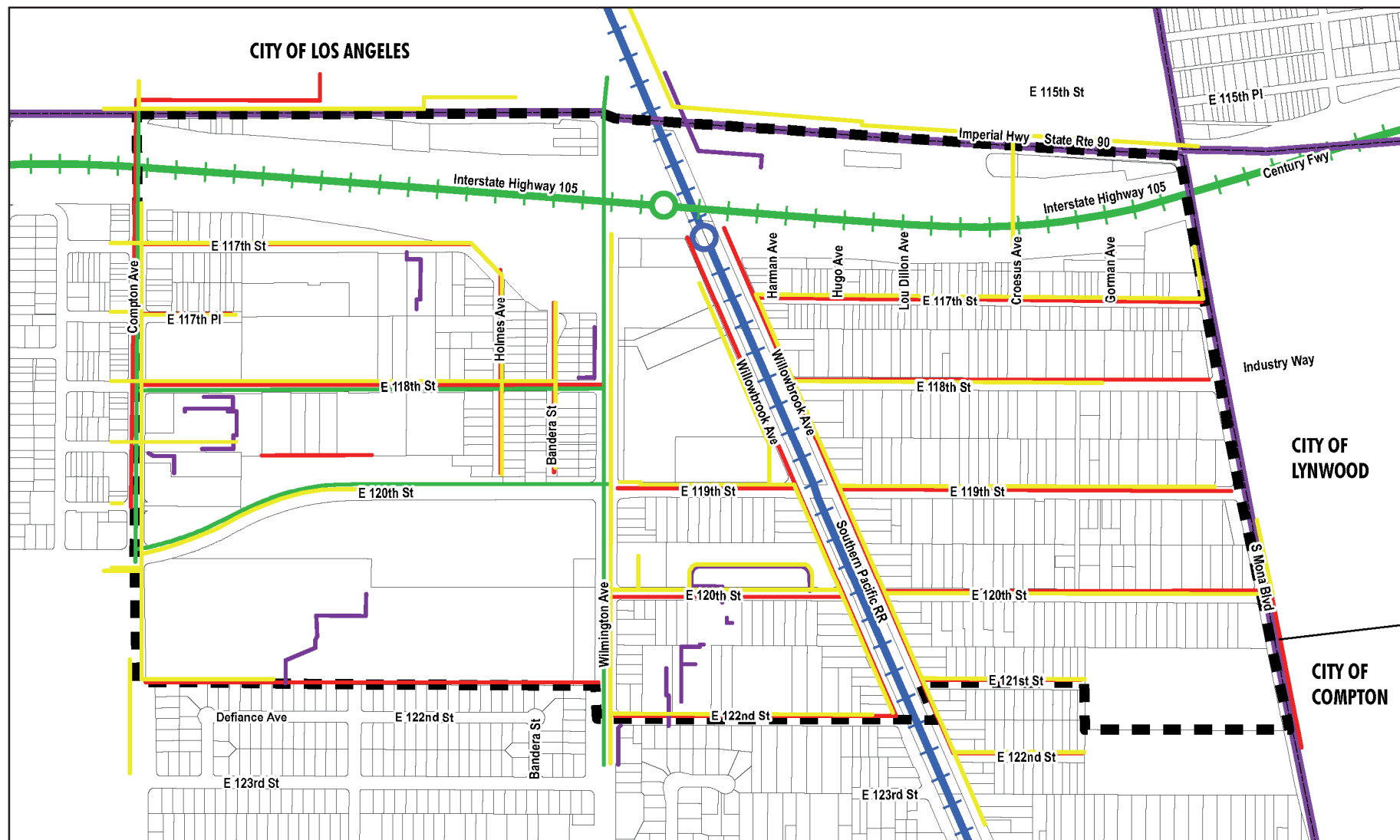
Rule 20A: *Rule 20A projects are paid for by all SCE customers and ratepayers, not just those who live in locations where facilities will be undergrounded. To qualify for full funding through utility rate proceeds, projects must produce a benefit to the general public, not just customers in the affected area, by satisfying one or more of these criteria:*

- *The location has an unusually heavy concentration of overhead facilities.*
- *The location is heavily traveled.*
- *The location qualifies as an arterial or major collector road in a local government’s general plan.*
- *The overhead equipment must be located within or pass through a civic, recreational or scenic area.*

Using CPUC formulas, SCE allocates rate funds to communities for undergrounding based on previous allocations, the ratio of customers served by overhead facilities to all the customers in the community, and the fraction that customers in the community represent of all SCE customers. Local governments use these formulas to project allocations, which allow them to prioritize projects and develop project schedules. Because funds are limited, local governments sometimes must wait and accumulate their allocations before starting an undergrounding project.

Rule 20B: *If an area is not eligible for Rule 20A or if local government cannot or chooses not to rely on the Rule 20A allocation process, Rule 20B allows rate funds to subsidize an undergrounding project. The subsidy includes an amount equal to about 5 to 20 percent of the total cost. The remaining cost is funded by local governments or through neighborhood special assessment districts.*

Rule 20C: *Rule 20C enables property owners to pay for undergrounding electric lines and equipment if neither Rule 20A nor 20B applies.*

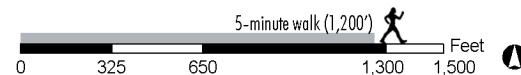


- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro C Line
- Metro A Line

- Electrical, Telecommunications, Cable TV Aerial Facilities
- Gas
- Underground Telecommunications Conduit
- Cable Television

**Figure 6-5
Existing Dry Utilities**

WILLOWBROOK TOD SPECIFIC PLAN



It is also worth noting that, prior to any undergrounding process, SCE will take the lead in contacting the other low voltage utilities that might be sharing the power poles with SCE aerial facilities, such as the telecommunication provider (AT&T) and cable TV provider (Spectrum) to coordinate and execute a joint trench. During this process, SCE will also reach out to DPW, Public Utility Division, for their assistance in contacting and coordinating with the aforementioned utility providers.

It is recommended that prior to approving new development, the County should contact the SCE regional manager in the Willowbrook area and set up an inspection at the new development site to determine whether the existing aerial SCE facilities in the area qualifies for Rule 20A funding.

6.6 Natural Gas System

The Southern California Gas Company/Sempra Utilities (The Gas Company), which is a private utility company, is the gas service provider for the Plan Area. Currently, gas pipelines are in all major streets in the Plan Area, specific locations of gas pipelines are illustrated in Figure 6-5.

The analysis on the capacity and capability to meet future demand will be conducted by The Gas Company in coordination with the County upon submittal of building plans by developers.

6.7 Telecommunications System

The Plan Area is within the service area of AT&T, which is a private utility company. AT&T is the provider of both local and long distance telecommunications in the Plan Area (Figure 6-5). The current local telecommunications network lacks the high speed internet service that will be in demand by future businesses and residents in the area.

AT&T will assess the demand for services and the ability to serve new developments on a case-by-case basis. The capacity and capability analysis for meeting future demands within the Plan Area will be conducted by AT&T after building plans are submitted by developers.

If there is “undergrounding” of existing aerial telecommunication facilities, SCE will reach out to AT&T and coordinate a joint trench. To save on costs, AT&T is likely to participate in the joint trench operation. SCE will dictate the layout of the trench, and the telecommunication lines would follow. According to the representative from AT&T, there are no major facility upgrades planned in the Plan Area in the near future. However, if there are facilities upgrades in the future, in most cases, AT&T will be responsible for the construction cost up front, and will recoup the cost later with the additional revenue from added customers.

6.8 Cable Television System

The cable service provider for this area is Spectrum, which is a private utility company. Most of the existing cable TV systems within the Plan Area are aerial facilities that share power poles with the electrical system, with a few existing underground conduits (Figure 6-5).

New land uses resulting from the Specific Plan will consist of both commercial and residential development; therefore, high speed internet and cable television services will be in demand. Similar

to a telecommunications system, Spectrum will assess the demand for services on a case-by-case basis and ultimately make the decisions concerning upgrades for the existing cable TV systems to meet the demand of the future developments.

If there is undergrounding of existing aerial telecommunication facilities, SCE will reach out to Spectrum and coordinate a joint trench. Similar to a telecommunications provider, Spectrum is likely to participate in the joint trench operation to save on construction costs.

In most cases, if there are facilities upgrades required, Spectrum will be responsible for the construction costs up front, and recouping the cost later on with the additional revenue from the added customers.

Economic Development Strategy

7.1 Introduction

The Plan Area is expected to undergo sizable growth in employment to support uses related to the hospital, medical offices, education and research and development, and retail and commercial offices. The purpose of this chapter is to provide the Economic Development Strategy for the Willowbrook TOD Specific Plan. The strategy provides economic analysis, a financing strategy, and policy recommendations to realize the full potential of Metro's investments in the redesigned Willowbrook/Rosa Parks Station and the County's investments in the MLK Medical Center Campus. The economic development strategy includes:

- Strategies that leverage public investments within the Specific Plan framework and build upon local assets to revitalize the Willowbrook community;
- Identification of potential community benefits from the implementation of the Specific Plan;
- Projections of employment and wage growth from implementation of the Specific Plan;
- Capital Improvement Program (CIP) that identifies and prioritizes a range of infrastructure and amenity improvements that benefit the community; and
- Financing approaches for the implementation of infrastructure and services.

The SCAG employment and demographic forecasts for the study area and the MLK Medical Campus Master Plan, as well as supporting documents and studies for the MLK Community Hospital and CDU informed the strategies.

7.2 Opportunities for Economic Development

The rehabilitation and expansion of the MLK Medical Campus, the redesign of the Willowbrook/Rosa Parks Station and the potential demand for ancillary medical services that may locate nearby, are the main economic opportunity drivers for the area. It is important to leverage these economic investments to provide employment and housing development opportunities to the Specific Plan area and the greater Willowbrook community.

7.3 Vision for a Community of Health and Wellness

The revitalized MLK Medical Campus is a catalyst for broader economic development and community service benefits to the Willowbrook and neighboring communities. By facilitating bicycle and pedestrian improvements and increasing transit access and housing choices, this Specific Plan helps realize the community's vision for health and wellness. This vision was inspired by a ULI Technical Assistance Panel study in 2009. The ULI panel identified the entire master-planned area as having the potential to become a health-focused, transit-oriented district.

The Plan Area has several interconnected subareas located to the west of the rail tracks and south of the freeway: 1) MLK Medical Center Subarea encompassing the MLK Medical Center campus; 2) CDU Campus Subarea encompassing CDU and King Drew Magnet High School; 3) Northwest Subarea including Lincoln-Drew Elementary School, Barack Obama Charter Elementary School, a proposed community college workforce training center, and an expanded County library; 4) the Kenneth Hahn Plaza Subarea; 5) single and multi-family homes and sites with retail and mixed-use development potential on Wilmington Avenue; and 6) the Willowbrook/Rosa Parks Station Subarea.

7.4 Economic Development Strategy

The Economic Development Strategy recommends implementation strategies. Key components of the strategy are discussed in this section and summarized in Table 7-1. The steps and actions within the recommended implementation strategy are contingent upon funding and allocation of resources. The timeline identified is a relative estimate in comparison to other steps and actions within the strategy. Other key components are public financing and economic incentive methods for implementing the conceptual CIP in the Plan Area, as presented in Chapter 8.

The Economic Development Strategy presents a range of public and private financing techniques and related programs that could be used as part of the implementation strategy for the CIP, including land use incentive strategies, financing program initiatives, and public-private partnerships that build upon the substantial public and private investment into the MLK Community Hospital, CDU and the redesign of the Willowbrook/Rosa Parks Station.

Additionally, the CIP compiles the required public infrastructure and related costs to implement the Specific Plan. The CIP also identifies the projects that are essential to attract private investment within the Plan Area, and presents short-, mid- and long-range priorities over the anticipated build-out of the Plan Area.

Furthermore, the Economic Development Strategy presents five categories of actions to facilitate local economic development objectives. These categories highlight actions available to local County government to promote the vision of the Specific Plan. These actions include: a) institutional arrangements; b) targeted development opportunities; c) land use planning; d) infrastructure and services planning; and e) implementation and financing strategies.

**Table 7-1:
Implementation Strategy**

Implementation Step ²	Key Action ²	Coordinating Agencies	Timeframe ¹
Develop Institutional Arrangements	<ul style="list-style-type: none"> Work with the MLK Health and Wellness Community Development Corporation (MLK-CDC). 	County of Los Angeles Community Based Stakeholders	Short-Term
Foster Public-Private Partnerships	<ul style="list-style-type: none"> Coordinate with MLK Community Hospital and Charles Drew University to integrate local workforce training and capabilities with ultimate job growth, direct spin-off activities in healthcare services and allied office uses. 	County of Los Angeles MLK Community Hospital Charles Drew University MLK-CDC	Ongoing
Improve Kenneth Hahn Plaza	<ul style="list-style-type: none"> Work with Kimco Realty Corporation, the operator of Kenneth Hahn Plaza, to enhance the retail and dining experience and coordinate pedestrian, safety, and landscape improvements at the Plaza. Assist in tenant recruitment. Work with Kimco Realty Corporation to explore mixed-use development. 	County of Los Angeles Kimco MLK-CDC	Ongoing Ongoing Long-term
Continue Partnership with Los Angeles County Workforce Collaborative	<ul style="list-style-type: none"> Continue to collaborate with the Workforce Collaborative's initiative to coordinate workforce training programs for residents with public and private employers. 	County of Los Angeles LAC Workforce Collaborative LACDA	Ongoing
Create Opportunities for Infill Mixed-Use Development	<ul style="list-style-type: none"> Coordinate with existing property owners on economic development opportunities of under-utilized and vacant sites. Coordinate with Compton Unified School District(CUSD) regarding the redevelopment of the school sites. Coordinate with the LACDA regarding the use of the Successor Agency-owned parcels. 	County of Los Angeles LACDA	Mid- to Long- Term
Pursue Local, State and Federal Infrastructure Funding Sources	<ul style="list-style-type: none"> Identify, monitor, and apply for other government funding sources, for infrastructure and transportation improvements (also see Tables 7-2, 8-2 and 8-3). 	County of Los Angeles LACDA Metro	Ongoing
Establish an Enhanced Infrastructure Financing District (EIFD)	<ul style="list-style-type: none"> Conduct feasibility analysis and necessary elections for establishing an EIFD for financing public improvements and amenities. 	County of Los Angeles LACDA	Short- to Mid- Term
Establish a Development Impact Fee (DIF) Program	<ul style="list-style-type: none"> Conduct feasibility analysis for establishing a DIF Program for equitably spreading infrastructure costs equitably over new development. 	County of Los Angeles LACDA DPW/DRP	Short- to Mid- Term
Establish a Parking Management District in the Northwest Subarea	<ul style="list-style-type: none"> Consider a parking management plan in the Northwest Subarea to improve the availability of parking supply and pricing. 	Partnership between County of Los Angeles and Major Property Owners/Developers	Mid- to Long- Term
Establish a Business Improvement District (BID)	<ul style="list-style-type: none"> Marketing/promotional events/security/maintenance above existing public services; small scale capital improvements. Assessments must be benefit based. 	Property Owners/Business Owners	Mid- to Long- Term

1.The timeline identified is a relative estimate in comparison to the other actions included in the strategy.

Short-Term: 0-2 Years; Mid-Term: 2-5 years; and Long-Term: > 5 years;

Ongoing: occurring annually over development period.

Source: Stanley R. Hoffman Associates, Inc.

2. The recommended steps and actions are contingent upon funding and allocation of resources.

A. Institutional Arrangements

Community Development Corporation. Established in 2015, the MLK Health and Wellness Community Development Corporation (MLK-CDC) aims to develop an “urban family-health campus” adjacent to the new MLK Community Hospital, starting with re-purposing the former King-Drew hospital building into the MLK Community Wellness Center that will house a variety of service providers, including mental health, family services, job training and creation, outpatient clinical, bio- science, physical wellness, and neighborhood place-making.

The Specific Plan recommends the expansion of the MLK-CDC to provide a broader coverage of the Plan Area. The expanded MLK-CDC would direct the visioning, formulation and implementation of economic development strategies/plans and offer an effective means to achieve Specific Plan goals. The expanded MLK-CDC would operate under the auspices of the County with stakeholder roles at the county and community levels through adequate representation in executive roles. The MLK-CDC could take on the critical function of directing and coordinating various actors and available resources in pursuing targeted economic development opportunities for real estate development and identifying infrastructure and services in coordination with other partners. The MLK-CDC could also play a central role in workforce development through skills gap assessments and through leveraging existing programs at the national, state and county levels and instituting new ones, as necessary.

Public-Private Partnerships. Public-private partnerships provide effective means in the current constrained economy to leverage limited resources and achieve mutually desired goals. Potential partnerships between local and regional public agencies include those with specialized institutions in education, health care and research and development such as the MLK Community Hospital and CDU. Partnerships with the private and non-profit sectors enable the integration of local workforce capabilities with the ultimate job growth. Expanding these partnerships will help achieve the Specific Plan goals.

Los Angeles County Workforce Collaborative. Initiative for a Competitive Inner City (ICIC) is underway to help coordinate national, state and county wide workforce training programs, and for targeted collaboration with key stakeholders, including the private sector. Continuing to build on the work of the Workforce Collaborative helps the Specific Plan goals of improved employment opportunities for Willowbrook residents.

B. Targeted Development Opportunities

Several specific and targeted development opportunities have been identified for Willowbrook under the Willowbrook TOD Specific Plan, including:

Leveraging Health Care and Education Opportunities. Ongoing collaboration with the MLK Community Hospital should be leveraged for spin-off activities in health-care services and allied office uses. Similarly, ongoing partnership with CDU has the potential to spur research and development activities in healthcare and science programs for employers who want or need to be part of the medical community around MLK Medical Center.

Kenneth Hahn Plaza. The Plaza offers an opportunity for enhancing the retail and dining experience in the community through store location and enhanced design and capital improvements; the shopping center also has the potential for mixed-use, transit-oriented development.

C. Land Use Planning

Land Assembly. Land assembly incentives become important, particularly in a post-redevelopment environment, where encouraging the consolidation of smaller parcels into a larger, developable area results in more efficient development. While land assembly incentives are limited in the Willowbrook area, one incentive is the expedited processing of development that furthers the goals and objectives of the Specific Plan vision.

Use of Successor Agency Parcels. The economic development strategy recommends the catalytic use of parcels inherited by the successor agency to the County redevelopment agency to further incentivize the Specific Plan goals and objectives.

Reconfiguring Publicly Owned Parcels. Land resources already owned by agencies should be assessed for reconfiguration and consolidation for new development opportunities. Local public schools in the Northwest Subarea of the Plan Area have the potential to be reconfigured to make additional land resources available for private sector development under either a lease or sale arrangement.

D. Implementation and Financing Strategy

Various financing and implementation methods are viewed as important in the long-term context of the Specific Plan vision for a health and wellness community that builds on MLK Medical Center and CDU investments and attracts both residential and non-residential development, preferably with a medical employment related focus. These actions, their primary uses, responsible parties for implementation and phasing are summarized in Table 7-2.

Some of the techniques need to start early to build resources for the future, such as an Enhanced Infrastructure Financing District (EIFD), and others may be implemented after some development has occurred, such as a Parking Management District or a Business Improvement District (BID) but are still considered important as part of the overall strategy. Some of the techniques are exclusively for capital financing or operations and maintenance, and some of these techniques can provide both funding approaches. The strategy also includes the early establishment of public-private partnerships that can foster confidence in the attractiveness and vision of the Plan Area.

Core Strategies. The core, early strategies include maximizing infrastructure financing through an EIFD and establishing a development impact fee program for essential infrastructure, such as parking or street improvements, that spreads the costs equitably over new development. Also considered important in the short-term is the expansion of the non-profit MLK-CDC to direct the visioning, formulation and implementation of Specific Plan goals and objectives. Additionally, part of the mid- to long-term strategy would be to create a parking management plan that could address both the long-term supply and operations side of the parking system, including unbundling the cost of parking from the underlying rent or lease payments.

Along with maximizing these funding sources, the CIP for the Specific Plan, as discussed conceptually in this Chapter 7, identifies top community priorities to facilitate timely provision of public infrastructure and services.

Grant Funding Opportunities. Ongoing grant funding opportunities at the local level include Cap-and-Trade under the Low Carbon Transit Operations Program and the Metro and Transportation Development Act (TDA) funding. The Metro and TDA grant opportunities are generally funded through

**Table 7-2:
Summary of Potential Financing Techniques**

Financing Methods	Primary Uses	Coordinating Agencies	Timeframe ¹
Development Impact Fee (DIF)	<ul style="list-style-type: none"> Capital improvements necessitated by new development. AB 1600 requires that a reasonable relationship or “nexus” must exist between the amount of the impact fee and the project on which it is imposed. 	County of Los Angeles	Short- to Mid- Term
Special Benefit Assessment	<ul style="list-style-type: none"> Capital improvements necessitated by new development. Assessments must be benefit based. 	County of Los Angeles	Mid- to Long- Term
Landscape Maintenance District (LMD)	<ul style="list-style-type: none"> Landscaping improvements. Landscaping operations and maintenance. LMD assessments must be benefit based. 	County of Los Angeles	Mid- to Long- Term
Mello-Roos Special Tax	<ul style="list-style-type: none"> Capital improvements. Operations and maintenance. Can finance projects and services with both local and general benefits. 	County of Los Angeles	Mid- to Long- Term
Business Improvement District (BID)	<ul style="list-style-type: none"> Marketing/promotional events/security/maintenance above existing public services; small scale capital improvements. Assessments must be benefit based. 	Property Owners/Business Owners	Mid- to Long- Term
Community Development Block Grant (CDBG)	<ul style="list-style-type: none"> Federal program that provides direct annual grants to cities, counties, and states across the country. Grants are intended to revitalize disadvantaged areas, expand affordable housing and economic opportunities. 	County of Los Angeles Department of Regional Planning (DRP)	Mid- to Long- Term
Enhanced Infrastructure Financing District (EIFD)	<ul style="list-style-type: none"> Use of County's future ad valorem property tax increment for financing public improvements and amenities. Requires a 55 percent vote of the electorate to incur bonded indebtedness; no increase in local property taxes. 	County of Los Angeles	Short- to Mid- Term
Community Revitalization and Investment Authorities (AB 2)	<ul style="list-style-type: none"> Similar to an EIFD, AB2 allows use of the County's property tax increment for capital financing. Subject to a majority protest vote and subject to certain conditions related to serving disadvantaged communities. 	County of Los Angeles	Short- to Mid- Term
Community Development Corporation (CDC)	<ul style="list-style-type: none"> Community development corporations (CDCs) are 501(c)(3) non-profit organizations for local areas. Community-based and provides economic development, education, community and affordable housing services. 	County of Los Angeles/Community-Based Stakeholders	Short- to Mid- Term

Table 7-2 (Continued)

Financing Methods	Primary Uses	Coordinating Agencies	Timeframe ¹
California Infrastructure and Economic Development Bank	<ul style="list-style-type: none"> Infrastructure State Revolving Fund (ISRF) Program provides economic development financing to agencies and non-profit corporations. Bank has broad authority to issue tax-exempt and taxable revenue bonds. 	Governor's Office of Business and Economic Development	Mid- to Long- Term
Parking Management District	<ul style="list-style-type: none"> Local districts that regulate parking supply and rates to meet the parking needs of the area. They can promote transit use, ridesharing, and alternative modes of transportation. 	Partnership between County of Los Angeles and Major Property Owners/Developers	Mid- to Long- Term
Cap-and-Trade (Pursuant to AB 32)	<ul style="list-style-type: none"> AB 32 authorizes the use of market mechanisms through an annual auction of surplus emissions reduction credits to generate funds, a portion of which can be targeted for clean transportation and sustainable community plans. The Low Carbon Transit Operations Program provides funding assistance for transit agencies to reduce GHG gas emissions and serve disadvantaged communities. 	Caltrans in coordination with the California Air Resources Board (CARB)	Ongoing applications for grant funding
Metro and Transportation Development Act (TDA)	<ul style="list-style-type: none"> Two percent of total TDA funds are allocated for bicycle and pedestrian projects. 	Metro	Bi-Annual Metro "Call for Projects" application process
Metro - Various Grant Programs	<ul style="list-style-type: none"> Congestion Mitigation and Air Quality (CMAQ) funds - bicycle, pedestrian and wayfinding projects. Metro Express Lanes Net Toll Revenue Re-Investment Grant Program - transit uses and roadway improvements. Metro Wayfinding Signage Grant Pilot Program. Metro Open Streets Program - regional car-free events. Surface Transportation Program - bicycle and pedestrian improvements and on-system roadway improvements. 	Metro	Ongoing applications for grant funding
California Capital Investment Incentive Program (CIIP)	<ul style="list-style-type: none"> Existing State program that provides property tax abatement for up to 15 consecutive years for large capital investment by qualified manufacturing facilities (must exceed \$150 million). 	County and Governor's Office of Business and Economic Development	
Other Outside Grants and Loans	<ul style="list-style-type: none"> Potential TOD grants from State's Strategic Growth Council (SSGC). SCAG Sustainable Communities Planning grants. Business façade improvement program grants. Affordable housing grants. 	DRP; SSGC; SCAG	Ongoing applications for grant funding

1. The timeline identified is a relative estimate in comparison to other actions included in the strategy.

Short-Term: 0-2 Years; Mid-Term: 2-5 years; and Long-Term: > 5 years;

Ongoing: occurring annually over development period.

local sales tax revenue measures on an annual or biannual cycle and have their own competitive application process. Other grant funding opportunities are also available at the federal, State and regional levels.

Additionally, programs such as the State of California's Infrastructure and Economic Development Bank (I-Bank) provides low-cost loans for qualified local programs; and Community Development Block Grant (CDBG) funds – while CDBG funds have been significantly reduced in recent years – still provide an ongoing source of funding to meet local priorities as part of the overall strategy.

Traditional Financing Techniques. Some of the more traditional financing techniques include: special benefit assessments, Mello-Roos special taxes and landscape lighting districts and are seen as mid- to long-term techniques as significant development begins to occur. The special tax and landscape lighting districts can fund both capital and operations and maintenance costs. Benefit assessments are subject to majority protest of the property owners, and special taxes are subject to 2/3 approval of the electorate.

A BID is a public/private partnership created to perform a variety of services to revitalize and improve commercial neighborhoods above existing local governmental services. Currently, there may not be enough business interest to form a BID, but over the mid- to long-term, interest may increase, particularly if businesses see this as a joint effort to efficiently use their collective funding to increase the quality and overall competitiveness of the area.

Emerging Techniques. Community Revitalization and Investment Authorities (CRIA), which was recently enacted into law, authorizes the revitalization of disadvantaged communities through planning and financing infrastructure improvements and upgrades; economic development activities; and affordable housing via tax increment financing based, in part, on the former community redevelopment law. A CRIA has some similarities to an EIFD, so both would not be implemented. The CRIA does have the power of eminent domain while the EIFD does not; also the CRIA has a number of restrictions that relate to serving disadvantaged communities and providing affordable housing. Finally, the County also has a California Capital Incentive Program (CCIP) that provides property tax abatement for up to 15 years for large capital investments by qualified manufacturing facilities (must exceed \$150 million); however, under an EIFD, this program would probably not be applicable as sufficient property tax increment revenues would likely not be available for both purposes.

7.5 Community Benefits

Community benefits from the implementation of the Economic Development Strategy are summarized below:

Identification and Coordination of Economic Opportunities. One of the pivotal benefits of public-private partnerships is the ongoing leveraging of opportunities that benefit firms/enterprises and the local workforce. A mechanism to implement workforce programs in response to private sector skill demands establishes an approach for realizing the potential for jobs growth and payroll increases within the community.

Capturing Benefits of Large Public Investments. Significant investments within Willowbrook in institutional and public transit development are expected to catalyze future growth in residential and non-residential uses over time. However, capturing and actually realizing these projected benefits depends upon programs in place, including those outlined above, and the establishment of the

EIFD for capital improvements for community recapture of fiscal benefits from increased property valuation. Appropriate land use zoning, such as mixed-use, could further help incentivize the potential around large public investments.

Expanded Business Activities and Local Job Opportunities. A combination of the various initiatives outlined above could result in expanded business activities in spin-off industries associated with health care services and research and development, and an expanded spillover demand for local services, which would also benefit the local community. Land use planning serves a critical role in providing locational opportunities for such activities. The community would benefit from the expansion of better paying jobs in coordination with appropriate skills development initiatives, as discussed previously.

Better Transit Access and Jobs-Housing Balance. Public transit investments additionally provide the benefit of improving two-way access to jobs and housing opportunities. The ability of workers to access job opportunities in other parts of the County has the benefit of increasing community prosperity. On the flip side, local firms' ability to access a qualified workforce from a larger commute shed is an important factor in locational choice in addition to availability of land. Expanded housing opportunities over time could have the stabilizing benefit of improving the match of jobs and housing within the sub-regional economy. Capital improvements connecting and improving local community access to transportation infrastructure is an important community benefit in this regard to realize the above economic outcomes.

Affordable Housing. With the loss of redevelopment agency affordable housing set-aside funds, the decline in available affordable housing funds has been substantial. The County, through its Housing Element, will continue to address existing and new sources of funding for affordable housing programs, such as new market housing tax credit programs and programs that are currently being considered by the California State legislature, but have not yet been adopted.

Enhancing Overall Quality of Life. An overarching community benefit of expanded jobs, access to transit, and improved local services, local public infrastructure, and public amenities will enhance local quality of life.

Increasing Public Safety. The proposed street and access improvements under this Specific Plan would also have the benefit of improving public safety. A neighborhood with expanded business activities, work opportunities, transit access, and housing opportunities would likely reinforce community identity and citizen participation in local events, resulting in greater social and economic cohesion.

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Capital Improvement Program

8.1 Introduction

The conceptual CIP is summarized in Table 8-1 and outlines the general categories of improvements, estimated costs, responsible agencies and phasing. The CIP sets forth an ideal scenario and establishes priority projects contingent on the availability of funding. The total estimated costs in this conceptual CIP scenario are about \$73.3 million. The bulk of these costs are for the Willowbrook/Rosa Parks Station relocation and improvements in the Kenneth Hahn Plaza by Metro. The phasing for this work is identified as short-to-mid range, depending upon when the funding resources become available.

The major local infrastructure and accessibility costs are estimated for Streetscape Improvements at about \$16.8 million, followed by about \$3.5 million for Transportation-related Improvements. These improvements will be overseen primarily by DPW and funded by a variety of other sources that are presented in this chapter. The phasing for this work is estimated for short-to-mid to long- range. Again, this work depends on the availability of funding, particularly if the newly adopted EIFD legislation is used; this is because the EIFD's bonding capacity will increase over time as property tax increment increases with new development.

Finally, Public Parking Facility Improvements are identified as possible incentives for cost reductions for private sector development within the northwest quadrant. However, no specific plans or garage cost estimates are available at this time. An option for full construction of a public garage might be cost sharing garage subsidies with the private sector. The phasing of these costs is seen as mid-to-long- range because it is assumed that it will take five years or longer to create public infrastructure and amenities to begin to draw private sector investment.

8.2 Potential CIP Improvements

A detailed listing of potential transportation and related improvements is presented in Table 8-2 along with an identification of responsible lead and partner agencies, potential funding sources and phasing. Table 8-3 presents the potential utility improvements by private or enterprise utility companies also with their responsible lead companies and partner agencies, potential funding sources and phasing.

**Table 8-1:
Summary of Capital improvement Plan Priorities**

Improvement	Estimated Cost	Coordinating Agencies	Timeframe ¹
Land Assembly ²	N/A	County and School Districts	Short- to Mid-Term
Transit-Related/Transit-Station Improvements	\$ 53,000,000	Metro ³	Short- to Mid-Term
Transportation Improvements	\$ 3,456,500	County of Los Angeles Dept. of Public Works (DPW)	Short- to Mid-Term
Streetscape Improvements	\$ 16,835,315	DPW	Short- to Mid-Term
Public Parking Garage Improvements/Subsidies	N/A	Potential Parking District ⁴	Mid- to Long-Term
TOTAL	\$ 73,291,815		

1. The timeline identified is a relative estimate in comparison to other improvements in the CIP.

Short-Term: 0-2 Years

Mid-Term: 2-5 Years

Long-Term: >5 Years

2. Potential land resources are available from the successor agency to redevelopment and from public school districts possibly reconfiguring their local campuses.

3. About \$10.25 million from the U.S. Department of Transportation (DOT) TIGER competitive grant funds is currently available; TIGER is an acronym for Transportation Investment Generating Economic Recovery.

4. A parking management district will incentivize private development through reduction in parking costs and Management of parking supply.

Source: Stanley R Hoffman Associates, Inc.

**Table 8-2:
Streetscape and Transportation: Improvements, Responsibility, Potential Funding Sources**

Improvements	Coordinating Agencies	Potential Funding Sources	Timeframe ¹
Transit-related Improvements			
Upgraded Metro Rosa Parks Transit Station	Metro	Metro	Short-Term
Vehicular/Pedestrian Improvements			
Road diet on Mona Boulevard	County of Los Angeles Department of Public Works (DPW)	<ul style="list-style-type: none"> • Metro Call for Projects • Cap and Trade • County of Los Angeles Transportation Funds • Federal and State grants and loans • Transportation Development Act Funds 	Mid-Term
Road diet on Willowbrook Avenue West (between the Metro Station and 119th Street)	DPW		Mid-Term
Traffic signal - intersection of Mona Blvd. and 119th Street - <i>Traffic signal analysis and subsequent implementation</i>	DPW		Short-Term
Traffic signal - intersection of Wilmington and 122nd Street - <i>Traffic signal analysis and subsequent implementation</i>	DPW		Mid-Term
Pedestrian signal - intersection of Mona Boulevard and 120th Street - <i>Pedestrian signal analysis and subsequent implementation</i>	DPW		Short-Term
Pedestrian sidewalk paving on the west side of Willowbrook Avenue West between Metro Station & 119th Street	DPW / Metro		Mid-Term
Pedestrian sidewalk improvements on Wilmington Avenue between Imperial Highway and 117th Street (both sides)	DPW		Mid-Term
Pedestrian sidewalk improvements along 119th Street between Wilmington Avenue and Willowbrook West (north side only)	Private development		Mid-Term
Pedestrian Oriented Improvements Package (Standard Improvement package) – 12 locations <i>Standard Improvement package includes:</i> - add zebra-style crosswalks to all crossings - add pedestrian countdown signals and audio signals to all crossings - add passive pedestrian detection to all crossings - add advanced stop bars to all approaches - add bulb-outs or reduce curb returns on corners where feasible	DPW		Mid-Term

Table 8-2 (Continued)

Bicycle Improvements			
Class I bicycle path and pedestrian trail on Mona Boulevard	DPW	• Metro • DPW • Cap and Trade • Federal and State Grants	Mid-Term
Class I bicycle path/Class IV cycle track on Willowbrook Avenue West	Metro/DPW		Short-Term
Class II bicycle lane on Imperial Highway between Compton Avenue and Mona Boulevard	DPW		Mid-Term
Class II bicycle lane on Wilmington Avenue between 120th and 124th Streets	DPW		Mid-Term
Class III bicycle route signage/stripping on Compton Avenue, 119th Street (between Wilmington & Mona), and Willowbrook Avenue West (south of 119th Street)	DPW		Mid-Term
Implement Bicycle Stations	MLK, CDU, Private Development		Short- to Mid- Term
Streetscape Enhancements			
Wilmington/119th/120th Streetscape Project (DPW-funded)	DPW	• Metro • DPW • Business Improvement District • Landscape and Lighting District	Short- to Mid-Term
Street trees on Specific Plan streets not included above	DPW		Long-Term
Pedestrian-scaled lighting on Specific Plan streets not included above	DPW		Long-Term
Street furniture	DPW		Mid-Term
Other Mobility Related Actions			
Community contest to rename 120th Street between Compton and Wilmington Avenues	DPW	• Metro • County of Los Angeles • CDC	Short-Term
Setup Transportation Demand Management Program	DPW		Short-Term
Parking			
Parking Management District	DPW	• County of Los Angeles, Enhanced Infrastructure Financing District • Future parking revenues	Mid-Term
	Property and Business Owners		

1. The timeline identified is a relative estimate in comparison to other identified improvements.

Timeline: Short-Term: 0-2 years; Mid-Term: 2-5 years; Long-Term: >5 years.

Source: Stanley R. Hoffman Associates, Inc., The Arroyo Group, The Mobility Group

Table 8-3:
Utilities: Improvements, Responsibility, Potential Funding Sources

Improvements	Oversight Responsibility - Lead and Partner	Potential Funding Sources	Phasing ¹
Evaluation of the need for water distribution lines upgrades in the Specific Plan area	Liberty Utilities, Golden State Water Company, and City of Los Angeles Dept. of Water and Power	Water Companies	Short- to Mid-Term
Detailed sewer study for Group 1	DPW and MLK Community Hospital	DPW through impact fees and connection charges	At time of development in Group 1
Other sewer related review by required by Sanitation Districts of Los Angeles County (LACSD)	Developers and LACSD	Development Impact Fees	Ongoing
Evaluation of demand for electrical services	<i>Primary responsibility:</i> Southern California Edison (SCE)	SCE through its utilities charges, as well as state and federal grants and	Ongoing
Evaluation of demand for telecommunication services	<i>Primary responsibility:</i> AT&T	AT&T funded through monthly phone charges	Ongoing
Evaluation of demand for cable services	<i>Primary responsibility:</i> Spectrum	Spectrum through its utilities charges, as well as state and federal grants and loans	Ongoing
Joint trenching with all utility providers	<i>Primary responsibility:</i> SCE <i>Secondary responsibility:</i> County of Los Angeles oversight and other private dry utilities companies	SCE in coordination with other utility providers	Ongoing

1. Short-term: 0-2 years.

Mid-term: 2-5 years.

Long-term: >5 years.

Source: Stanley R. Hoffman Associates, Inc., JMC² Civil Engineering



County of Los Angeles **WILLOWBROOK** TOD SPECIFIC PLAN

August 2018

Revised May 2023

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County of Los Angeles

WILLOWBROOK

TRANSIT-ORIENTED DISTRICT

SPECIFIC PLAN

August 2018
Revised May 2023



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Introduction

1.1 Specific Plan Location

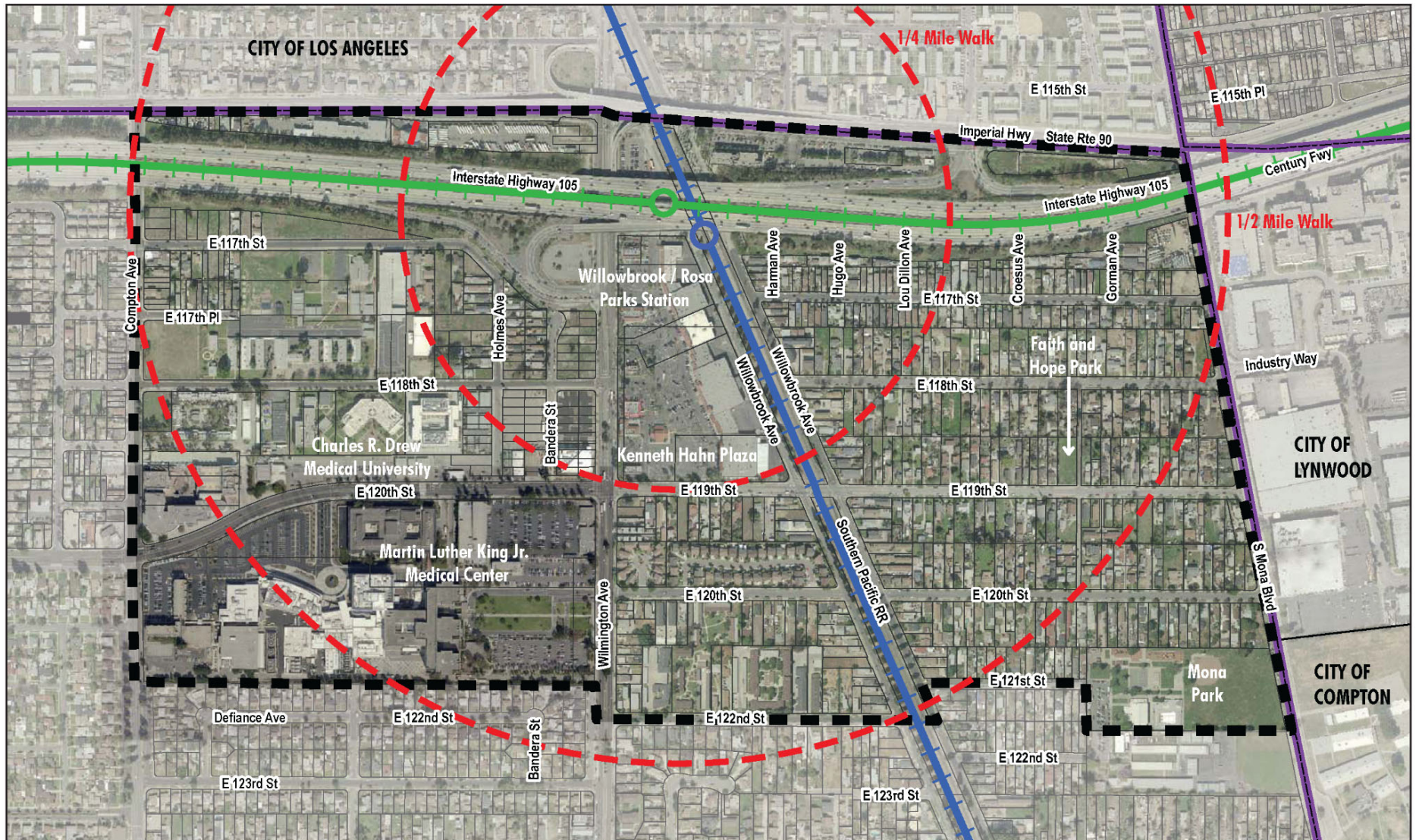
The Willowbrook Transit-Oriented District (TOD) Specific Plan (Willowbrook TOD Specific Plan or Specific Plan) area (Plan Area) is located in the unincorporated community of Willowbrook within the County of Los Angeles (County). It is located along the I-105 freeway and at the junction of the Metro A (formerly Blue) and C (formerly Green) Lines. The ~~project plan~~ area is approximately ten miles south of downtown Los Angeles and is bordered by the incorporated cities of Los Angeles to the north, and Lynwood and Compton to the east.

The Willowbrook TOD Specific Plan is focused around the Willowbrook/Rosa Parks Station, ~~which is~~ a transfer station ~~for on~~ the Metro Blue-A Line and Green-C Line (see Figure 1-1). The Plan Area is approximately 312 acres. Some of the key facilities ~~that are~~ located within the Plan Area are Martin Luther King, Jr. (MLK) Medical Center, Charles R. Drew University of Medicine and Science (CDU), Kenneth Hahn Plaza, Willowbrook Library, and MLK Center for Public Health.

***What is a Specific Plan?*¹**

A specific plan is a tool to systematically implement the General Plan within an identified project area. Specific plans are used to ensure that multiple property owners and developers adhere to a common plan or coordinate multiple phases of a long-term development. Specific plans must further the goals and policies of the General Plan. Specific plans must be consistent with the General Plan. No local public works project may be approved; no tentative map or parcel map for which a tentative map was not required may be approved, and no zoning ordinance may be adopted or amended within an area covered by a specific plan unless it is consistent with the adopted specific plan. The authority for preparation and adoption of specific plans is set forth in the California Government Code section 65450 et seq.

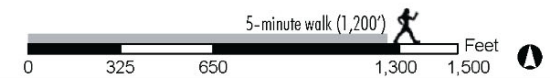
¹ Chapter 6: Land Use Element, Los Angeles County General Plan.



-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro C Line
-  Metro A Line
-  1/4 Mile and 1/2 Mile radius around Willowbrook/Rosa Parks Station

**Figure 1-1
Specific Plan Area**

WILLOWBROOK TOD SPECIFIC PLAN



1.2 Background

The Los Angeles County General Plan 2035 (General Plan) identifies TODs as priority policy areas throughout the County. To create vibrant and healthy TODs, the County envisions developing specific plans for all of the TODs that are located in the unincorporated areas.

The Willowbrook TOD Specific Plan ~~demonstrates~~ establishes a clear vision for the Willowbrook community, allows development that will lead to community benefits, and facilitates economic development. The Specific Plan is accompanied by a Program Environmental Impact Report (PEIR), which was prepared for the Specific Plan. The PEIR provides environmental clearance for specified levels of development intensity ~~and to~~ potentially streamlines future CEQA review.

1.3 Purpose

The purpose of the Willowbrook TOD Specific Plan is to ~~allow for revitalization of~~ revitalize the community within the project area and ~~encouraging improvement to improve of~~ access to all modes of transportation, including transit, walking, and bicycling. Consistent with the goals and policies outlined in the General Plan, the Willowbrook TOD Specific Plan will:

- encourage transit-oriented development,
- promote active transportation,
- allow development that reduces vehicles miles traveled,
- allow development that creates community benefits, and
- ~~streamline the environmental review process for future projects.~~

The Specific Plan is anticipated to facilitate development, especially residential and employment-generating uses, proximate to the Willowbrook/Rosa Parks Station. The primary objectives of the Specific Plan are to identify land use options that include mixed uses, increased housing opportunities, and neighborhood-serving retail uses. In addition, the Specific Plan is intended to foster a healthy community by improving pedestrian linkages between the Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, MLK Medical Center, CDU, future mixed-use areas, and existing residential neighborhoods; ~~as well as improving the public realm.~~

1.4 Relevant Studies and Plans

The following recent and ongoing planning projects have provided direction and established a foundation for the Willowbrook TOD Specific Plan:

- Willowbrook Area Access Improvements, County of Los Angeles Department of Public Works (2015-2017)
- Willowbrook Community Parks and Recreation Plan (2016)
- Los Angeles County General Plan 2035 (2015)
- Rosa Parks Station Improvements Planning, Metro (2014-2015)
- Los Angeles County Transit-Oriented Districts Access Study, Southern California Association of Governments (SCAG) and County of Los Angeles (2013)

- Implementation Plan for the MLK Medical Campus and the Willowbrook Wellness Community, Initiative for Inner City Competitiveness, Next Street and Genesis LA (2013)

- Martin Luther King Medical Center Campus Master Plan & the Willowbrook MLK Wellness Community Vision (2012)
- County of Los Angeles Bicycle Master Plan (2012)
- Rosa Parks Metro Station Master Plan & Transit-Oriented District (2010)
- Martin Luther King, Jr. Medical Center and Surrounding Project Area, Urban Land Institute (ULI) Technical Assistance Panel (2009)
- Imperial-Wilmington Rosa Parks Metro Blue Line Rail Station Non-Motorized Access Plan, City and County of Los Angeles, Metro, (2009)

1.5 Community Engagement

Prior to and throughout the planning process for the Willowbrook TOD Specific Plan, staff from the County of Los Angeles's Department of Regional Planning (DRP) conducted community outreach, and participation with the Willowbrook community. They met with and received engaging and receiving input from community groups and stakeholders including the following (in alphabetical order):

- Best Start Watts-Willowbrook
- Caltrans
- CDU
- City of Los Angeles
- City of Lynwood
- Commercial and residential developers of both market rate and affordable projects housing}
- Concerned Citizens of Willowbrook, Dr. Delores Glass
- Gabrielino Band of Mission Indians - Kizh Nation
- Housing Authority of the City of Los Angeles (HACLA)
- Kimco Realty Corporation
- Metro
- MLK Community Hospital
- Watts Century/Latino
- Watts Labor Community Action Committee

In addition, DRP staff met regularly with a County Project Task Force, comprised of representatives from various County departments, and received input and feedback about the goals and direction of the Specific Plan.

DRP staff met with representatives of the Gabrieleno Band of Mission Indians - Kizh Nation, to determine whether known tribal cultural resources are present in the Plan Area. While specific resources have not yet been identifieddiscovered, the Plan Area is proximate to a known early trade route that connected to the coast at San Pedro. Additionally, its characteristics of being relatively near historical water sources and hunting grounds would have made the Plan Areahave been favorable to supporting to settlementhabitation. As is common in Southern California, it is possible that artifacts with tribal significance could be discovered in the Plan Area in activities that involve ground disturbance. Therefore, these activities should be undertaken with care to adequately protect potential resources.

First NationsEarly Peoples

The project site is located within the territory of the native population known today as the Gabrieleno. Prior to European contact, the Gabrieleno occupied a diverse area that included the watersheds of the Los Angeles, San Gabriel, and Santa Ana rivers; the Los Angeles basin and foothills; and offshore islands of Santa Catalina, San Nicolas, and San Clemente. In ancient times the village areas adjoined and overlapped with each other, at least during the Late Prehistoric and Protohistoric periods where resources were shared by adjoining villages. The village lifestyle was focused around commerce with local and long distance trade a prominent function of the community. Resources were attained through a hunter-gatherer lifestyle while living in permanent communities located near the presence of water, a stable food supply, and some measure of protection from flooding. Houses were made of tule mats on a framework of willow poles. Basketry and steatite vessels were used rather than ceramics; ceramics became common only toward the end of the Mission Period in the nineteenth century. Villages in and around the project area exhibited similar organization and resource procurement strategies and were based on clan or lineage groups with their home base sites marked by midden deposits often with bedrock mortars and fire hearth locations. Average community populations generally ranged from 50-100 inhabitants, although larger settlements existed. Trade routes provided for daily travel, recreation, and commerce activities to occur over thousands of years. During seasonal rounds to exploit natural resources, small groups would move along trading routes within their traditional territory in search of specific plants, minerals, and animals. Their gathering strategies often left behind signs of special use sites at the locations of the resources. As well, ceremonial areas would contain buried caches of relics that were used for ceremony and would be buried and dug up every time a ceremony was conducted. These buried caches were stored in the earth much like a storage cabinet would be used in a home for protection from damage and the elements. However, there were also buried caches of relics that were never meant to be dug up and were placed in a specific orientation in the earth.

1.6 Specific Plan Goals and Policies

The goals and policies set the framework for the Willowbrook TOD Specific Plan. They serve as guidelines for decision-making and provide direction for future activities. The goals and policies were derived from input received from the community and stakeholders during the community engagement process, County Project Task Force, and DRP staff during the planning process. The seven major goals identified for the Plan Area are as follows:

1. Preserve and enhance the character of the Willowbrook community.
2. Improve the Willowbrook/Rosa Parks Station and its environs.
3. Encourage transit-oriented development.
4. Provide affordable housing opportunities.
5. Promote active transportation and reduce vehicle miles traveled.
6. Improve quality of life for existing residents with improvements to the public realm.
7. Improve economic vitality and employment opportunities.

Goal 1: Preserve and enhance the character of the Willowbrook community.

Policy 1.1: Where appropriate, preserve the character of the residential neighborhoods.

Policy 1.2: Provide a mix of land uses in the Mixed-Use Zones to accommodate employment, retail, and residential uses, as well as local-serving amenities.

Policy 1.3: Preserve existing neighborhood-serving retail at Kenneth Hahn Plaza.

Policy 1.4: Improve compliance with County zoning and building codes on private property by expanding code enforcement by various County Departments.

Policy 1.5: Facilitate the expansion of the MLK Medical Center and CDU campus that is compatible and sensitive to the surrounding neighborhoods.

Policy 1.6: Coordinate with CDU to integrate the planned growth of the University's campus with the surrounding community, including creating pedestrian linkages and open space connections with other area institutions and the Willowbrook/Rosa Parks Station.

Policy 1.7: Implement the concepts and ideas contained in the *MLK Medical Center Campus Master Plan & the Willowbrook MLK Wellness Community Vision* by encouraging well-designed and continuous pedestrian paths and connections between the Willowbrook/Rosa Parks Station and the employment, campus, retailing, and residential areas.

Goal 2: Improve the Willowbrook/Rosa Parks Station and its environs.

Policy 2.1: Coordinate with Metro to enhance pedestrian and bicycle connections from the Willowbrook/Rosa Parks Station to the surrounding community.

Policy 2.2: Coordinate with Metro to enhance safety and circulation between the various transit modes at the Willowbrook/Rosa Parks Station to encourage transit use.

Policy 2.3: Work with Metro and Kenneth Hahn Plaza to create better connections and access to the surrounding employment, campus, retail, and residential areas.

Goal 3: Encourage transit-oriented development.

Policy 3.1: Provide a variety of housing choices within walking distance of the Willowbrook/Rosa Parks Station.

Policy 3.2: Implement mixed-use zoning in targeted areas to promote employment-generating uses proximate to housing and the Willowbrook/Rosa Parks Station.

Policy 3.3: Incentivize lot consolidation where appropriate to facilitate the development of cohesive projects in Mixed-Use Zones.

Goal 4: Provide affordable housing opportunities.

Policy 4.1: Preserve existing stock of affordable housing.

Policy 4.2: Promote housing affordability through diversification of housing choices (ownership, rental, single-family, multi-family) for varied income groups.

Goal 5: Promote active transportation and reduce vehicle miles traveled.

Policy 5.1: Provide a multi-modal transportation system of complete streets.

Policy 5.2: Enhance access to transit and the Willowbrook/Rosa Parks Station.

Policy 5.3: Provide a connected pedestrian and bicycle network that links Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, new mixed-use areas, CDU, MLK Medical Center and residential neighborhoods.

Policy 5.4: Facilitate mixed-use development that maximizes pedestrian connectivity and minimizes the need for vehicle travel.

Goal 6: Improve quality of life for existing residents with improvements to the public realm.

Policy 6.1: Enhance the public realm with street trees, street furniture, bicycle facilities, sidewalks and pedestrian paths.

Policy 6.2: Provide a consistent canopy of shade trees throughout the Specific Plan area to enhance pedestrian comfort.

Policy 6.3: Provide pedestrian-scaled lighting to improve safety and enhance pedestrian environment.

Policy 6.4: Encourage outdoor dining and seating areas and other pedestrian-friendly uses in mixed use areas.

Policy 6.5: Explore joint use agreements with schools to better utilize existing and future open space resources.

Policy 6.6: Require new development to provide public open space as a community benefit, as appropriate. Consider providing incentives to developers for such provisions.

Policy 6.7: Build pocket parks and community gardens on available County-owned vacant lots.

Goal 7: Improve economic vitality and employment opportunities.

Policy 7.1: Create economic opportunities for the Willowbrook community by fostering a complementary variety of employment, retail, residential, and institutional uses.

Policy 7.2: Build on the Willowbrook community’s economic base as a “healthcare cluster” by working with appropriate partners to provide workforce development opportunities for local residents.

Policy 7.3: Facilitate public-private partnerships to share responsibility for implementing this Specific Plan and achieving its goals.

Policy 7.4: Encourage a mix of national brand and local merchant businesses.

Policy 7.5: Efficiently manage the supply and demand of parking to accommodate customer, commuter, and resident parking, and encourage the use of shared parking where possible.

Land Use and Urban Design Framework

2.1 Introduction

The goal of the Land Use and Urban Design Framework is to develop the Plan Area as a system of cohesive and integrated uses, spaces, structures, and environments. A clear framework provides visitors and residents with an understanding of how to easily and safely find their way around and to efficiently identify uses and activities. Arrangement of land uses into identifiable subareas, improved streetscapes, connected open spaces, and enhanced landmarks will help make the Plan Area accessible and distinctive.

This Chapter describes the approach to land use and urban design that will promote and allow transformation of the Plan Area into a vibrant transit-oriented district, while strengthening its connections to the adjacent residential neighborhoods and the rest of the Willowbrook community. This chapter also presents the short-term and long-term land use strategies for the various institutions and facilities in the Plan Area, which will be important in achieving the community goals.

2.2 Land Use and Urban Design Framework

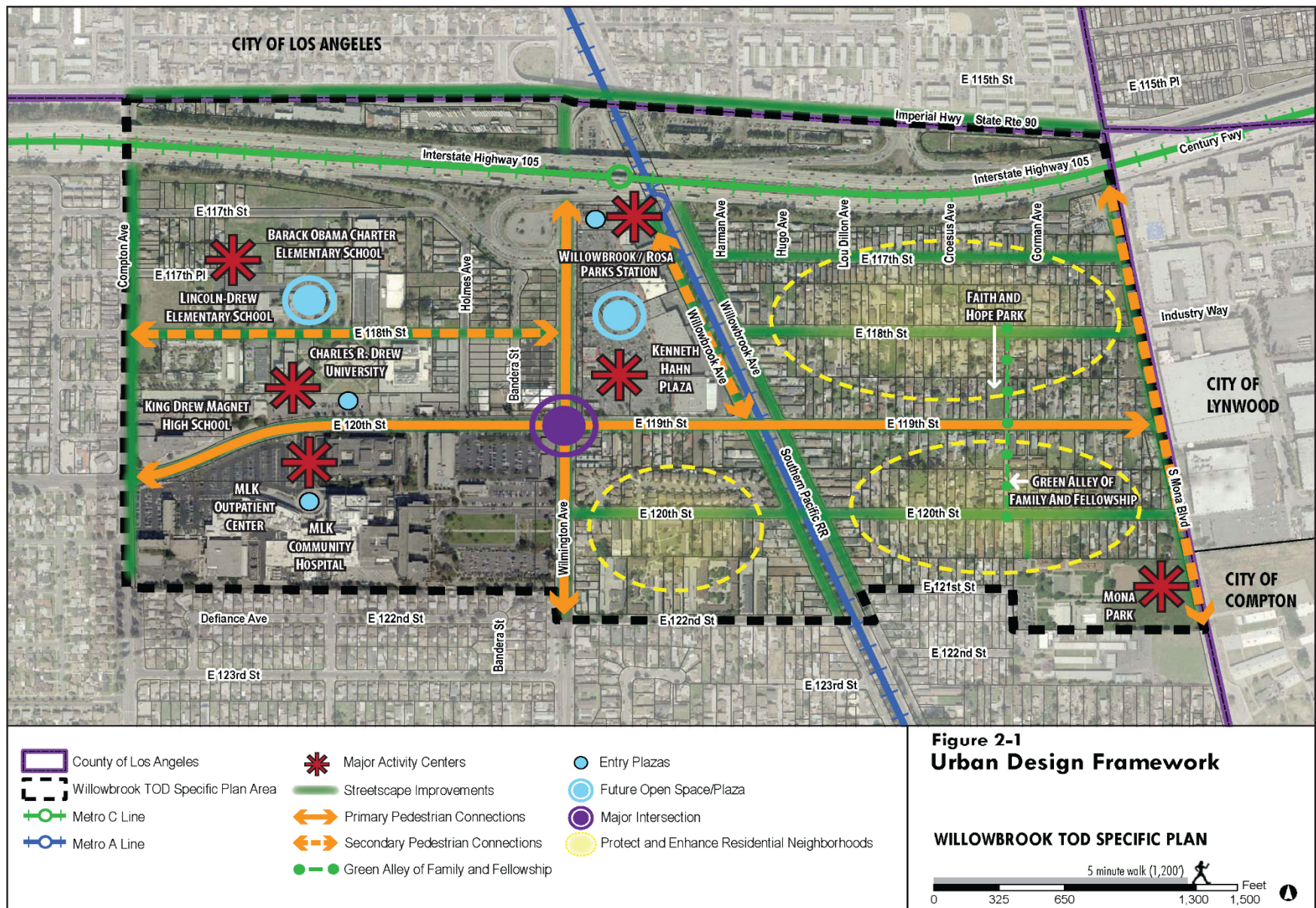
Elements of the Land Use and Urban Design Framework, as illustrated in Figure 2-1, include:

Specific Plan Subareas

The major activity centers in the Plan Area include the Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, MLK Medical Center campus, CDU campus, and Mona Park. Section 2.3 describes the distinct subareas into which the Plan Area can be divided. The existing uses in, and strategies for, planned and future uses in these subareas are also discussed in this chapter.

Mobility Improvements

Improvements to the vehicular, pedestrian, bicycle, and transit network that enhance connectivity and ease of movements for active transportation modes are further described in Chapter 4. Road diets for major pedestrian connections are further described in Chapter 4.



Pedestrian and Bicycle Connections

The Urban Design Framework establishes enhanced pedestrian and bicycle connections along the street network between the Willowbrook/Rosa Parks Station and the activity centers. Additional bicycle facilities, such as a Class I bicycle path or ~~Class IV~~ bikeway on Mona Boulevard, to complete the bicycle network within the Plan Area are also described in Chapter 4. Enhancements to the pedestrian experience such as pedestrian-oriented intersection improvements and wider sidewalks, where feasible, are also included.

Major Intersection – Wilmington Avenue and East 120th/East 119th Street

The two major streets within the Plan Area are Wilmington Avenue in the north-south direction and East 120th/East 119th Street in the east-west direction. These two streets connect the major activity centers to each other, as well as connect across the rail tracks with the residential neighborhoods to the east. They also intersect to form the pedestrian core or “heart” of the Plan Area. Design concepts for special paving treatments at this intersection are described in Chapter 5.

Streetscape and Open Spaces

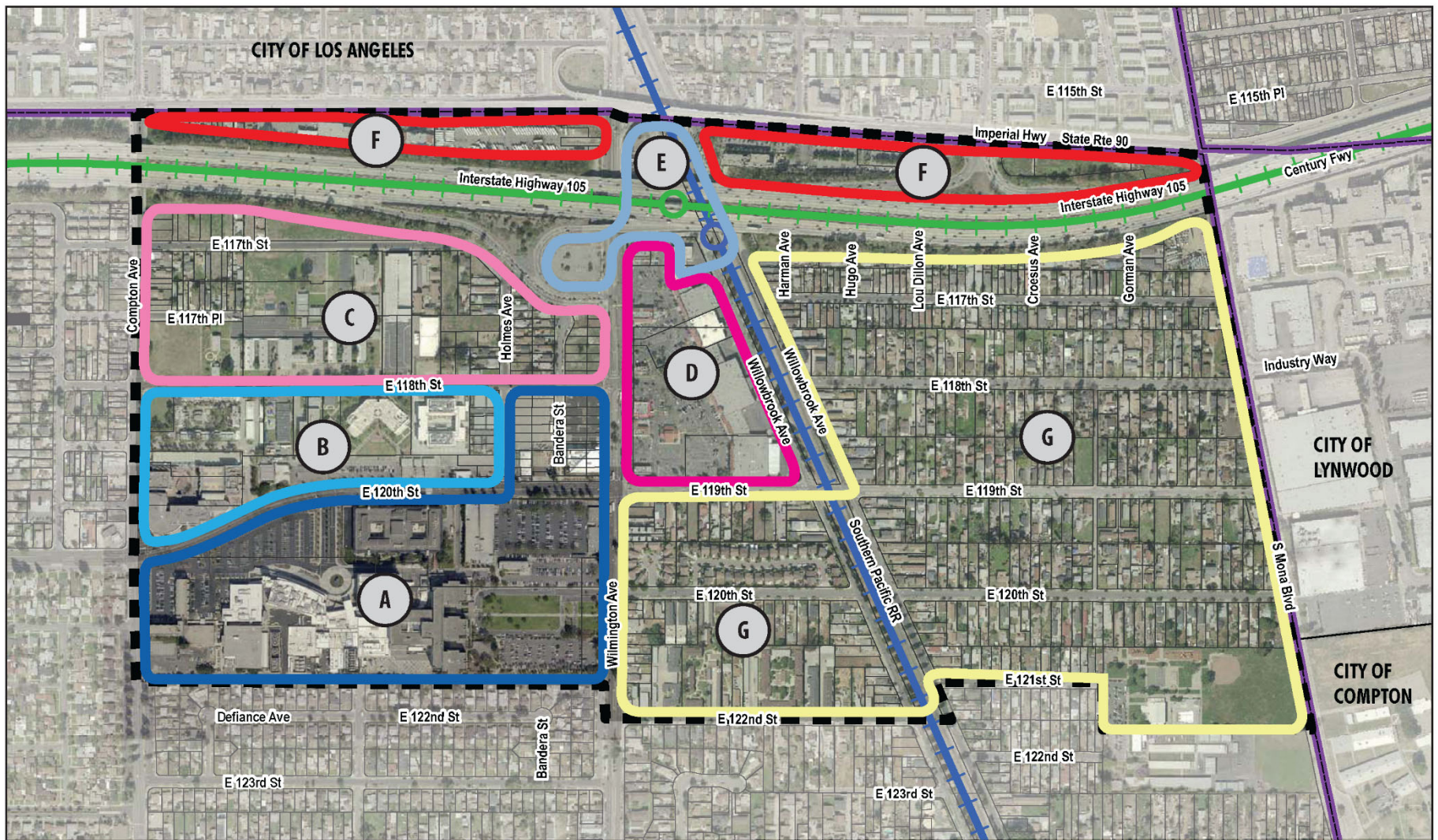
Another important element of the Urban Design Framework is improved streetscapes and connected open spaces. These streetscape improvements are intended to enhance and unify the visual and spatial experience of the driver, pedestrian, and the bicyclist, and help provide key linkages between the activity centers and the residential neighborhoods in the Plan Area. Recommended streetscape improvements consist of an interrelated palette of street trees, street furniture, street lighting, wayfinding signage and landscaping. These elements are described in Chapter 5.

Existing and planned open space resources in the Plan Area include Mona Park located along Mona Boulevard in the southeast corner of the Specific Plan area. Faith and Hope Park is located mid-block along East 119th Street and will be linked to Mona Park by the Green Alley of Family and Fellowship. ~~Another-Other~~ important open space resources ~~are-is~~ the MLK Fitness Garden, ~~primarily~~ located primarily along 120th Street at the MLK Center for Public Health and the proposed Fellowship Garden of Love at Holmes Community Garden that will be located at the corner of East 118th Street and Holmes Avenue.

The Specific Plan encourages the creation of additional public open space as a part of new or expanded mixed-use developments in the Plan Area.

Residential Neighborhoods

The preservation and enhancement of the existing residential neighborhoods is an important element of the Urban Design Framework. Key strategies include maintaining the density and character of the neighborhoods (Chapters 2 and 3) such that local equestrians will continue to feel comfortable riding their horses on residential streets and ensure that residents can continue to ~~maintain-keep~~ chickens ~~on their properties~~ and use outdoor clotheslines. Further strategies include, improving safety and comfort for navigating the street network (Chapter 4), and improving connections to the activity centers in the Plan Area (Chapter 5).



- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro C Line
- Metro A Line

- MLK Medical Center and Associated Facilities
- CDU Campus Area
- Northwest Subarea
- Kenneth Hahn Plaza

- Willowbrook/Rosa Parks Station
- Imperial Highway Corridor
- Residential Neighborhoods

**Figure 2-2
Specific Plan Subareas**

WILLOWBROOK TOD SPECIFIC PLAN



2.3 Specific Plan Subareas

The Plan Area supports a range of land uses, including residential, retail, office, and other commercial, as well as educational and institutional facilities and services. It can be divided into the following seven subareas as shown in Figure 2-2:

- MLK Medical Center and Associated Facilities
- CDU Campus Area
- Northwest Subarea
- Kenneth Hahn Plaza
- Willowbrook/Rosa Parks Station
- Imperial Highway Corridor
- Residential Neighborhoods

The uses in these subareas are complementary and supportive of each other. These subareas connect through the street network and a system of pedestrian walkways and open space areas. This section describes the existing conditions and the strategies for planned and future development within each subarea.

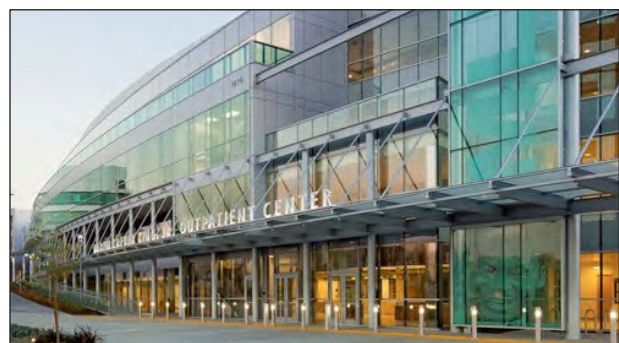
A. MLK Medical Center and Associated Facilities

Existing Conditions

The MLK Community Hospital reopened in 2015 after being closed in 2007. It is located on approximately 38 acres of land, and the campus is bounded by Wilmington Avenue to the east, East 120th Street to the north, Compton Avenue to the west, and the Specific Plan boundary to the south. The hospital is expected to serve 1.2 million residents in South Los Angeles, including Compton, Inglewood, Watts, Willowbrook, and Lynwood. It is also expected to create more than 1,800 jobs in the area with approximately 900 directly at the hospital.¹



Martin Luther King, Jr. Community Hospital



MLK Outpatient Center

Several other facilities serving the health care needs of the community are located adjacent to the hospital, including the MLK Center for Public Health, which focuses on preventative care, and the MLK Mental Health Center and ~~the~~ MLK Outpatient Center, all operated by the County. The MLK Outpatient Center operates more than 70 primary care and specialty care clinics to serve the health care needs of

residents in the surrounding communities. The MLK Center for Public Health includes the MLK Fitness Garden, ~~which is primarily~~ located primarily along the 120th Street frontage, ~~and that~~ incorporates a walking path and par course fitness equipment for employees and the public.



MLK Center for Public Health



MLK Fitness Garden

The County seeks to establish MLK Medical Center Campus as a center of excellence for health care delivery, urban health promotion and prevention, health workforce development, academic research and teaching, and economic development.² In 2009, the County commissioned the preparation of an EIR that analyzed the potential for significant environmental impacts in association with the proposed MLK Medical Center Campus Redevelopment Project (MLK EIR). The MLK EIR analyzes two tiers: Tier I,³ project-level development, and Tier II,⁴ program-level development. In addition, in 2009, the County Board of Supervisors found that the repurposing of the existing facilities for hospital use was categorically exempt from CEQA.

Following the certification of the MLK EIR, the County approved the *MLK Medical Center Campus Master Plan & The Willowbrook MLK Wellness Community Vision* document in 2012 as a guiding framework for future development, ~~which. It~~ calls for a coordinated system of pedestrian and bicycle circulation within the campus and the surrounding area.

Tier I development, as described in the MLK EIR, has been completed. As part of Tier II, a 50,000 square-foot medical office building and a ~~1,400-car~~ 1,400-car parking garage are also in the planning and design stages. Other projects in the pipeline include expansion/construction of two clinics focused on providing services for children. The Los Angeles County Department of Children and Family Services (DCFS)

² Martin Luther King, Jr. Medical Center Campus Redevelopment Draft EIR, August 2010.

³ Tier I analyzed the vacation of the emergency room, storage buildings, cooling towers, and existing Los Angeles County Martin Luther King, Jr. Multi-Service Ambulatory Care Center (MACC), and the development of two new environmentally sustainable buildings: the 132,000 square-foot new MACC and the 24,700 square-foot Ancillary Building. Tier I development consisted of approximately 170,332 square feet of new development. In addition, tenant improvements in existing buildings, site improvements, and potential relocation of the MRI Building was analyzed in Tier I.

⁴ Tier II analyzed a campus-wide Master Plan. It is anticipated that the development described in the Master Plan would prepare the proposed project site for future mixed-use campus-related development that would provide the health services necessary to respond to and address the needs of the community. Tier II would have the potential to build out approximately 1,814,696 square feet of development on the proposed project site with mixed uses including medical office, commercial, retail, office space, recreation, and other development in support of the campus. The net new development of the proposed project would be approximately 1,476,010 square feet. Tier II also analyzed the construction of up to 100 residential units, to be developed at a multi-family density consistent with surrounding residential area multi-family development densities. In addition, the Tier II components would entail the reuse or replacement of the existing MACC building, emergency room, storage building, and cooling towers.

operates the HUB Clinic, which provides mental health services to children and families who receive services through DCFS. Along with the Children's Behavioral Clinic, the two clinics will be sited directly to the north of the MLK Center for Public Health building, replacing the surface parking lot. Parking will be relocated in the block to the west.

Specific Plan Strategy

The Willowbrook TOD Specific Plan envisions that the MLK Medical Center campus will continue to grow and thrive as a regional destination for medical, health and associated services. The development on the campus will be broadly guided by the *MLK Medical Center Master Plan and the Willowbrook MLK Wellness Community Vision*. Required parking for the medical uses will be provided on campus. The Specific Plan also builds upon the recommendations for enhanced pedestrian connections between the MLK Medical Center campus and the other activity areas in the Plan Area.



MLK Medical Center Campus Master Plan & The Willowbrook MLK Wellness Community Vision (Source: Gensler).

B. CDU Campus Subarea

Existing Conditions

Immediately north of the MLK Medical Center campus are CDU, a private university, and King Drew Magnet High School, part of the Los Angeles Unified School District (LAUSD). These institutions are bounded by Holmes Avenue to the east, Compton Avenue to the west, East 120th Street to the south and East 118th Street to the north.

CDU has been in the Willowbrook community since the 1960s and is recognized by the Department of Education under Title III, Part B as a Historically Black Graduate Institution. CDU provides training in

medicine, medical technology, public health, and nursing under the auspices of three schools: [the](#) College of Medicine, [the](#) College of Science and Health, and [the](#) Mervyn M. Dymally School of Nursing.

Hospital-related occupations have been the focus of King Drew Magnet High School's curriculum since its establishment in the early 1980s. The Medicine and Science Careers Program, the centerpiece of the school's offerings, provides an opportunity for students to access experience-based learning in environments including hospitals, clinics, research laboratories, and university facilities. The school is located on the northeast corner of Compton Avenue and East 120th Street.

Other uses located within this area include a multi-family project on CDU-owned land on East 118th Street, several surface parking lots that serve CDU, and other County facilities in the area that are located along East 120th Street.



Charles R. Drew University of Medicine and Science and LAUSD King Drew Magnet High School are anchors in the CDU Campus Subarea.

Specific Plan Strategy

The Willowbrook TOD Specific Plan strategy includes supporting CDU's long range planning efforts. CDU is preparing a master plan that contemplates an increase in its student population from 625 [students](#) to 1450 [students](#) over a twenty to twenty-five year period. Today, CDU is primarily a commuter campus. The master plan includes new residential facilities (119 units) within the campus. The proposed housing types include dormitories for undergraduate students, shared housing for graduate students, and housing for visiting faculty. The master plan also provides conference facilities, incubator space, research labs, and additional student classrooms and facilities. The growth of the CDU campus will ~~contribute additional buying power of bring~~ the campus community (faculty, students, workers, etc.) [buying power to the Willowbrook community](#).

With MLK Community Hospital, MLK Center for Public Health, and other health-related facilities in the area, CDU and King Drew Magnet High School play an important role in fostering Willowbrook as a "health focused cluster" in South Los Angeles. The Willowbrook TOD Specific Plan reinforces the importance of these two facilities and builds upon the recommendations of CDU's master plan.

CDU's master plan aims to unify the campus into a cohesive setting with the creation of quad spaces and new buildings. The planned buildings are four to six stories in height. CDU's master plan expands the parking structure and parking lot located on East 118th Street to provide all required parking for the campus.

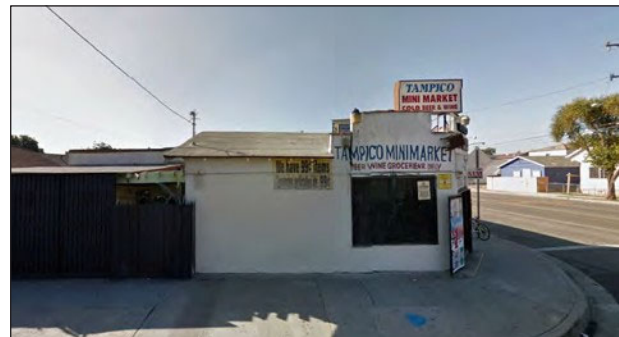
The Willowbrook TOD Specific Plan requires new campus buildings to be set back from the adjoining streets. To be responsive to the scale of the residential uses across Compton Avenue, new buildings along Compton Avenue are limited to four stories in height and ~~must are to~~ be set further away from the Compton Avenue property line. Architects and /designers for CDU must consider architectural cohesiveness in terms of materials and style and outdoor spaces as new buildings are designed and built. ~~The quad spaces and the required yards must be extensively landscaped.~~

CDU's master plan also includes a pedestrian, bicycle and shuttle circulation network system that connects with the activity centers in the Specific Plan area. Campus pedestrian and bicycle paths are connected with the public street network and activity centers in the vicinity, including the hospital to the south, and Willowbrook/Rosa Parks Station and Kenneth Hahn Plaza to the east. The CDU shuttle ~~system routes~~ also connects to these the activity centers ~~noted above and t.~~ There is an opportunity to consolidate the various shuttles in the area to improve shuttle routes and service frequency.

C. Northwest Subarea

Existing Conditions

The northwest subarea is an aggregation of a variety of uses – educational, retail, residential and institutional. Several vacant lots, owned by the ~~Community Development Commission of the County of Los Angeles~~ Los Angeles County Development Authority (LACDAC), are located along East 117th Street. A large vacant site on the northeast corner of East 118th Street and Compton Avenue is owned by the Compton Unified School District (Compton Unified). Educational uses include Lincoln-Drew Elementary School (a part of Compton Unified), and Barack Obama Charter Elementary School. Both are located north of East 118th Street. Parking facilities for CDU are located adjacent to the schools. Other uses in this subarea



Two elementary schools, CDU parking and a corner market are found in the Northwest Subarea.

~~this subarea~~ include a water well and reservoir operated by the Liberty Water Company. The residential uses are a mix of single-family homes, duplexes and multi-family structures. The block between Wilmington Avenue and Bandera Street, north of East 118th Street, is the site for The Gateway at Willowbrook, an affordable senior housing project that is currently under construction. The relocated ~~Los Angeles County~~ Willowbrook Public Library will be located on the ground floor of this development.

Specific Plan Strategy

The northwest subarea ~~has an advantageous location proximate to~~ is adjacent to the MLK Medical Center and CDU to the south and Willowbrook/Rosa Parks Station and Kenneth Hahn Plaza to the east. The Specific Plan envisions that over time, this subarea will capitalize on this proximity and become a mixed-use district with employment generating uses and residential infill.

The Work Force Collaborative, among other groups and studies, has suggested that the Willowbrook community become a “health cluster” with MLK Medical Center campus and CDU as the anchors, leading to numerous opportunities for health-related occupations at the two facilities. There is also opportunity for growth in related ~~health fields. The Specific Plan encourages employment generating uses~~ such as medical back office, laboratory facilities, hospital equipment facilities, and medical or university support businesses, ~~etc. that provide that would provide~~ job opportunities for ~~both~~ local residents ~~and others. The Specific Plan also envisions r~~Residential infill housing would increase ~~that provides~~ housing supply options for current and future workers, students, and residents in the area.

~~Theo achieve this long term vision, it is anticipated that the~~ Specific Plan will facilitate the is transition to more intensive uses. ~~However, several uses within this subarea need to remain or be re-accommodated. The CDU garage site will continue to serve the parking needs of the CDU campus. T whilehe the~~ existing single-family residential uses ~~ces~~ within the subarea may ~~will~~ continue as legally non-conforming ~~uses until they transition to other uses.~~

Lincoln-Drew Elementary School and Barack Obama Charter Elementary School

The two school sites could be consolidated and reconfigured for more efficient use of the land. The California Department of Education Guide to School Site Analysis states that infill school sites can be much smaller than ~~the previously typical recommended elementary school site size previously recommended of 10 acres in urban areas.~~ With the available LACD AC owned and Compton Unified-owned properties in the subarea and the partial vacation of East 117th Street, a 15-acre development parcel could be assembled with minimal acquisitions (See Figure 2-3). ~~(assuming a portion of the East 117th Street right of way is vacated — see discussion below).~~ ~~FA~~ five to six acres ~~portion of the assembled development parcel w~~ could be reserved for new school construction, with the remainder of the parcel sold available for development. The proceeds ~~from the sale~~ could be used to fund the reconstruction of the elementary and charter schools and a ny large scale development should provide a public open space for the community.

Commercial users would likely include corporations looking to associate with the new MLK Medical Center campus and/or CDU, and the parcel could be an ideal “back office” administrative location. The parcel is large enough to accommodate a business park, which could attract a variety of healthcare related office uses, or function as a regional administrative facility for a healthcare provider. A portion of this parcel could also be allocated to a healthcare/wellness incubator campus.

The development parcel would also be ~~very~~ attractive to several types of residential uses. The ~~development parcel's~~ location near CDU makes it an attractive location for university housing and its proximity to the Willowbrook/Rosa Parks Station makes it a prime location for work force housing and transit-oriented development.

This subarea will benefit from parking reductions in the Specific Plan and there is an opportunity to explore creating a parking management district. The LACDCA-owned properties between East 117th Street and the 105 Freeway can be the location for parking, while at the same time serving the additional purpose of providing a buffer between the freeway and new development.

its proximity to the Willowbrook/Rosa Parks Station makes it a prime location for transit-oriented development. Its location is also ideal for a sizeable workforce housing development.

If the LACDC-owned and Compton Unified-owned properties are aggregated, there is an opportunity to create an even larger consolidated development site by vacating a portion of East 117th Street that is west of the CDU garage site. It should be noted that any such vacation may only happen so long as there is no impact on access to any of the adjacent properties. Any new large-scale development should provide a public open space for the community.

This subarea will benefit from parking reductions in the Specific Plan. There is also the opportunity to explore the creation of a parking management district within the subarea. The LACDC-owned properties between East 117th Street and the 105 Freeway can be the location for parking, while at the same time serving the additional purpose of providing a buffer between the freeway and new development.

Examples of employment generating uses and multi-family residential developments from other communities are depicted on the following page.

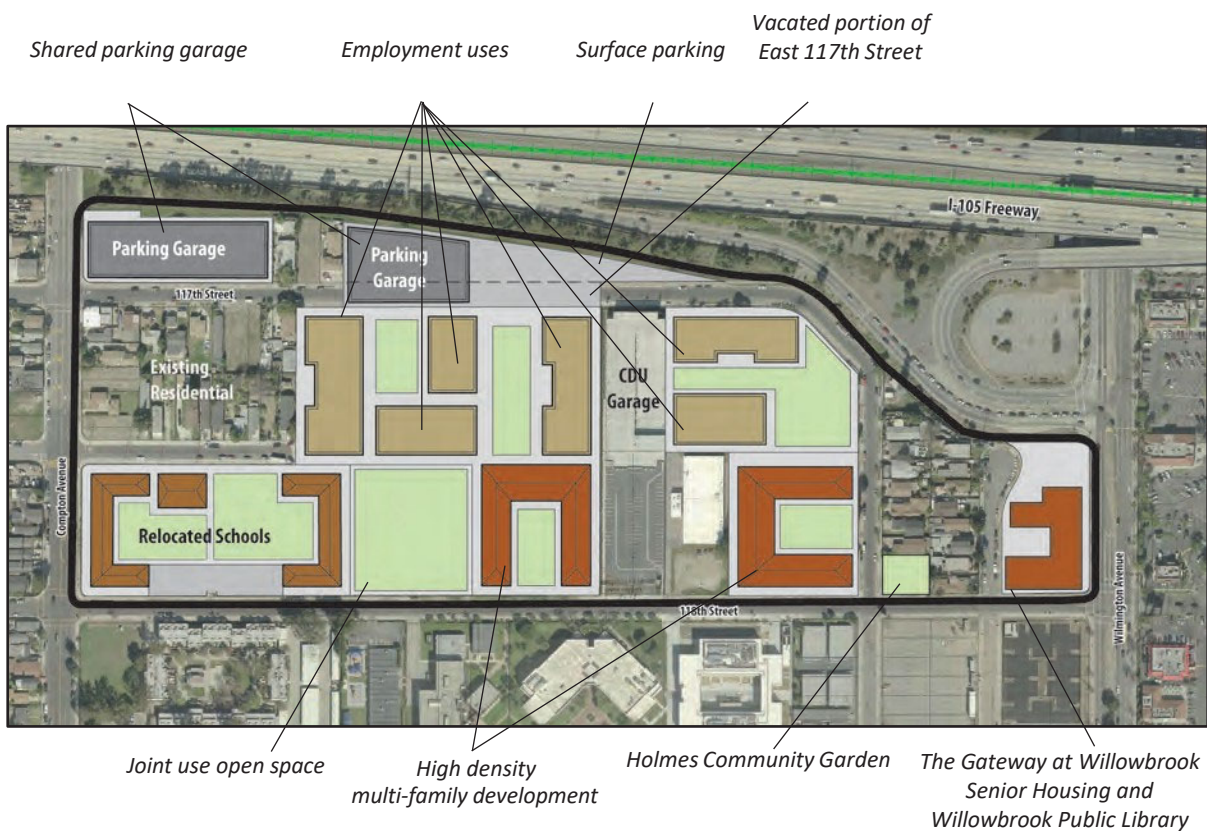


Figure 2-3: Illustrative Site Plan for the Northwest Subarea

This site plan shows one way the Northwest subarea could be developed under the development regulations and standards of the Specific Plan. Through lot consolidation and development of a unified project at higher densities, this subarea offers the potential for an employment-generating district with new residential opportunities. This plan is illustrative only; it is not intended to prescribe this particular site layout or use.



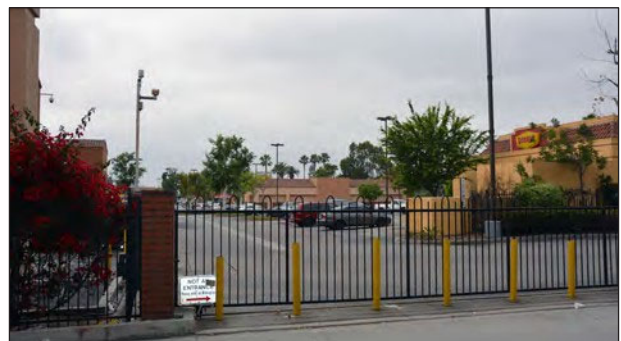
The Northwest subarea is envisioned to accommodate employment-generating uses in office and/or business park settings. Multi-family developments are anticipated to provide housing options for workforce, students, and transit-dependent users housing options.

D. Kenneth Hahn Plaza

Existing Conditions

Kenneth Hahn Plaza is a privately-owned shopping center on County-owned land. It is located immediately south of the Willowbrook/Rosa Parks Station, and is bounded by Wilmington Avenue to the west, East 119th Street to the south and Willowbrook Avenue to the east. The major anchor tenant is a Food 4 Less a grocery store. Other tenants include Rite-Aid, General Discount, and DaVita Dialysis Center. Food establishments in the center include McDonald's, Taco Bell, Pizza Hut, and Denny's, which is the only sit-down restaurant in the area. The Plaza is also the site of the Los Angeles County Willowbrook Public Library (soon to be relocated) and a Los Angeles County Sheriff substation.

The shopping center has a suburban layout with buildings sited at the rear and parking in front. Most of the fast-food restaurants are located in smaller structures adjacent to Wilmington Avenue. The shopping center presents an uninviting and blank facade towards East 119th Street, which has mostly single-family uses along the south side. The loading areas are located along Willowbrook Avenue. An internal service road between the buildings and rear property line provides access for trucks and other service vehicles. The entire site is surrounded by a six-foot tall wrought iron security fence blocking a pedestrian connection between the Willowbrook/Rosa Parks Station and the shopping center.



Kenneth Hahn Plaza currently has neighborhood serving retail as anchors. It is also isolated from its surroundings by fencing.

The Kenneth Hahn Plaza site is owned by the LACD [AC](#). Metro is acquiring approximately 1.5 acres of land on the northern end of the site for the expansion of the Willowbrook/Rosa Parks Station. The rest of the site is being purchased by the shopping center operator, Kimco Realty Corporation.

Specific Plan Strategy

The Kenneth Hahn Plaza site is the primary retail location within the Plan Area. While the site is a prime opportunity for redevelopment, there is also strong need and community desire ~~and a need~~ to retain a grocery store and other neighborhood serving retail in this location. ~~The Willowbrook TOD Specific Plan sets forth the long-term and short-term goals for this area.~~

Long-Term

Over the long-term, the Specific Plan envisions the Kenneth Hahn Plaza will transform to a mixed-use TOD project that takes advantage of the proximity of the Willowbrook/Rosa Parks Station. The MLK Medical Center Campus Master Plan, ~~ULI~~ (Urban Land Institute (ULI) Technical Advisory Panel study, and Rosa Park Station Study, all have also envisioned a mixed-use TOD project on the site. The Specific Plan ~~envisages that this envisions the~~ site ~~will provide~~ ing an integrated, complementary residential and commercial development ~~with a continued emphasis emphasizing on~~ neighborhood serving retail, restaurant and service uses ~~to serve local Willowbrook residents, employees, pedestrians and consumers.~~ Development can be integrated horizontally or d-vertically, with residential or office uses above retail uses, ~~or horizontally on site.~~



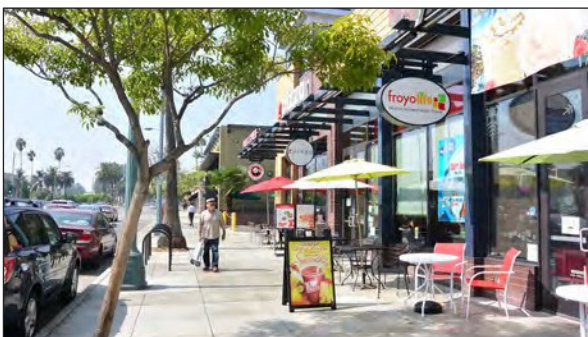
Over the long-term, new mixed-use buildings with neighborhood serving retail on the ground floor could transform Kenneth Hahn Plaza to a vibrant transit-oriented development.

Densities allowed per this Specific Plan are appropriate for both vertical and horizontal mixed-use projects. ~~Buildings may be~~ with buildings up to four stories tall. Residential uses must be designed to minimize the noise and vibration impacts of the rail traffic along Willowbrook Avenue. Parking may either be in consolidated lots, under podiums or in structures.

East 119th Street is the primary connection with the residential neighborhoods across the rail tracks within the Plan Area. As such, it is an extremely important connection that needs to be improved in terms of appearance, safety, visibility, and amenities. Infill uses along East 119th Street, at least on the north side, could either be storefronts with retail or service uses (child care, senior care, attorney offices etc.) or residential town-home/walk-ups ~~facing the single-family residential uses on the south side of the street~~. The massing and scale of these buildings will should be lower than the rest of the site as a transition to the ~~in light of the~~ single-family residential uses on the south side of East 119th Street. As development occurs along the north side of East 119th Street, a required greater setback will enlarge the public realm along this important pedestrian travel corridor.



Retail or service uses will activate the north side of East 119th Street, the primary east-west connection in the Specific Plan area.



New retail infill along Wilmington Avenue should activate the Wilmington frontage.

As a part of the Kenneth Hahn Plaza redevelopment, a large outdoor plaza or gathering space should be created similar to other successful shopping centers in the region. Such a space should have amenities such as street furniture, landscaped open space, public art, a water feature, and concessions. The site also has the potential for retail infill along Wilmington Avenue that will help create a more pedestrian friendly environment along that street.

~~As private development occurs on the Kenneth Hahn Plaza along the north side of East 119th Street, buildings will be required to be set back from the East 119th Street property line. This segment of East 119th Street is an important pedestrian travel corridor and is a connection to the residential neighborhoods to the east. Although privately maintained, this setback area could be improved with sidewalk paving and thus become a part of the~~

expanded sidewalk.

As a part of the site's redevelopment, a large outdoor plaza or gathering space should be created, similar to other successful shopping centers in the region. Such a space should have amenities such as street furniture, landscaped open space, public art, a water feature, and concessions.



The redevelopment of Kenneth Hahn Plaza should include a large outdoor plaza or gathering space.

The site also has the potential for retail infill along Wilmington Avenue while maintaining views to the existing retail on the site. Additional massing along the Wilmington frontage will also help create a more pedestrian friendly environment along that street.

Short-Term

In the short-term, it is likely that the Kenneth Hahn Plaza will continue as a shopping center that provides neighborhood-serving retail and service uses.

A better mix of retail tenants is needed to serve the shopping needs of Willowbrook area residents and employees, and visitors to the newly opened MLK Community Hospital and other existing facilities in the Plan Area. Active tenant recruitment will be necessary to improve the tenant mix at the Kenneth Hahn Plaza.

Physical upgrades and improvements that can improve the environment in the Kenneth Hahn Plaza include enhanced landscaping in the parking lot and along the periphery of the shopping center. Landscaping can also help create clearly identifiable paths of travel for pedestrians to navigate around the shopping center and minimize conflicts between the drive-thru lane entrances and pedestrian travel. The main entry drive on Wilmington Avenue should be improved with sidewalks on both sides of the driveway, better landscaping, and marked crosswalks for pedestrian safety.

Removal of the perimeter fence should be considered to better integrate the Kenneth Hall Plaza with surrounding uses. At a minimum, pedestrian access to the Willowbrook/Rosa Parks Station should be restored by removing the fence between the two facilities. Other improvements include better lighting in the parking lot and improved pedestrian linkages with the surrounding activity centers.

~~should be restored by removing the fence between the two facilities. Other improvements include better lighting in the parking lot, and improved pedestrian linkages with the surrounding activity centers, such as the hospital, CDU, mixed use areas, as well as the residential neighborhoods.~~

E. Willowbrook/Rosa Parks Station

Existing Conditions

The Plan Area has many options for public transportation, including bus service and the Willowbrook/Rosa Parks Station, which is a major transfer point between the Metro ~~Blue A Line~~ and ~~Green C Line~~ light rail lines. The ~~Green C Line~~ platform is located in the median of the ~~elevated I-105 Freeway (Glenn Anderson) Freeway above and the Blue A Line platform which is at street levelgrade, one level below.~~ In addition to the two rail lines, the ~~Willowbrook/Rosa Parks Station~~ is served by six Metro bus lines, ~~and as well as~~ several municipal bus lines and community shuttles.

A Metro public parking lot is located north of the I-105 freeway at Wilmington Avenue. Another Metro park-and-ride lot is located across Wilmington Avenue, within the off-ramp loop.

The physical environment at the Willowbrook/Rosa Parks Station needs major improvements. It is poorly lit ~~as well as~~, difficult to access and navigate, and ~~it is~~ poorly connected to its surrounding environment. ~~The Willowbrook/Rosa Parks Station~~ is blocked off from Kenneth Hahn Plaza by a fence, and access to the residential neighborhoods to the east of the tracks is limited. This station also has one of the highest crime rates in the Metro system.



The Willowbrook/Rosa Parks Station environment is inhospitable and unattractive.

Metro is working on the Willowbrook/Rosa Parks Station Improvement Project that is intended to solve many ~~of the above~~ identified issues. The design, still preliminary at the time of writing this Specific Plan, ~~proposes acquiring 1.5 acres from Kenneth Hahn Plaza, includes~~ relocating the ~~Blue A Line~~ platform ~~to the south,~~ restructuring the entry area ~~and creating better connections~~ for automobiles, buses, ~~bicyclists,~~ and pedestrians, ~~and creating better connections for all modes of transportation. Acquisition of approximately 1.5 acres of the Kenneth Hahn Plaza is a part of this Metro project.~~ The following improvements are included:

- Extend the ~~Blue A Line~~ platform to reduce crowding;
- Create a new southern at-grade pedestrian crossing ~~and entrance~~ to the ~~extended Blue A Line~~ platform;
- ~~Line platform;~~
- Upgrade the elevators, escalators, and stairs ~~to the Metro Blue and Green Line platforms;~~

- Improve the East Bus Depot to create a safer and more comfortable area for patrons;
- Reconfigure the West Bus Depot to relocate the bays from underneath the freeway;
- Reconfigure both Park & Ride lots for optimal circulation and pedestrian safety;
- Improve pedestrian circulation through the Station by creating a new Transit Hall;
- Add a new Sheriff's facility;



- ~~Improve the East Bus Depot to create a safer and more comfortable area for patrons;~~
- ~~Reconfigure the West Bus Depot to relocate the bays from underneath the freeway;~~
- ~~Reconfigure both Park & Ride lots for optimal circulation and pedestrian safety;~~
- ~~Improve pedestrian circulation through the Station by creating a new Transit Hall;~~
- ~~Add a new Sheriff's facility;~~
- Add a new Mobility Hub, including a Bike Hub;
- Add a new Metro Customer Service Center;
- Add new canopies on the extended **Blue-A** Line platform and at the open-air bus bays; and
- Improve lighting, landscaping, and signage throughout the site.



These images illustrate the reconceived entry plaza for the Willowbrook/Rosa Parks Station and its relationship with the rest of Kenneth Hahn Plaza. (Source: Metro)

Specific Plan Strategy

The Specific Plan supports the improvements planned at the Willowbrook/Rosa Parks Station. Several of the improvements ~~planned at the Willowbrook/Rosa Parks Station~~ are critical to improving pedestrian access and connectivity in the Plan Area. ~~However, additional improvements are necessary to meet the goals of the Specific Plan.~~

~~The at-grade rail right-of-way is a barrier between the residential e-eastern neighborhoods to the easts~~ and institutional uses to the west. Improvements to the at-grade crossing at the north end of the **Blue-A** Line platform ~~will better will greatly enhance pedestrian connectivityconnect to~~ the residential neighborhoods to the east of the ~~rail tracks— and complement the planned crossing~~ ~~Another at-grade crossing is planned~~ at the south end of the ~~Blue Line~~ platform.

Improvements to the intersection of Wilmington Avenue and the I-105 Freeway off-ramps are ~~also necessaryneeded~~ to improve access across Wilmington Avenue ~~and to the Metro parking lotPark & Ride lot in the on-ramp that is located in the ramp loop. These are included in the improvements and are~~ described in greater detail in Chapter 4 ~~of the Specific Plan.~~

~~Another Lastly, a~~ key improvement ~~in this Specific Plan iswill~~ enhanced bicycle access to the Station ~~—. Aa~~ Class I bicycle path or ~~Class IV~~ cycle track ~~is~~ planned along Willowbrook Avenue West between Imperial Highway and East 119th Street ~~will complete, completing~~ a missing connection ~~within the bicycle network in the Specific Plan area.~~ This stretch of Willowbrook Avenue West will also become a dedicated busway ~~instead of its current use as a public roadway. Coordination with Metro to ensure all improvements proposed for Willowbrook/Rosa Parks Station are implemented will be necessary.~~

F. Imperial Highway Corridor

Existing Conditions

Imperial Highway is one of the busiest east-west streets in Los Angeles County. It is generally three lanes in each direction ~~within the Plan Area and is also grade-separated on a viaduct with frontage access roads~~ through the central portion across crossing above Wilmington Avenue. ~~Frontage access roads provide access to some of the properties. The streetscape environment~~ It is wholly generally uninviting to pedestrians.

~~The~~ uses along Imperial Highway are generally a mix of auto repair, retail, residential, Metro facilities, and underutilized and vacant lots. A school bus parking lot and a Metro maintenance yard are located to the west of Wilmington Avenue. A barber shop and ~~auto shop~~ (towing yard) are located further west, near Compton Avenue. A couple ~~of~~ of new homes have been built recently in this section as well. Other Metro facilities, including a parking lot, are located to the east of Wilmington Avenue. A vacant site owned by the Housing Authority for the City of Los Angeles (HACLA) is also located along this segment of Imperial Highway. On the north side of Imperial Highway, in the City of Los Angeles, ~~is the~~ Imperial Courts, a public housing ~~project. While Imperial Courts is not in the Plan Area,~~ there is interest in creating a pedestrian connection across the street crossing to the HACLA site.

Specific Plan Strategy

The parcels ~~within the Plan Area~~ along Imperial Highway are sandwiched between the street and the I-105 Freeway. This corridor is suited to less intensive, commercial and service uses. Some of the existing uses, such as Metro maintenance yards and parking facilities, fall within this category. Other similar uses, such as self-service public storage facilities, ~~are permitted by the Specific Plan may be appropriate.~~ HACLA has tentative plans to ~~use their vacant parcels to house build~~ a WorkSource center and some of the area will serve, ~~as well as~~ temporary staging space during Metro's station improvements. Another use under consideration is ~~to house~~ a communications equipment building for high-speed Internet service for the City of Los Angeles.

If the WorkSource Center or another use that ~~is intended or likely to will~~ attract pedestrian traffic is proposed ~~in the Imperial Highway Corridor subarea,~~ pedestrian crossings on Imperial Highway, including new crosswalks where ~~ren~~ feasible, should be considered. ~~to facilitate pedestrians crossing Imperial Highway because there is~~ Currently, there are no crossings crossing between Mona Boulevard and Willowbrook Avenue. While new residential uses along this corridor are prohibited, the existing residential uses in the subarea may continue as non-conforming uses

~~While new residential uses along this corridor are prohibited, existing residential uses will be allowed to continue as non-conforming uses.~~



Low-intensity commercial and service uses are appropriate for the Imperial Highway corridor.

G. Residential Neighborhoods

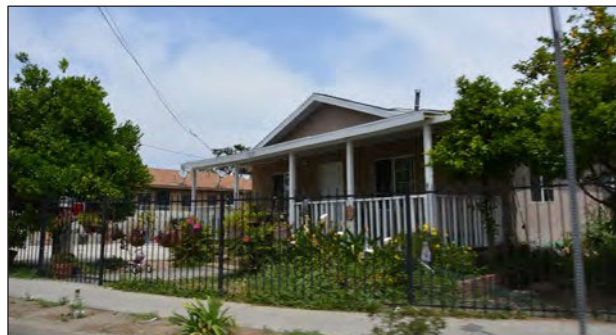
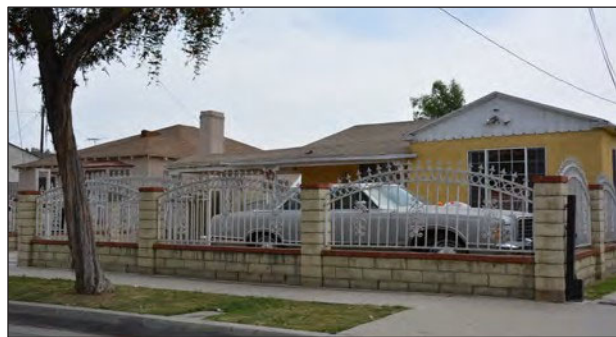
Existing Conditions

Residential uses within the Plan Area are a mix of single-family homes, two-family units, and apartments, condominiums, and townhouses. The residential area south of Kenneth Hahn Plaza, east of Wilmington Avenue and west of Willowbrook Avenue is primarily multi-family, with a mix of mostly two-story apartments and detached town-homes. The area bounded by Mona Boulevard, I-105 Freeway, Willowbrook Avenue, and East 121st Street has mostly one-story, single-family homes except along Willowbrook Avenue East fronting the Metro A Line tracks which is primarily multi-family. The homes are often fenced and have landscaped front yards. Parking vehicles in the front yard setback and on sidewalks as well as general property upkeep are identified challenges. Residential parcel configurations vary dramatically across the Plan Area. In a majority of the residential blocks, parcels are 90 feet wide and more than 200 feet deep. However, some of the parcels are as narrow as 30 feet wide and approximately 100 feet deep. Many of the larger parcels have two or more units, some built without approval or permits.

Most of these multi-family developments are two stories in height.

The area bounded by Mona Boulevard, I-105 Freeway, Willowbrook Avenue, and East 121st Street has mostly single-family uses, with the exception of the Willowbrook Avenue East frontage along the Metro Blue Line tracks. The latter is primarily multi-family uses. The single-family homes are predominantly single-story with an occasional two-story portion. The homes are fenced along the front property line. Many of the homes have landscaped front yards. Parking and property upkeep are identified problems, with vehicles often parked in the front setback and on sidewalks.

Residential parcel configurations vary dramatically across the Plan Area. In a majority of the residential blocks, parcels are 90 feet wide and more than 200 feet deep. However, some of the parcels are as narrow as 30 feet wide and approximately 100 feet deep. Many of the larger parcels have two (or more) units constructed on them, some illegally.



Single-family homes are the predominant use east of the rail tracks within the Willowbrook TOD Specific Plan area.

Some of the other ~~issues identified for the residential areas~~ challenges include illegal conversions/construction, lack of street amenities, and outdated provisions in the Willowbrook Community Standards District (CSD).

÷

Specific Plan Strategy

The Willowbrook TOD Specific Plan preserves and enhances existing residential uses in this area. In keeping with the community's desires, the Specific Plan does not increase base residential densities. The Specific Plan lays out the street improvements for the residential neighborhoods including a street tree program, pedestrian-scaled street lighting, and other street amenities. The Specific Plan also improves pedestrian and bicycle connectivity for the residents to the major activity centers in the Plan Area.

- ~~Illegal conversions/construction.~~
- ~~Lack of street amenities.~~
- ~~The Willowbrook Community Standards District (CSD) has several restrictions in place that need to be addressed.~~



Several important community churches are located in the Specific Plan area.

• This subarea includes an industrially zoned property at the northeastern corner of the residential neighborhoods on Mona Boulevard and I 105 Freeway. The site is just over an acre in size and is approximately 225 feet in depth. The current use is a commercial vehicle parking lot. The community has indicated a desire for better aesthetic screening and long-term conversion to residential use.

Specific Plan Strategy

The Willowbrook TOD Specific Plan preserves and enhances existing residential uses in this area. In keeping with the community's desires, the Specific Plan does not increase base residential densities. Specific Plan lays out the street improvements for the residential neighborhoods including a street tree program, pedestrian-scaled street lighting, and other street amenities. The Specific Plan also improves pedestrian and bicycle connectivity for the residents to the major activity centers in the Plan Area.

Industrial Property on Mona Boulevard

As described earlier, the only industrially zoned property in the Plan Area is currently being used as a truck parking lot, which is an incompatible use in a single- and two-family residential neighborhood. The Specific Plan rezones this property to residential in keeping with its surroundings. The current industrial use may continue as non-conforming per the provisions in Title 22 of the Los Angeles County Code (Title 22) and as noted in Chapter 3 of this Specific Plan. Per Chapter 22.172 (Nonconforming Uses, Buildings and Structures), a non-conforming use on an unimproved property shall terminate one year after a use becomes non-conforming. This Specific Plan extends the time limit for termination to five years from the adoption date of this Specific Plan.

Residential Street Improvements

~~The Specific Plan lays out the street improvements for the residential neighborhoods including a street tree program, pedestrian-scaled street lighting, and other street amenities. The Specific Plan also improves pedestrian and bicycle connectivity for the residents to the major activity centers in the Plan Area.~~

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~~Specific Plan Implementation~~

~~3.1 Introduction~~

~~This Chapter establishes the zones, use regulations, development and design standards for the Plan Area. The zones with accompanying use regulations, and development and design standards, are intended to provide property owners, business owners, developers, and their designers with basic development and design criteria that are intended to reinforce the desired building and district character.~~

~~This Chapter shall be used in conjunction with all other relevant adopted County plans and ordinances, including:~~

- ~~• Los Angeles County General Plan~~
- ~~• Los Angeles County Code~~

~~3.2 Applicability~~

~~The provisions of this Specific Plan shall apply to all properties included in the Plan Area. No construction, modification, addition, placement or installation of any building or structure shall occur, nor shall any new use commence on any lot, on or after the effective date of this Specific Plan, except in conformity with the provisions of the Willowbrook TOD Specific Plan and except as noted in section 3.2A, below.~~

~~A. Applications filed prior to Specific Plan Adoption~~

~~The following provisions shall apply to complete applications filed prior to the effective date of this Specific Plan and accompanying General Plan amendment.~~

~~The applicant may choose whether the application will be reviewed for consistency with the land use designations and zoning regulations in effect prior~~

~~to the adoption of this Specific Plan or with the land use designations and zoning consistent with this Specific Plan. In either case, approval of the application is not guaranteed.~~

~~If the applicant chooses to have the application reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan, the application~~

~~may be modified prior to consideration by the Los Angeles County Board of Supervisors, the Regional Planning Commission, the Hearing Officer or the Director, and still be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan as long as the requested modification does not:~~

- ~~• Change the project's housing type (e.g., from single-family to two-family or multi-family);~~
- ~~• Increase the project's residential density;~~
- ~~• Increase the project's floor area or lot coverage for non-residential space;~~
- ~~• Increase the amount of grading for the project; or~~
- ~~• Increase the area of ground disturbance resulting from the project.~~

~~Such a modification may necessitate submittal by the applicant of revised, updated, or additional materials, including, but not limited to, site plans, elevations, and oak tree reports. If the requested modification does not meet all of the criteria listed above, the modified project shall be reviewed for consistency with this Specific Plan and the General Plan.~~

~~An application for a modification to an approved but not used permit, that is valid on the effective date of this Specific Plan, where the modification will result in a project that substantially conforms with the previously approved project, as determined by the Director, may, at the election of the applicant, be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, an application for a modification to such permit shall be considered a new application and shall be reviewed for consistency with this Specific Plan and the General Plan.~~

~~If an approved permit has been used prior to the effective date of this Specific Plan and the permit contains a grant term, the approved use may continue until the end of the grant term, and, at the end of the grant term, shall be subject to this Specific Plan and General Plan policies in effect at that time. If, during the grant term, a request for a modification to the previously approved permit is made, and the modification will result in a use that substantially conforms with the previously approved permit, as determined by the Director, the modified permit shall be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, a request for a modification to the previously approved permit shall be subject to this Specific Plan and the General Plan policies in effect at~~

~~the time of filing the application for the modification.~~

~~If an approved permit has been used prior to the effective date of this Specific Plan, and the permit does not have a grant term, the approved use may continue indefinitely without regard to the policies in this Specific Plan. Notwithstanding the preceding sentence, all applicable non-conforming use provisions of Title 22 shall apply to the previously approved permit. If a request for a modification to the previously approved permit is made, and the modification will result in a use that substantially conforms with the previously approved permit, as determined by the Director, the modified permit shall be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, a request for a modification to the previously approved permit shall be subject to the Specific Plan and the General Plan policies in effect at the time of filing the application for the modification.~~

~~B. Consistency with Title 22 of the Los Angeles County Code~~

~~The Willowbrook TOD Specific Plan does not convey any rights not otherwise granted under the provisions and procedures contained in Los Angeles County Code, Title 22: Planning and Zoning (Title 22) and other applicable ordinances, except as specifically provided herein. Where general land use regulations and/or development standards of Title 22 are inconsistent with this Specific Plan, the standards and regulations of the Specific Plan shall prevail and supersede the applicable provisions of Title 22. The provisions of this Specific Plan shall also supersede the provisions of the Willowbrook Community Standards District (Chapter 22.352).~~

~~Any issue not specifically covered in the Specific Plan shall be subject to Title 22 and/or other titles of the Los Angeles County Code, or to interpretation by the Director if not specifically covered in the County's existing regulations.~~

~~3.3 Nonconforming Uses~~

~~Single-family residences located in zones that do not allow such uses may be continuously maintained as such, pursuant to the provisions of Chapter 22.172 (Nonconforming Uses, Buildings and Structures).~~

~~The existing legally established M-1 use on the parcel on the corner of Mona Boulevard and I-105 Freeway (Assessor Identification Number 6150-017-021) at the time of adoption of this Specific Plan may be continuously maintained as such, pursuant to the provisions of Chapter 22.172 (Nonconforming Uses, Buildings and Structures) of Title 22 with the following exception:~~

~~• The current use shall terminate five years after the adoption of the Specific Plan.~~

~~Any other use within the Plan Area that is nonconforming to the requirements and standards of this Specific Plan shall be subject to Chapter 22.172 (Nonconforming Uses, Buildings and Structures).~~

3.4 Specific Plan Zones

3.1 Specific Plan Zones

The Willowbrook TOD Specific Plan establishes zoning for parcels within the Specific Plan boundary as identified in Figure 3-1. The zones for the Plan Area are as follows:

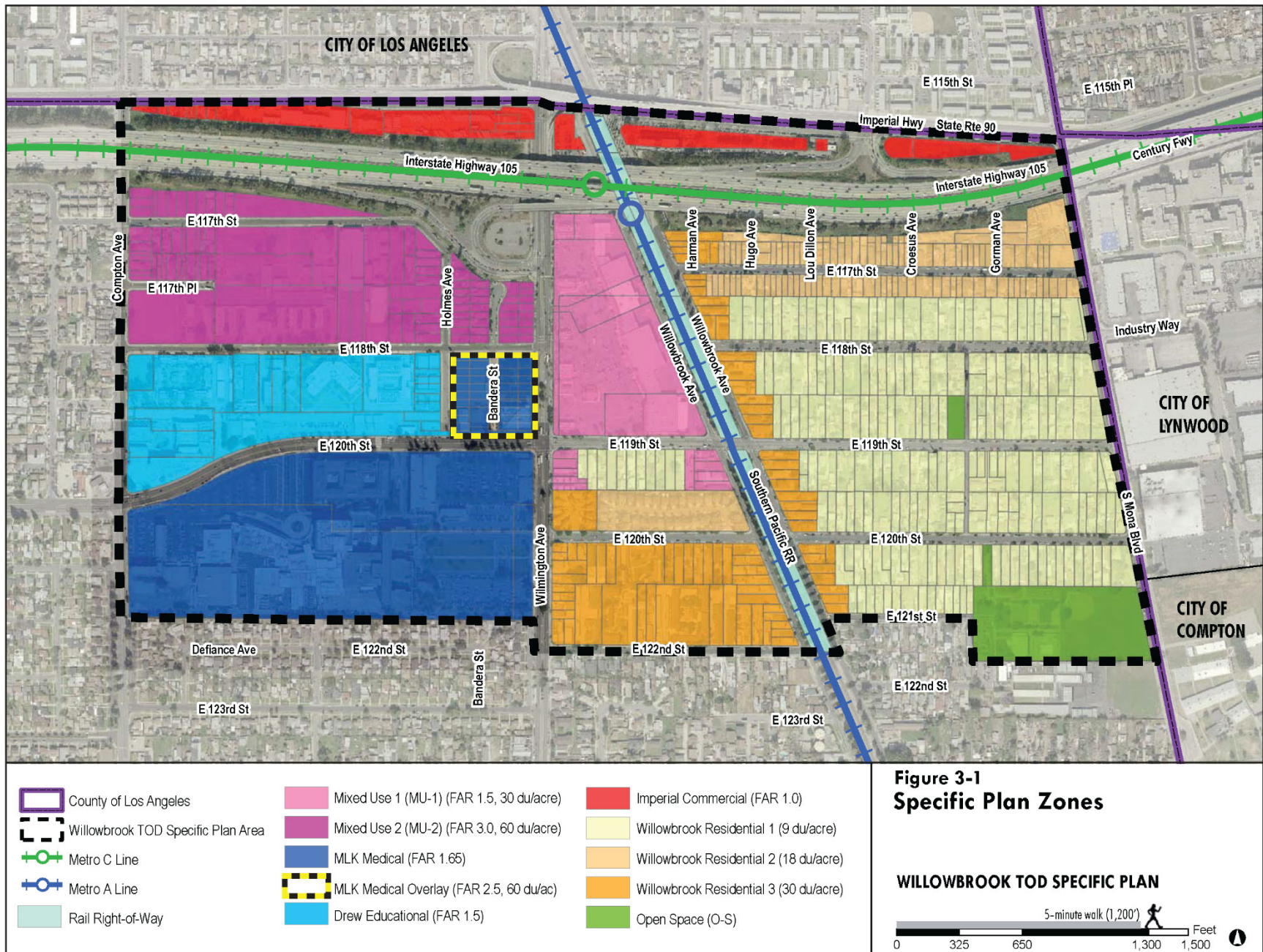
- ~~Mixed-Use 1 (MU-1)~~
- ~~Mixed-Use 2 (MU-2)~~
- ~~MLK Medical and Overlay~~
- ~~Drew Educational~~
- ~~Imperial Commercial~~
- ~~Willowbrook Residential 1~~
- ~~Willowbrook Residential 2~~
- ~~Willowbrook Residential 3~~
- ~~Open Space (O-S)~~

~~Please refer to Title 22, Planning and Zoning, Chapter 22.412 for The following sections of this Chapter identify the land use regulations and development and design standards for each Specific Plan zone. Parking requirements for uses in the Specific Plan are contained also in Section Chapter 3.1322.412. of this Chapter. Additional design guidelines are contained in Sections 3.2 14- 3.618 of —this Chapter. Note: Figure 3-1, Specific Plan Zones, is included herein for illustrative purposes only.~~

A. Mixed Use Zones

~~The Mixed Use Zones are established to promote walking, bicycling, recreation, transit use, and community reinvestment and are intended to facilitate integrated commercial and residential development through optimal site planning and efficient use of land.~~

~~**1. Mixed Use 1 (MU-1) Zone.** The Mixed Use 1 (MU-1) zone is intended to ~~provide~~encourage commercial and residential development, with an emphasis on neighborhood serving retail, restaurant, and service uses. The area is appropriate for a large retail or mixed use center, with a neighborhood plaza or community gathering space as a focal point and strong pedestrian connections to the Willowbrook/Rosa Parks Station, as well as the educational and medical campuses to the west.~~



3.5 Mixed Use Zones

A. Purpose

The Mixed Use Zones are established to promote walking, bicycling, recreation, transit use, and community reinvestment and are intended to facilitate integrated commercial and residential development through optimal site planning and efficient use of land.

1. Mixed Use 1 (MU-1) Zone. The Mixed Use 1 (MU-1) zone is intended to provide commercial and residential development, with an emphasis on neighborhood serving retail, restaurant, and service uses. The area is appropriate for a large retail or mixed use center, with a neighborhood plaza or community gathering space as a focal point and strong pedestrian connections to the Willowbrook/Rosa Parks Station, as well as the educational and medical campuses to the west.

2. Mixed Use 2 (MU-2) Zone. The Mixed Use 2 (MU-2) zone is intended to provide commercial and residential development, with an emphasis on employment-generating uses and residential infill development. The area is appropriate for office, business park, or mixed use developments, with open space components and pedestrian connections to the Willowbrook/Rosa Parks Station, and the educational and medical campuses to the south.

B. MLK Medical Zone and MLK Medical Overlay

The MLK Medical Zone and MLK Medical Overlay are established to meet the existing and future needs of the MLK Medical Center campus, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote medical, clinic, medical office, and associated supportive uses such as incidental retail, supportive residential and parking, and expand pedestrian linkages and connectivity between the MLK Medical Center, CDU, Willowbrook/Rosa Parks Station, and the Willowbrook community.

The MLK Medical Overlay, as identified in Figure 3-1, applies to the two blocks bounded by Wilmington Avenue, East 120th Street, Holmes Street and East 118th Street. The properties within this Overlay are suitable for more intensive uses because of their proximity to Willowbrook/Rosa Parks Station. Besides continuing to allow existing medical and public service uses, additional medical and new residential development are permitted on properties within this Overlay.

C. Drew Educational Zone

The Drew Educational Zone is established to meet the existing and future needs of CDU and King Drew Magnet High School, while ensuring compatibility with adjacent land uses. The intent is to create a medical university campus for CDU by maintaining and promoting educational and associated support uses, while maintaining sensitivity to surrounding development. This; zoning-e also- seeks expanded pedestrian linkages and connectivity between the CDU campus, MLK Medical Center, Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, and the Willowbrook community. should be consistent

D. Imperial Commercial Zone

The Imperial Commercial Zone is established to meet the commerce and service needs of the resident and business communities, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote commercial uses between Imperial Highway and the I-105 Freeway. The Imperial Commercial Zone provides for the development of a broad range of retail and service uses, as well as

freeway-oriented, regional-serving retail, office complexes, and light manufacturing businesses.

E. Willowbrook Residential 1 Zone

The Willowbrook Residential 1 Zone is established to preserve and enhance desirable characteristics of single-family residential areas.

F. Willowbrook Residential 2 Zone

The purpose of the Willowbrook Residential 2 Zone is to preserve and enhance single-family neighborhood characteristics while also providing opportunities for two-family residences. The intent is to promote desirable characteristics of low to medium density neighborhoods.

G. Willowbrook Residential 3 Zone

The Willowbrook Residential 3 Zone is established to provide opportunities for developments containing multiple units, such as apartments or condominiums. The intent is to promote desirable characteristics of medium density neighborhoods and provide a variety of housing options to serve the needs of the Willowbrook community.

H. Open Space (O-S) Zone

The provisions of Chapter 22.16 apply to all development in the Open Space (O-S) Zone.

B. Land Use Regulations

- ~~1. General.~~ This section prescribes the land use regulations for the Mixed Use Zones.
- ~~2. Permit and review requirements.~~ Table 3-1 identifies the permit or review required to establish each use.

**Table 3-1:
Permit and Review Requirements**

Abbreviation	Permit or Review Requirement	Reference
—	Not Permitted	
P	Permitted	
SPR	Ministerial Site Plan Review	Chapter 22.186
CUP	Conditional Use Permit	Chapter 22.158
SEP	Special Events Permit	Chapter 22.192

~~3. **Principal uses.** Table 3-2 identifies the principal uses and the permit or review required to establish each use. Additional regulations contained in Title 22 and/or this Specific Plan are also identified. Principal uses identified in Table 3-2 below may be established on a single site either as an integrated project, or as stand-alone use, subject to the provisions of this Chapter.~~

~~4. **Use requirements in Mixed Use 1 (MU-1) Zone.** The following use requirements apply in the MU-1 Zone:~~

- ~~a) Retail uses are required on the ground floor fronting Wilmington Avenue and East 119th Street.~~

~~Table 3-2:~~

~~Principal Use Regulations for Mixed Use Zones~~

[illegible]

Table 3-2 (continued)

Home improvement, retail sales and service-			

Day care			

~~Table 3-2 (continued)~~[illegible]

5. ~~Accessory uses. Table 3-3 identifies uses accessory to an established principal use listed in Table 3-2.~~

~~Table 3-3:~~

~~Accessory Use Regulations for Mixed Use Zones~~

~~6. Temporary uses. Table 3-4 identifies uses that are temporary and accessory to an established principal use listed in Table 3-2.~~

~~Table 3-4:
Temporary Use Regulations for Mixed Use Zones~~

~~7. Uses not listed. If a use is not specifically listed in Tables 3-2, 3-3 or 3-4, the Director shall have the authority to determine whether a use is permitted if that use is comparable to a particular use permitted in the zone.~~

~~8. Prohibited uses. Uses expressly prohibited in the Mixed Use Zones are as follows:~~

~~a) Bars and cocktails lounges.~~

~~b) Drive through establishments.~~

~~c) Liquor stores.~~

~~d) Pawn shops.~~

~~e) Tobacco and vape shops.~~

~~f) Vehicle related uses, including vehicle sales, rentals, storage, washing and service.~~

~~g) Alternative financial services.~~

~~C. Development and Design Standards~~

~~1. General. Table 3-5 identifies the development standards applicable to all development in the Mixed Use Zones. Additional regulations contained in Title 22 and/or this Specific Plan are also identified.~~

~~Table 3-5:~~

~~Development Standards for Mixed Uses~~

Lot Area – Minimum	1 acre	1 acre	For new lots only. Lots may be subdivided to
Residential Common Open Space	50 sf/du	50 sf/du for	Required for residential uses only. Minimum

~~2. Building height adjacent to residential zone or use. At residential adjacent locations, buildings shall maintain lower profiles to provide a transition between mixed use developments and residential zones and uses. The following standards apply:~~

~~a) New development in the MU-1 Zone fronting East 119th Street shall not exceed 40 feet and three stories for a depth of 50 feet from East 119th Street property line.~~

~~b) New development in the MU-2 Zone fronting Compton Avenue shall step back the fourth story of the building by a minimum of 10 feet.~~

~~c) New development sharing an interior property line with an existing single or two family residence shall step back the fourth story of the building by a minimum of 10 feet (see Figure 3-2).~~

Minimum Setback

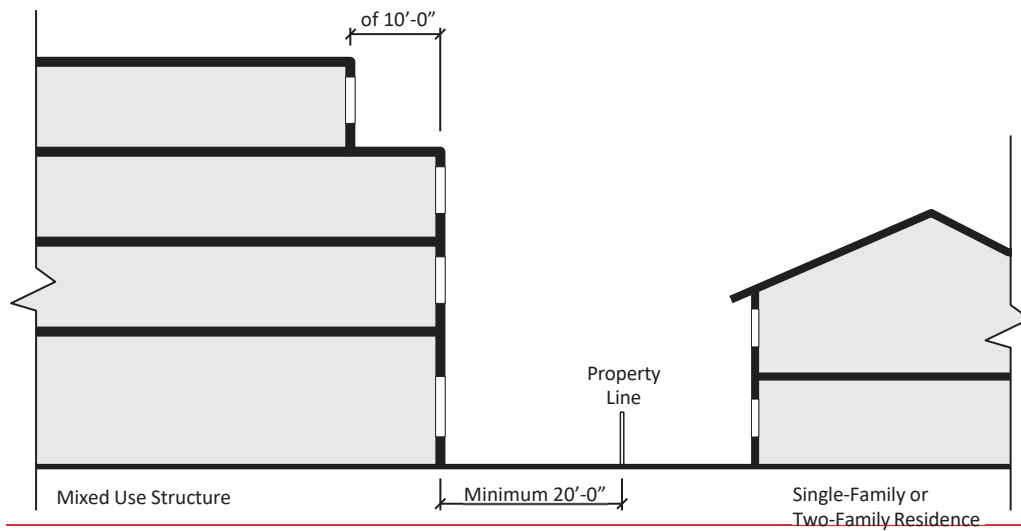


Figure 3-2: Building height step back adjacent to residential zone or use

3. Minimum transparency requirements.

a) Windows and openings facing streets shall constitute a minimum of 50 percent of street-level building facades, with the exception of residential units with individual entries, in which case windows and openings facing streets shall constitute a minimum of 30 percent of street level building facades.

b) On upper floors, windows and openings facing streets shall constitute a minimum of 40 percent of building facades for commercial uses, and 20 percent of building facades for residential uses.

4. Minimum floor-to-ceiling height. The floor-to-ceiling height of the ground floor for a street-facing retail component of a mixed-use development shall be a minimum of 15 feet.

~~5. Signs. The provisions of Chapter 22.114 (Signs) for signs in the C-3 Zone shall apply to all development in the Mixed Use Zones, with the following exceptions:~~

~~a) Roof signs shall be prohibited.~~

~~b) Signs on perimeter fences shall be prohibited.~~

~~6. Standards for outdoor dining. Outdoor dining is encouraged in the Mixed Use Zones and may be provided either in private patios or within the public sidewalk right-of-way. Standards for outdoor sidewalk dining are provided in Chapter 16.27 (Outdoor Sidewalk Dining), Los Angeles County Code, Title 16: Highways. The following standards apply to outdoor patio dining spaces.~~

~~a) Patio dining locations. Patio dining shall be permitted within private property adjacent to the streets or public alley walkways and shall be used exclusively for dining, drinking and circulation therein. Outdoor patio dining is allowed within the street yard area.~~

~~b) Patio Dining Coverage. Patio dining spaces may be open or covered with temporary structures such as trellises, umbrellas or permanent structures such as overhangs and upper floors of the building.~~

~~c) Demarcation of patio dining spaces. The patio dining spaces adjacent to streets or alley walkways shall be demarcated by either temporary or permanent boundary definers such as railings, fencing, or a combination of railings or fencing, and landscaping in planter boxes, or movable bollards.~~

~~d) Parking Requirements. The area used for patio dining shall be included in the eating or drinking establishment's area for the purposes of calculating the establishment's parking requirement.~~

~~e) Hours of operation. The outdoor dining space hours of operation shall be limited to the hours of operation of the associated dining establishment.~~

~~f) Building Code Awnings. All awnings shall conform to the requirements of Los Angeles County Code, Title 26: Building Code, related to roof coverings.~~

~~g) — Music. There shall be no amplified sound or music in the outdoor patio dining areas.~~

~~h) — Display of outdoor menus. A single-sided framed menu attached to a moveable barrier that defines the patio dining space is permitted within the patio dining space. The size of the frame shall not exceed three square feet. Freestanding pedestal menus, sandwich boards or A-frame displays are prohibited.~~

~~i) — Health and Safety Code. All applicable provisions of Los Angeles County Code, Title 11: Health and Safety shall be observed in all areas of the restaurant.~~

~~7. — Residential open space.~~

~~a) — The minimum required common and private open space area for residential uses is listed in Table 3-5. More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.~~

~~b) — Open space areas shall have no parking, driveway or right of way encroachments.~~

~~c) — Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy. Private open space areas that are intended for use by residents only shall not be accessible from the commercial portion of the mixed use development.~~

~~8. — Site landscaping.~~

~~a) — A minimum of 10 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.~~

~~b) — Chapter 22.126 (Tree Planting Requirements) establishes site tree requirements, including those for~~

~~surface parking lots.~~

~~c) The provisions of Section 4.106.5 (Landscape Design), Los Angeles County Code, Title 31: Green Building Standards Code (Title 31), shall also apply.~~

~~9. Pedestrian circulation.~~

- ~~a) Pedestrian walkways shall be a minimum of four feet in width.~~
- ~~b) The use of asphalt for paving walkways is prohibited.~~

~~10. Vehicular circulation.~~

~~Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, turning, or loading shall occur on site and not within the public right of way.~~

~~11. Parking lots.~~

- ~~a) Parking areas shall be designed with end stall turnarounds or a continuous circulation pattern.~~
- ~~b) Parking is not permitted in the required street fronting yard.~~

~~c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.~~

~~d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.~~

~~e) Separate parking facilities shall be provided for residential uses and commercial uses.~~

~~12. Fences, walls, gates and hedges.~~

~~a) Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:~~

~~• A minimum 2 foot change in horizontal plane for at least 10 feet.~~

~~• A minimum 18 inch change in height for at least 10 feet.~~

~~• A minimum 18 inch high raised planter for at least half the length of the wall.~~

~~• Use of pilasters at 25-foot maximum intervals and at changes in wall planes.~~

~~b) Section 22.110 (General Site Regulations) establishes the maximum height of fences, walls, and hedges.~~

~~13. Utility and mechanical equipment.~~

~~a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof~~

~~mounted equipment.~~

~~b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being “tacked on.”~~

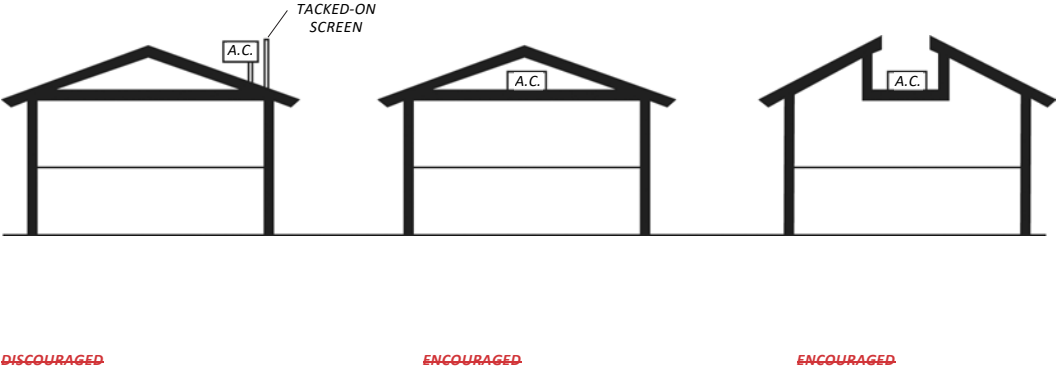


Figure 3-3: Equipment Screening

~~14. Refuse and recyclable collection facilities.~~

~~Separate refuse collection facilities shall be provided and maintained for the residential units in a mixed-use development from those for the commercial uses. It shall be clearly marked for residential use only.~~

~~D. Performance Standards~~

~~The purpose of this section is to ensure that residential uses in Mixed Use Zones are not adversely impacted by adjacent non-residential uses, including but not limited to traffic, noise, light, and safety impacts.~~

~~1. Hours of operation. The hours of operation for non-residential uses shall be no earlier than~~

~~6:00 a.m. and no later than 11:00 p.m. daily, unless modified by a conditional use permit.~~

~~2. Loading. Loading, unloading and all maintenance activities shall be conducted within the hours of operation noted above, and in such a fashion so as to prevent annoyance to adjacent residents and tenants.~~

~~3. Noise. Noise generated by activities on the premises shall be controlled in such a manner so as not to create a nuisance or hazard on any adjacent property, in accordance with Chapter 12.08 (Noise Control), Los Angeles County Code, Title 12: Environmental Protection. Residential units shall be constructed and designed to reduce the noise, particularly when located proximate to the Southern Pacific Railroad (SPRR), Metro Blue Line tracks, and Caltrans right-of-way. Proper design may include, but shall not be limited to, building orientation, double-paned windows, wall and ceiling insulation and orientation of vents. Common walls between residential and non-residential uses shall be constructed to minimize the transmission of noise and vibration.~~

~~4. Light and Glare. All outdoor lighting associated with non-residential uses adjacent to or within the immediate vicinity of residential uses shall be designated with fixtures and poles that illuminate non-residential uses, while minimizing light trespass into residential areas. An unacceptable level of light trespass shall be 0.8 foot candles or greater when the light trespass falls onto an adjoining residentially-zoned lot, or open space-zoned lot.~~

~~5. Operating activities prohibited. The following operating activities shall be prohibited in non-residential uses located within mixed-use developments:~~

~~a) Storage or shipping of flammable liquids or hazardous materials beyond that normally associated with a residential use; and~~

~~b) Welding, machining, or open flame work.~~

~~6. Graffiti. To encourage the maintenance of exterior walls free from graffiti that would impact pedestrian views, all structures, walls, and fences open to public view shall remain free of graffiti. In the event of such graffiti occurring, the property owner, tenant, or their agent shall remove or cover said graffiti within 72 hours, weather permitting. Paint utilized in covering such graffiti shall be a color that matches, as closely as possible, the color of the adjacent surfaces.~~

~~7. Security. The residential units shall be designed to ensure the security of residents through the provision of separate and secured entrances and exits that are directly accessible to secured parking areas. Where residential units are in the same structure as a non-residential use, access to residential units shall be from a secured area located on the first floor at the ground level. Non-residential and residential uses located on the same floor shall not have common entrance hallways or common balconies.~~

~~8. Outdoor Electrical Outlets. All new development shall have electrical outlets such that all landscaping equipment could be electrically operated.~~

3.6 MLK Medical Zone and MLK Medical Overlay

A. Purpose

The MLK Medical Zone and MLK Medical Overlay are established to meet the existing and future needs of the MLK Medical Center campus, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote medical, clinic, medical office, and associated supportive uses such as incidental retail, supportive residential and parking, and expand pedestrian linkages and connectivity between the MLK Medical Center, CDU, Willowbrook/Rosa Parks Station, and the Willowbrook community.

The MLK Medical Overlay, as identified in Figure 3-1, applies to the two blocks bounded by Wilmington Avenue, East 120th Street, Holmes Street and East 118th Street. The properties within this Overlay are suitable for more intensive uses because of their proximity to Willowbrook/Rosa Parks Station. Besides continuing to allow existing medical and public service uses, additional medical and new residential development are permitted on properties within this Overlay.

B. Land Use Regulations

The land use regulations for the Institutional (IT) Zone contained in Chapter 22.26 shall apply to all development in the MLK Medical Zone and Overlay with the following modifications:

- 1. Senior citizen housing development.** Senior citizen housing developments shall be permitted in the MLK Medical Zone and Overlay.
- 2. Multi-family residential uses.** Multi-family residential uses shall be permitted in the MLK Medical Overlay.

C. Development and Design Standards

The standards for the IT Zone contained in Chapter 22.26 and general site regulations contained in Chapter 22.110 shall apply to all development in the MLK Medical Zone and Overlay with the following modifications:

- 1. Maximum height.**
 - a) The maximum building height of multi-family residential buildings in the MLK Medical Overlay shall be 75 feet and six stories.
- 2. Maximum floor area ratio (FAR).**
 - a) The maximum FAR in the MLK Medical Zone shall be 1.65.
 - b) The maximum FAR in the MLK Medical Overlay shall be 2.5.
- 3. Maximum residential density.**
 - a) The maximum residential density in the MLK Medical Overlay shall be 60 dwelling units/acre.

D. Performance Standards

The purpose of this section is to ensure that residential uses in MLK Medical Zone and Overlay are not adversely impacted by adjacent medical and other non-residential uses. Performance Standards in Section 3.5.D shall apply to all development where both residential and non-residential uses are present.

3.7 Drew Educational Zone

A. Purpose

The Drew Educational Zone is established to meet the existing and future needs of CDU and King Drew Magnet High School, while ensuring compatibility with adjacent land uses. The intent is to create a medical university campus for CDU by maintaining and promoting educational and associated support uses, while maintaining sensitivity to surrounding development; as well as expand pedestrian linkages and connectivity between the CDU campus, MLK Medical Center, Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, and the Willowbrook community. The future CDU Master Plan shall comply with the regulations and standards contained in this Section 3.7.

B. Land Use Regulations

The land use regulations for the Institutional (IT) Zone contained in Chapter 22.26 shall apply to all development in the Drew Educational Zone with the following additions:

- 1. Student and faculty housing.** Student and faculty housing shall be permitted in the Drew Educational Zone.
- 2. Restaurants and supportive retail.** Restaurant and supportive retail uses shall be permitted in the Drew Educational Zone as an ancillary use providing necessary support to CDU's primary activities or operations.

C. Development and Design Standards

The standards for the IT Zone contained in Chapter 22.26 shall apply to all development in the Drew Educational Zone, with the following modifications:

- 1. Maximum height.**
The maximum building height shall be 75 feet and 6 stories. New development fronting Compton Avenue shall be limited to 50 feet and four stories for the first 100 feet from the property line fronting Compton Avenue. Development standards related to height per Chapter 22.04 and Chapter 22.110 of Title 22 shall also apply.
- 2. Maximum floor area ratio (FAR).**
The maximum FAR in the Drew Educational Zone shall be 1.5.
- 3. Street fronting yard depths.**
The minimum street fronting yard depths shall be 15 feet from Compton Avenue and 10 feet from East 118th Street, East 120th Street, and Holmes Avenue.
- 4. Required residential open space.**
 - a) The minimum open space area requirements for multi-family developments in the Drew Educational Zone shall be as follows:
 - i. 50 square feet/dwelling unit of common open space area with a minimum dimension of 25 feet; and
 - ii. 50 square feet/dwelling unit of private open space area with a minimum dimension of 5 feet.

- ~~b) More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.~~
- ~~c) Open space areas shall have no parking, driveway or right of way encroachments.~~
- ~~d) Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy.~~
- ~~e) Private open space areas that are intended for use by residents only shall not be accessible from the non-residential portion of the development.~~

~~5. Site landscaping.~~

- ~~a) A minimum of 20 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.~~
- ~~b) Chapter 22.126 (Tree Planting Requirements) establishes site tree requirements, including those for surface parking lots.~~
- ~~c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.~~

~~6. Pedestrian circulation.~~

- ~~a) Pedestrian walkways shall be a minimum of four feet in width.~~
- ~~b) The use of asphalt for paving walkways is prohibited.~~

~~7. Vehicular circulation.~~

~~Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, loading, or turning shall occur onsite and not within the public right of way.~~

~~8. Parking lots.~~

- ~~a) Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.~~
- ~~b) Parking is not permitted in the required street-fronting yard.~~
- ~~c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.~~
- ~~d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.~~
- ~~e) Separate parking facilities shall be provided for residential uses and institutional uses.~~

~~9. Fences, walls, gates, and hedges.~~

~~Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:~~

- ~~• A minimum 2-foot change in horizontal plane for at least 10 feet.~~
- ~~• A minimum 18-inch change in height for at least 10 feet.~~
- ~~• A minimum 18-inch high raised planter for at least half the length of the wall.~~
- ~~• Use of pilasters at 25-foot maximum intervals and at changes in wall planes.~~

~~10. Utility and mechanical equipment.~~

- ~~a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment.~~
- ~~b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."~~

3.8 Imperial Commercial Zone

A. Purpose

The Imperial Commercial Zone is established to meet the commerce and service needs of the resident and business communities, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote commercial uses between Imperial Highway and the I-105 Freeway. The Imperial Commercial Zone provides for the development of a broad range of retail and service uses, as well as freeway-oriented, regional-serving retail, office complexes, and light manufacturing businesses.

B. Land Use Regulations

The land use regulations for the Unlimited Commercial (C-3) Zone contained in Chapter 22.20 shall apply to all development in the Imperial Commercial Zone, with the following additions and exceptions:

- 1. Self-service storage facilities.** A self-service storage facility shall be permitted in the Imperial Commercial Zone with a Conditional Use Permit (CUP) application (Chapter 22.158), and subject to the provisions of Section 22.140.560.
- 2. Limitations on use.** The following uses shall not be permitted in the Imperial Commercial Zone:
 - a) Liquor stores.
 - b) Tobacco and vape shops.
 - c) New residential uses.

C. Development and Design Standards

The standards for the C-3 Zone contained in Chapter 22.20 shall apply to all new development in the Imperial Commercial Zone, with the following modifications:

- 1. Maximum lot coverage.**
The maximum lot coverage by structures of any type in the Imperial Commercial Zone shall be 50 percent.
- 2. Maximum height.**
The maximum building height in the Imperial Commercial Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.04 and Chapter 22.110 shall also apply.
- 3. Maximum floor area ratio (FAR).**
The maximum FAR in the Imperial Commercial Zone shall be 1.0.
- 4. Site landscaping.**
 - a) A minimum of 10 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.
 - b) Chapter 22.126 (Tree Planting Requirements) establishes site tree requirements, including those for surface parking lots.

~~e) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.~~

~~5. Pedestrian circulation.~~

- ~~a) Pedestrian walkways shall be a minimum of four feet in width.~~
- ~~b) The use of asphalt for paving walkways is prohibited.~~

~~6. Vehicular circulation.~~

~~Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, loading or turning shall occur on site and not within the public right of way.~~

~~7. Parking lots.~~

- ~~a) Parking areas shall be designed with end stall turnarounds or a continuous circulation pattern.~~
- ~~b) Parking is not permitted in the required street-fronting yard.~~
- ~~c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.~~
- ~~d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.~~

~~8. Utility and mechanical equipment.~~

- ~~a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment.~~
- ~~b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."~~

~~3.9 Willowbrook Residential 1 Zone~~

~~A. Purpose~~

~~The Willowbrook Residential 1 Zone is established to preserve and enhance desirable characteristics of single-family residential areas.~~

~~B. Land Use Regulations~~

~~The land use regulations for the Single-Family Residence (R-1) Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 1 Zone, with the following modification:~~

~~Noncommercial keeping of chickens.~~ ~~The noncommercial keeping of chickens shall be permitted as an accessory use in the Willowbrook Residential 1 Zone, subject to the following provisions:~~

- ~~a) No more than five chickens per residence are allowed, located within the rear yard and kept outside the dwelling unit.~~
- ~~b) All such chickens shall be kept in an enclosure within the rear yard and located a minimum of 35 feet from any habitable building.~~
- ~~c) Roosters shall not be permitted.~~
- ~~d) For the purposes of this section, the occasional sale of eggs or chickens, which is incidental to the keeping of chickens as a hobby, does not constitute a commercial use.~~

C. Development and Design Standards

The standards for the R-1 Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 1 Zone, with the following modifications:

1. ~~Maximum height.~~

~~The maximum height permitted in the Willowbrook Residential 1 Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.04 and Chapter 22.110 shall also apply.~~

2. ~~Minimum floor area.~~

~~The minimum floor area of a new single family residence in the Willowbrook Residential 1 Zone shall be 1,200 square feet.~~

3. ~~Driveways and walkways.~~

~~The use of asphalt for paving driveways and walkways is prohibited.~~

4. ~~Front yard landscaping.~~

- ~~a) With the exception of the required paved driveway and walkway having a width not to exceed four feet, all areas within the street fronting yard shall be landscaped and maintained.~~
- ~~b) Where possible, existing trees shall be maintained.~~
- ~~c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.~~

5. ~~Fences.~~

- ~~a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review.~~
- ~~b) Those portions of fences more than three and one half feet high must be substantially open (75 percent transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction.~~
- ~~c) Barbed wire and chain link fencing are prohibited.~~
- ~~d) Hanging, displaying or drying clothes on fencing is not permitted.~~

6. ~~Clotheslines.~~

~~Clotheslines or clothesline structures are permitted, provided they are located to the rear of the structure, and not visible from adjoining streets when viewed at ground level.~~

7. ~~Utility and mechanical equipment.~~

- ~~a) All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or they shall be screened from view. Satellite dishes shall also be located away from public view.~~
- ~~b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."~~

3.10 Willowbrook Residential 2 Zone

A. Purpose

The purpose of the Willowbrook Residential 2 Zone is to preserve and enhance single-family neighborhood characteristics, while also providing an environment suitable for two-family residences. The intent is to promote desirable characteristics of low to medium density neighborhoods.

B. Land Use Regulations

The land use regulations for the Two-Family Residence (R-2) Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 2 Zone.

C. Development and Design Standards

The standards for the R-2 Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 2 Zone, with the following modifications:

1. Maximum height.

The maximum height permitted in the Willowbrook Residential 2 Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.04 and Chapter 22.110 shall also apply.

2. Driveways and walkways.

The use of asphalt for paving driveways and walkways is prohibited.

3. Front yard landscaping.

- a) With the exception of the required paved driveway and walkway having a width not to exceed four feet, all areas within the street-fronting yard shall be landscaped and maintained.
- b) Where possible, existing trees shall be maintained.
- c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

4. Fences.

- a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review.
- b) Those portions of fences more than three and one-half feet high must be substantially open (75 percent transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction.
- c) Barbed wire and chain link fencing are prohibited.
- d) Hanging, displaying or drying clothes on fencing is not permitted.

5. Clotheslines.

Clotheslines or clothesline structures are permitted, provided they are located to the rear of the structure, and not visible from adjoining streets when viewed at ground level.

6. Utility and mechanical equipment.

- a) ~~All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or they shall be screened from view. Satellite dishes shall also be located away from public view.~~
- b) ~~Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."~~

3.11 Willowbrook Residential 3 Zone

A. Purpose

~~The Willowbrook Residential 3 Zone is established to provide opportunities for developments containing multiple units, such as apartments or condominiums. The intent is to promote desirable characteristics of medium density neighborhoods, and provide a variety of housing options to serve the needs of the Willowbrook community.~~

B. Land Use Regulations

~~The land use regulations for the Limited Multiple Residence (R-3) Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 3 Zone.~~

C. Development and Design Standards

~~The standards for the R-3 Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 3 Zone, with the following modifications:~~

1. Maximum height.

~~The maximum height permitted in the Willowbrook Residential 3 Zone shall be 35 feet and three stories. Development standards related to height per Chapter 22.04 and Chapter 22.110 shall also apply.~~

2. Required residential open space.

- a) ~~The minimum open space area requirements for multi-family developments in the Willowbrook Residential 3 Zone shall be as follows:~~
 - i. ~~50 square feet/dwelling unit of common open space area with a minimum dimension of 25 feet; and~~
 - ii. ~~50 square feet/dwelling unit of private open space area with a minimum dimension of 5 feet.~~
- b) ~~More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.~~
- c) ~~Open space areas shall have no parking, driveway or right-of-way encroachments.~~
- d) ~~Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy.~~

~~3. Site landscaping.~~

- ~~a) A minimum of 20 percent of the lot shall be landscaped or hardscaped with open, useable outdoor space. Required common open space areas may be counted in the calculated landscaped area.~~
- ~~b) With the exception of the required paved driveway and walkway(s), all areas within the street fronting yard shall be landscaped and maintained.~~
- ~~c) Chapter 22.126 (Tree Planting Requirements) establishes site tree requirements, including those for surface parking lots.~~
- ~~d) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.~~

~~4. Pedestrian circulation.~~

- ~~a) Pedestrian walkways shall be a minimum of four feet in width.~~
- ~~b) The use of asphalt for paving walkways is prohibited.~~

~~5. Vehicular circulation.~~

- ~~a) Vehicular access, drives and circulation routes shall be designed so that all movements involved in loading, parking, or turning shall occur onsite and not within the public right-of-way.~~
- ~~b) Principal vehicular access into multi-family developments shall be through an entry driveway, rather than a parking aisle. Entry driveways lead to designated parking and shall not be lined with or offer direct access to parking spaces.~~

~~6. Parking.~~

- ~~a) Parking for multi-family developments shall be provided on-site in on-grade or underground structures, surface parking lots, carports, or attached garages.~~
- ~~b) Parking shall be located proximate to the building and located to the rear of the parcel wherever possible. Parking shall be designed to avoid awkward turning maneuvers and the backing of vehicles into public streets. Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.~~
- ~~c) Parking shall not occupy more than 30 percent of any linear street frontage.~~
- ~~d) Parking is not permitted in the street fronting yard.~~
- ~~e) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.~~
- ~~f) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.~~
- ~~g) Carports and parking structures shall be architecturally compatible with the design of the main structures in the project.~~

~~7. Fences, walls, gates, and hedges.~~

- ~~a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review. Those portions of fences more than three and one-half feet high must be substantially open (75 percent transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction.~~
- ~~b) Barbed wire and chain link fencing are prohibited.~~
- ~~c) Hanging, displaying or drying clothes on fencing is not permitted.~~

- d) ~~Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:~~
- ~~• A minimum 2-foot change in horizontal plane for at least 10 feet.~~
 - ~~• A minimum 18-inch change in height for at least 10 feet.~~
 - ~~• A minimum 18-inch high raised planter for at least half the length of the wall.~~
 - ~~• Use of pilasters at 25-foot maximum intervals and at changes in wall planes.~~

~~8. Utility and mechanical equipment.~~

- a) ~~All utility and mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment. Satellite dishes shall also be located away from public view.~~
- b) ~~Screening elements shall be an integral part of the building; no screening method shall give the appearance of being “tacked on.”~~

~~9. Refuse and recyclable collection facilities.~~

~~Refuse enclosures shall be located for the convenience of the residents and be easily accessible for refuse and recyclables collection, but shall not impede circulation during loading operations to the extent feasible.~~

~~3.12 Open Space (O-S) Zone~~

~~A. General~~

~~The provisions of Chapter 22.16 shall apply to all development in the Open Space (O-S) Zone in the Plan Area.~~

~~3.13 Parking~~

~~A. Introduction~~

~~The Willowbrook TOD Specific Plan provides modifications to the parking requirements contained in Chapter 22.112. These modifications are intended to provide a parking supply that is more closely tailored to transit-oriented development and to allow greater flexibility in the provision of required parking.~~

~~B. Parking Requirements~~

~~The standards for parking requirements contained in Chapter 22.112 shall apply to all development in the Plan Area with the following modifications:~~

- ~~1. This Specific Plan establishes a TOD Parking Reduction Overlay Zone with reduced parking requirements and maximum parking standards. The boundaries for this zone are shown in Figure 3-4. Within the TOD Parking Reduction Overlay Zone, the following standards apply:~~
 - a) ~~**Residential uses.** Minimum and maximum parking standards for residential uses are established in Table 3-6.~~
 - b) ~~**Non-residential uses.** Maximum parking standards for non-residential uses are established in Chapter 22.112. Minimum parking standards for non-residential uses are 40 percent of the maximum requirement.~~

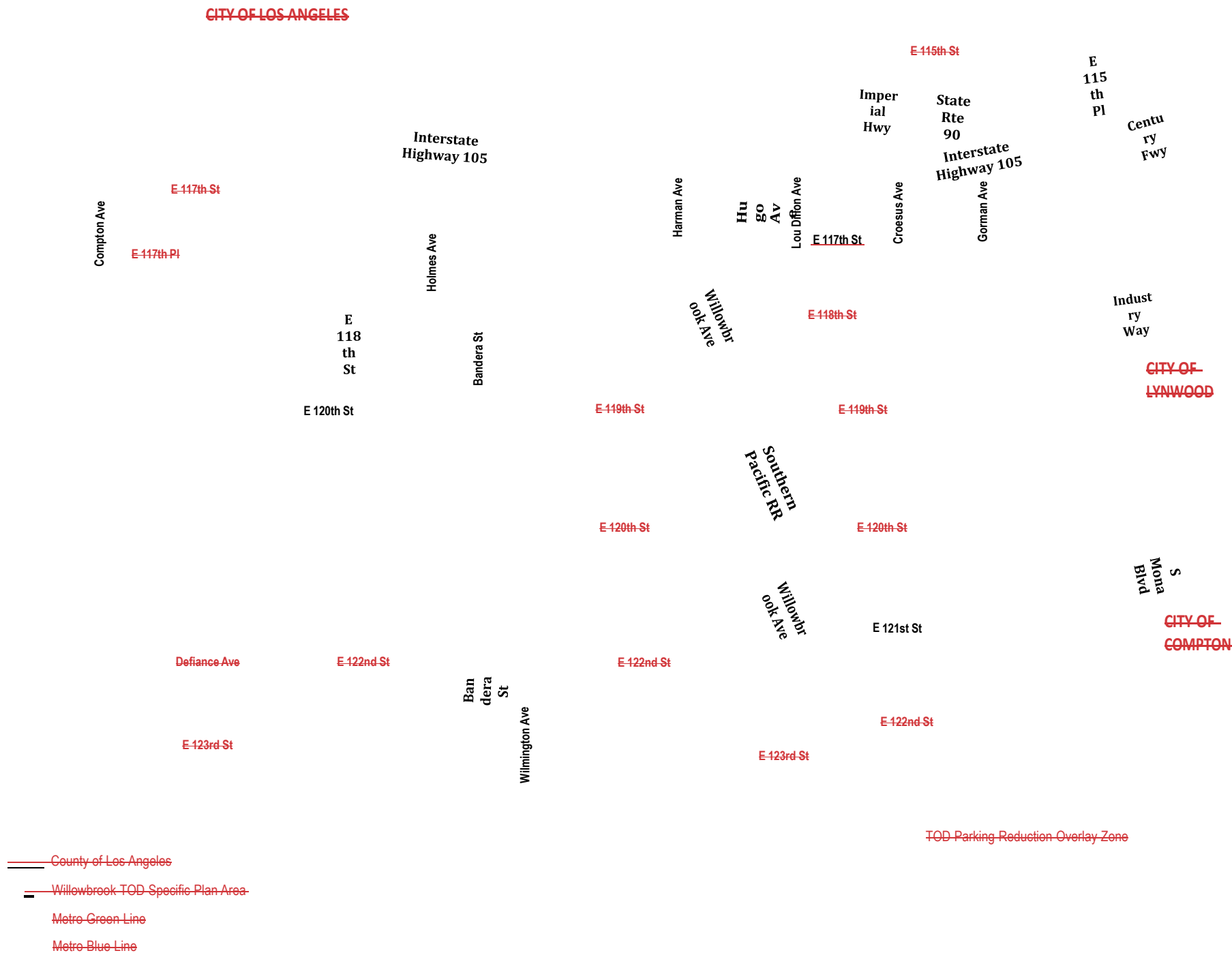
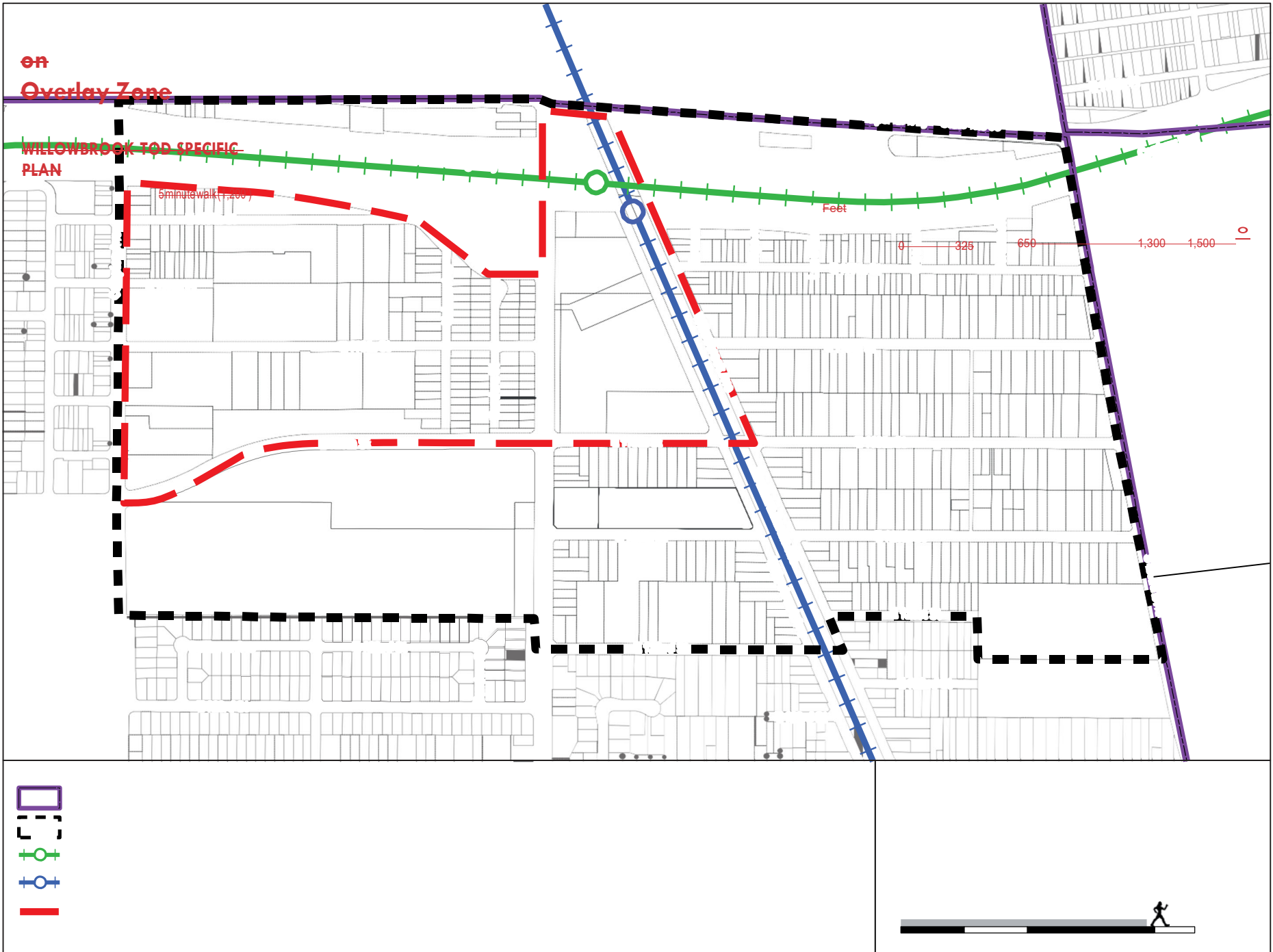


Figure 3-4
TOD
Parking
Reducti



**Table 3-6:
Parking Requirements for Residential Uses**

Land-Use	Minimum	Maximum
Residential		
Single Family	2.0/DU	2.0/DU
Bachelor Apartment	0.60/DU	1.0/DU
1-Bed Apartment	0.90/DU	1.5/DU
2-Bed Apartment	1.20/DU	2.0/DU
Guest ¹	0.15/DU	0.25/DU

1.—Guest parking is only required when the number of required guest spaces is greater than one.

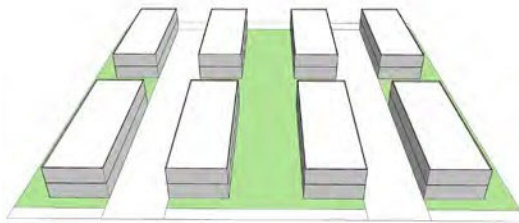
- 2.—Parking requirements for non-residential uses may be met within 1,500 feet from the site of the development.*
- 3.—A Transportation Demand Management Program shall be implemented for all new non-residential uses in the Specific Plan Area exceeding 50,000 square feet to reduce vehicle trips and reduce parking needs in the TOD (as identified in Chapter 4, Section 4.7).*
- 4.—Required residential parking shall be provided on site; and is not permitted within the required front yard or corner side yard. Required parking for new single family uses may be uncovered. A Ministerial Site Plan Review (SPR) application, as provided in Chapter 22.188, is required for the conversion of covered parking for existing single family uses to uncovered parking.*
- 5.—Tandem parking is allowed for single-family uses.*
- 6.—The provisions regarding shared parking in Chapter 22.180 (Parking Permits) of Title 22 shall apply.*

3.14.2 Design Guidelines for Mixed-Use, Commercial and Institutional Uses

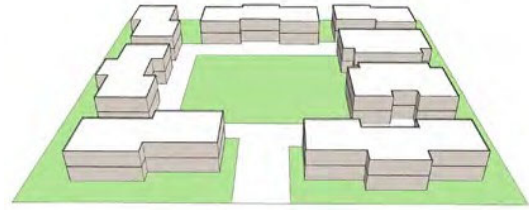
The purpose of this section is to facilitate high quality development for mixed-use, institutional and commercial uses within the Plan Area. These design guidelines address site planning and building design and are in addition to the ~~above~~ land use regulations and development standards contained in ~~this chapter~~ Chapter 22.412.

1. Building siting and orientation.

- a) ~~Buildings should be sited to avoid random and irregular building relationships, and arranged to create a sense of unity and overall harmony. To the maximum extent possible, n~~New structures should be clustered to create plazas and open space areas and avoid creating ~~of~~ “barracks-like” rows ~~of structures~~. Where clustering is impractical, a visual link between separate structures should be established. ~~This link can be accomplished through the use of an~~ such as arcade system, trellis or other open structure.



Avoid “barracks-like” layout



Orient buildings to create useable open space

Figure 3-25: Site Layout to Create Usable Space

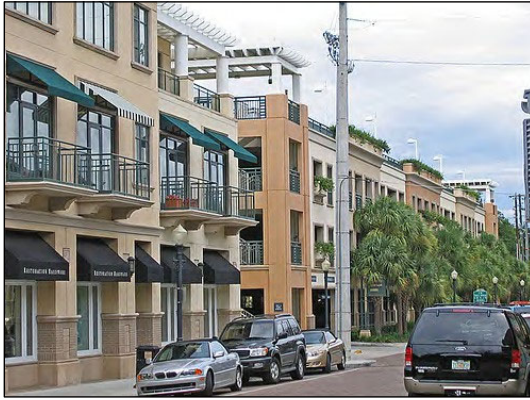


Arcades and trellises provide shade, connections, and define usable space.

- b) Commercial facilities development in mixed-use projects should be oriented to the street, with parking generally located in the rear, side or ~~underneath buildings~~ underground. The perimeter of parking areas and driveways adjacent to streets and sidewalks should be screened with an attractive low wall, berm, fence or landscaping.
- c) Existing residential uses should be buffered from ~~incompatible~~ commercial development to mitigate negative impacts ~~due to~~ such as noise, vibration, shading, light, and glare, ~~and aesthetics.~~ Intensified landscaping, increased yard depths and ~~or appropriate~~ building orientation ~~should be utilized as a means of providing~~ can improve adequate separation ~~between such land uses.~~
- d) Entry and edge design features such as landscaping, architectural signage ~~and monumentation~~ like monument signs, and/or enhanced paving should be incorporated.

2. Building modulation and detailing.

- a) Building design should avoid large monotonous facades, long straight-line building fronts, plain box shapes, and barren exterior treatments. All building elevations visible from a public right-of-way, including freeways, should be highly articulated and consistently incorporate the chosen design theme ~~in a consistent manner~~.

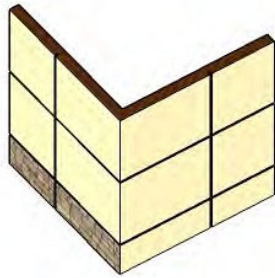


Varied building forms, volumetric and planar changes, and variations in roof forms and height, contribute to a well-articulated building massing.

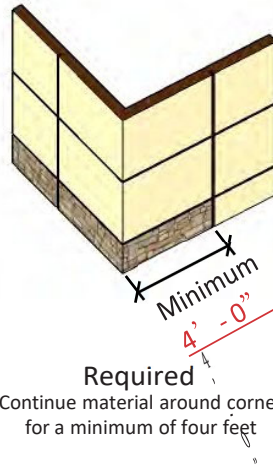
- b) Commercial facades of mixed-use projects should be modulated at least every 50 feet by changes in building mass or facade treatment, such as projected entrance windows, roof form or other architectural features.
- c) Building facades should be designed to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; varying color or materials of each individual module within a harmonious palette of colors and materials, etc.
- d) Building form and articulation should ~~be used to~~ accentuate public entrances, de-emphasize service areas, and define and shelter pedestrian walkways and exterior spaces.
- e) Awnings, canopies, and arcades should be used as appropriate to provide visual interest and shade.
- ~~f)~~ Buildings should be treated as a whole and finished appropriately on all sides to provide continuity. The Bbacks of buildings ~~shall~~ should use similar materials; however, less expensive and more utilitarian substituted materials are acceptable, provided they are compatible with the overall design.

g) ~~Materials~~ ~~changes shall~~ should not ~~occur~~ change at corners. The same material ~~shall~~ should continue around corners for a minimum distance of four feet ~~and, if if~~ feasible, ~~the same material should~~ continue to the next change in the wall plane.

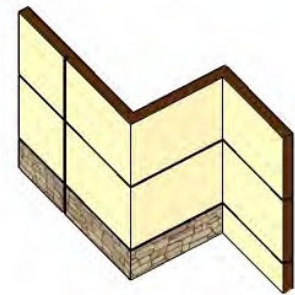
f) _____



~~Prohibited~~ Discouraged
Change of material at corner



Required
Continue material around corner
for a minimum of four feet



Encouraged
Continue material around corner
to a change in wall plane

Figure 3-36: Treatments for Material Changes at Corners

3. Building entrances.

- In mixed ~~--~~ use projects, ~~when~~ residential and commercial components ~~uses are combined in the same structure, should have~~ separate entrances ~~shall be provided for each use.~~
- The main entrance or entrances should be emphasized and oriented to the street ~~or,~~ major plazas, or open space. Main entries to buildings should be clearly demarcated using special materials, color, detailing, or equivalent architectural treatment, ~~visible~~ and accessible from the street and/or pedestrian walkways. ~~Examples include the use of special materials, color, detailing, or equivalent architectural treatment.~~
- Secondary entries may be from parking areas.



~~Entrances should be clearly demarcated. Entrances may be emphasized with building elements, material and color variations and signage.~~

4. ~~Transparency.~~

- ~~Placement and orientation of doorways and windows should create strong, direct relationships with the street. Street-facing façades of all buildings shall incorporate windows and openings providing light to adjacent spaces and rooms, and to create visual interest and provide "eyes on the street."~~

~~Windows and openings shall provide clear and transparent views into the ground floor retail uses or shall display merchandise to reinforce a pedestrian scale. Dark tinted, reflective or opaque glazing is not permitted for any required wall opening along street level retail façades.~~



Retail uses shall have transparent windows that help activate the street.

b) —

4. Transparency.

- a) Placement and orientation of doorways and windows should create a strong, direct relationship with the street. Street-facing façades of all buildings shall incorporate windows and openings providing interior light, create visual interest, and provide “eyes on the street.”
- b) Windows and openings ~~shall~~ should provide clear and transparent views into ground floor retail uses which should display merchandise to reinforce a pedestrian scale. Dark tinted, reflective, or opaque glazing should not be applied to any required wall openings of street level retail façades.
- c) Security doors or gates should not be directly visible when not in use. They should be recessed in the interior walls or ceiling or otherwise built-in to the interior of the space.

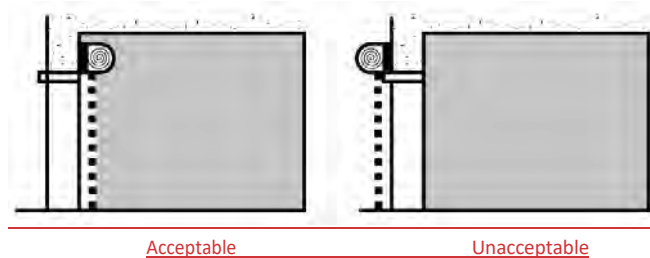


Figure 3-4: Roll-Up Doors



~~Retail uses shall have transparent windows that help activate the street.~~

5. Privacy for residential units.

- a) Buildings should be oriented to promote privacy to the greatest extent possible. In mixed use projects, residential windows should face away from loading areas and docks. To the extent residential windows face the windows of an adjacent unit; the windows should be offset to maximize privacy.
- b) Windows, balconies or similar openings should be oriented so as not to have a direct line-of-sight into adjacent units within the development. In addition, units above the first story should be designed so that they do not look directly onto private patios or backyards of adjoining residential property or units.

b)6.

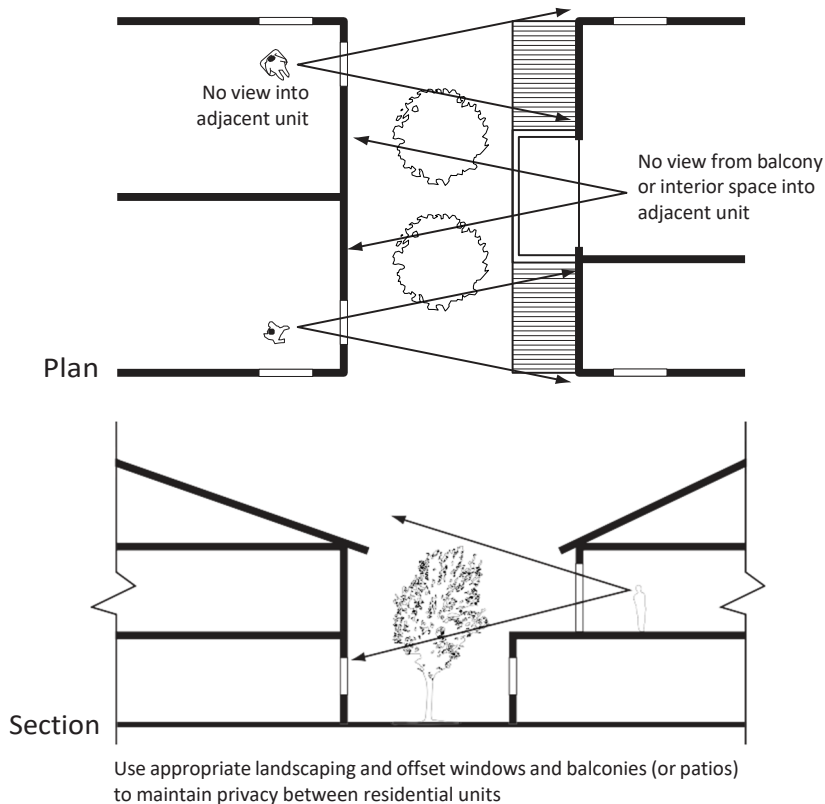


Figure 3-57: Privacy for Residential Units

6.7. Vehicle circulation and access.

- a) Site access and internal circulation in all developments ~~shall~~ should be safe, efficient, and convenient. Vehicular traffic should be adequately separated from pedestrian circulation. Vehicular entrances ~~shall~~ should be clearly identified to minimize pedestrian/vehicle conflict.
- b) The number of site access points or driveway aprons should be minimized ~~for aesthetic purposes, to achieve efficient and productive use of paved pathways, and to eliminate for efficiency and to~~ reduce traffic hazards. They should be located as far as possible from street intersections and be coordinated with existing or planned median openings and driveways on the opposite side of the roadway. Common driveways that promote access to more than one site are encouraged.

7. Parking lots.

- a) Parking lots should be designed with a clear hierarchy of circulation: major access drives with no direct access to parking spaces; major circulation drives with little or no parking; and parking aisles for direct access to parking spaces. Loading and service areas should be provided with separate access and circulation whenever possible.
~~—Parking for multi-family developments shall—~~ should be provided on-site in surface or underground structures, parking lots, carports, or attached garages.
- b) The visual impact of parking lots should be mitigated with landscaping, walls, or ~~screened by~~ building screenings.

8. Parking structures.

- a) The massing, height, and architectural design of ~~the a~~ parking structure should be consistent and compatible with the rest of the development ~~within which the parking structure is to be located.~~
- ~~b) a)~~ Vehicular access to parking structures should ~~be designed so as not to not~~ obstruct the free flow of traffic on ~~adjoining~~ public streets. ~~Vehicular and~~ entrances and exits should be located ~~so as to minimize turning movement to reduce traffic conflicts as vehicles enter and exit the structure.~~
- ~~c) b)~~ Pedestrian access ~~points to the a~~ parking structure should ~~be located to~~ avoid pedestrian/vehicle conflicts.



Parking structures should be designed to integrate with the area they serve. Well-designed structures, ground-level storefronts, and enhanced landscape treatments can minimize the visual impact of parking structures.

9. 8. Pedestrian circulation.

- a) All developments ~~shall be designed to enhance pedestrian movement within the project and to adjacent uses. The design of new projects shall~~should promote pedestrian walkability and connectivity both onsite and offsite. Pedestrian walkways should be safe, visually attractive, and adequately separated from vehicular traffic.
- b) ~~Pedestrian walkways between parking areas and buildings shall be provided. Pedestrian walkways should also be provided to link dwelling units~~residential with commercial or institutional uses in a mixed-use or institutional development ~~respectively,~~ as well as with common open space, plazas and courtyards, and public sidewalks.
- ~~b)c) Where possible, multi-family developments should incorporate pedestrian connections to adjoining residential and commercial projects.~~
- ~~c)d) Where p~~Pedestrian paths or walkways that cross parking areas or driveways , should have decorative paving or —some other method shall be used to define the pedestrian space and to —delineate pedestrian space~~crossings.~~
- ~~d)e) Pedestrian paths or~~ walkways to building entrances ~~shall be~~should be paved with high quality paving materials ~~such as, but not limited to, like~~ stone pavers, ~~decorative~~ concrete pavers, ~~and or~~ brick and well-defined by landscaping.

10. 9. Residential open space.

- a) Common open space areas ~~shall should~~ be conveniently located, to the residential portion of a mixed-use development, separated and secure from the non-residential component in mixed-use developments and exclusively available for residents, and clearly marked for residential use only.
- b) Common open space areas ~~shall should~~ contain amenities appropriate to the project's size; ~~i.e. for example,~~ pools, spas, recreation buildings are encouraged for larger projects, while barbeque areas and gazebos may be more appropriate for smaller projects. Children's play areas ~~shall should be sited to~~ be visible from residential units and not visible from the public right-of-way.



Play areas can be incorporated into the common areas.



Pools, spas, and other recreational amenities are encouraged for larger projects.

- c) ~~In mixed-use projects, accessible Rooftop open space~~ accessible to all residents ~~may be used~~ account toward required ~~common useable open space by the residents. It may also be used~~ for or it may count toward required ~~private useable open space, when the if it space~~ is directly accessible from the unit(s).

11.10. Fences, walls, gates, and hedges.

- a) ~~Walls and fences shall be planned and and other boundary elements should be~~ designed as integral parts of all projects for screening and security. as necessary to screen vehicles, loading and storage areas, and utility structures, and shall be consistent with the landscaping and building design. ~~Decorative types of view fencing adjacent to public rights of way, such as wrought iron, are~~ generally encouraged for street fencing.

12.11. Exterior lighting.

- a) ~~Exterior lighting shall be used to provide illumination for the~~ should provide ~~security and safety of on-site and visibility of~~ areas such as building entrances, parking, loading and service areas, shipping and receiving, walkways, driveways, and open space areas.
- a)b) ~~and working areas. The design of light fixtures and their structural support shall be~~ should be architecturally compatible with the main buildings on-site.
- b)c) ~~Exterior lighting Site, parking lot and building security lighting shall~~ should not impact surrounding or neighboring properties. All lighting should be shielded to confine light spread on-site. The type and location of such lighting shall preclude direct glare onto adjoining property, streets, or skyward. All lighting fixtures must be shielded to confine light spread on-site.
- e)d) ~~Exterior lighting in mixed-use projects, all site, parking lot and building security lighting shall~~ should be appropriately shielded so as not to spill over, into or otherwise adversely not adversely impact the residential portion of a mixed-use the development.

13. 12. Outdoor Storage, Loading and Service areas.

- a) ~~Areas for service entrances, loading docks and bays, or trash and recycling collection facilities should be clearly defined. They should not be visible from public rights-of-way and should be~~ generally screened from view.
- a)b) ~~Loading and service areas should be designed to prevent not interference with vehicular or pedestrian circulation and parking, and to provide an unobstructed area for trucks to maneuver when accessing loading spaces.~~
- c) ~~Loading areas should be located away from main customer entrances and the street and, preferably located toward the rear of the property. to minimize odors and noise affecting residential units or adjacent residential uses.~~
- d) ~~All outdoor storage, loading, and service areas should be located to minimize odors and noise affecting residential units or adjacent residential uses.~~
- e) ~~Outdoor storage, loading and service areas should not be visible from public rights-of-way and should be screened from public view.~~

c) Overhead (roll-up) doors shall not be directly open to public view, and shall be substantially screened from the street and residentially zoned properties. Screening may be accomplished in a variety of ways, including the use of wing walls, the recessing of overhead doors (building articulation), landscaping, or a combination of these techniques. Fixed hardware for roll up doors shall be located on the inside of buildings to minimize visual clutter. If located adjacent to a residential use, the design of overhead doors shall minimize noise through devices such as rubber seals and/or other dampening features.

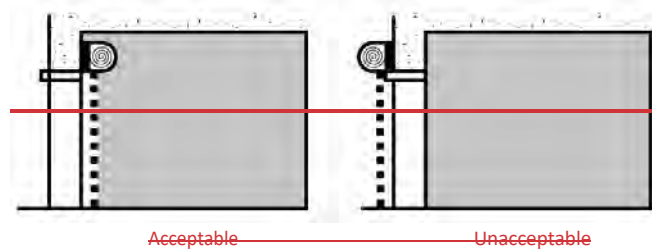


Figure 3-8: Roll Up Doors

14. Outdoor storage and service areas.

- a) ~~Outdoor storage and service areas (including, but not limited to, service entrances, loading docks and bays, outdoor storage of commercial vehicles) shall be clearly defined and designated for convenient access. They shall not conflict with vehicular access, on-site parking facilities, pedestrian walkways, and customer entrances.~~
- b) ~~All outdoor storage and service areas for commercial uses shall be located so as to be convenient to the commercial users and where associated odors and noise will not adversely impact the residential units in a mixed use project, or adjacent residential uses.~~
- c) ~~Outdoor storage and service areas shall not be visible from public right-of-ways and shall be screened from on-site and off-site public view with a combination of building features, decorative walls and landscaping consistent with the architectural style of the building.~~

15. Refuse and recyclable collection facilities.

- a) ~~Refuse and recyclable storage shall be enclosed within or adjacent to the main structure or located within separate freestanding enclosures. The location of refuse and recyclable collection facilities shall be coordinated with the location of loading/service areas, and not be readily visible to public view.~~
- b) ~~In mixed use developments, all refuse collection facilities for commercial uses shall be located so as to be convenient to the commercial users and where associated odors and noise will not adversely impact the residential uses.~~
- c) ~~Screening for refuse and recyclable collection facilities shall use a combination of building features, decorative walls and landscaping consistent with the architectural style of the building.~~

3.15 3.3 Design Guidelines for Multi-Family Residential Uses

The purpose of this section is to facilitate high quality multi-family residential development within the Plan Area. These design guidelines address site planning and building design and are in addition to the above land use regulations and development standards for multi-family uses ~~in this chapter~~.

1. Scale and mass.

- a) Multi-family residential projects should avoid having a massive "big-box" appearance. Multi-family units in large projects should be clustered, or broken up into groups of structures. ~~Windows should be provided facing the street for all units~~ Units facing adjacent to the street should have windows to break up massing and to contribute to "eyes on the street" ~~which helps for~~ neighborhood security.

2. Building orientation and entrances.

- a) Multi-family developments should have a strong street presence with public entrances and unit entrances oriented towards the street. ~~The majority of unit entrances must be accessed from the street or from the main open space. Where there is a common building entrance for all units, it should be accessed from the street or main open space.~~
- b) Multi-family developments should have transitional spaces in the form of stoops, overhangs and porches between public areas and entrances to the units.

3. Building articulation and detailing.

- a) Multi-family building design should avoid large monotonous façades, long straight line building fronts, plain box shapes, and barren exterior treatment. All building elevations visible from a public right-of-way should be articulated and incorporate the chosen design theme in a consistent manner.
- b) Multi-family building facades should be designed so as to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; and varying color or materials of each individual module within a harmonious palette of colors and materials, ~~etc.~~



Articulate buildings with architectural detailing, staggered building planes, material and color changes, and varied roof lines.

4. Privacy for residences.

- a) Multi-family buildings should be oriented to promote privacy to the greatest extent possible. ~~Openings above the first story such as windows, Windows and, balconies or similar openings above the first story~~ should be oriented ~~so as not to~~ not have a direct line-of-sight into adjacent units within the development.
- b) Multi-family units above the first story should ~~be designed so that they do not~~ look directly onto private patios or backyards of adjoining residential property or units. Landscaping should be used to aid in privacy screening in ground floor units.

5. Vehicle circulation and access.

- a) Site access and internal circulation in multi-family developments ~~shall~~ should be safe, efficient, and convenient. Vehicular traffic ~~shall~~ should be adequately separated from pedestrian circulation. Vehicular entrances ~~shall~~ should be clearly identified ~~and easily accessible~~ to minimize ~~pedestrian/ vehicle~~ conflict.
- b) The number of site access points or driveway aprons should be minimized ~~for aesthetic purposes, to achieve efficient and productive use of paved accessways, and~~ to eliminate traffic hazards. They should be located as far as possible from street intersections and be coordinated with existing or planned median openings and driveways on the opposite side of the roadway.

6. Parking.

Parking for multi-family developments ~~shall~~ should be provided on-site in on-grade or underground structures, parking courts, carports, or attached garages. Adverse visual impacts ~~from parking lots, carports, structures, and garage doors on the residential character of the street or project site shall~~ should be minimized through proper siting and design.

7. Pedestrian circulation.

- a) Pedestrian circulation ~~shall~~ should be adequately separated from vehicular traffic. Pedestrian entrances and walkways should be clearly identified and easily accessible to minimize ~~pedestrian/vehicle~~ conflict.
- b) Pedestrian walkways should link dwelling units with common open space and recreation areas, parking areas, and the street at the project entries. Where possible, multi-family developments ~~shall~~ should ~~connect incorporate pedestrian connections to~~ adjoining residential and commercial projects ~~and other compatible land use facilities~~.
- c) Pedestrian walkways ~~shouda~~ should be safe, visually attractive, and well defined by landscaping and lights.

8. Exterior lighting.

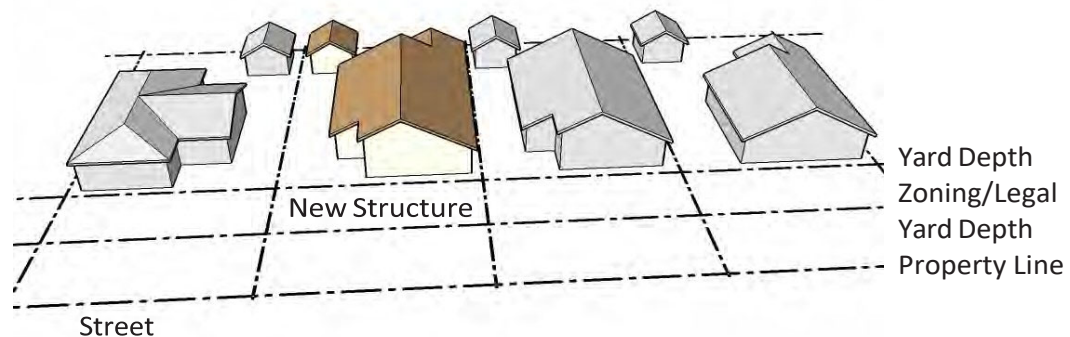
- a) Every multi-family project should have adequate lighting ~~to provide~~ for security and visibility, particularly along walkways and driveways, at entrances to parking areas, and within open space areas.
- b) Site lighting ~~shall~~ should not impact ~~surrounding or neighboring properties~~. ~~The type and location of site and building lighting shall preclude direct glare onto adjoining property, streets, or skyward. All and all~~ lighting fixtures ~~must~~ should be shielded to confine light spread on-site.
- c) The design of all exterior light fixtures should be compatible with the building's architecture.

3.16 3.4 Design Guidelines for Single-Family Residential Uses

The purpose of this section is to facilitate high quality single-family residential development within the Willowbrook Residential 1 and Willowbrook Residential 2 Zones. These design guidelines address site planning and building design and are in addition to the ~~above~~ land use regulations and development standards ~~for the Willowbrook Residential 1 and Willowbrook Residential 2 Zones~~ in Chapter 22.412.

1. Yard areas.

~~1. While meeting the minimum front and side yard depths established in Chapter 22.18, a~~ new dwelling's front and side yard depths should approximate that of adjacent residences. ~~The front and side yard pattern on the block shall be identified and respected to help unify the neighborhood.~~



New structure respects the yard depths established along a street

Figure 3-69: Yard Depths

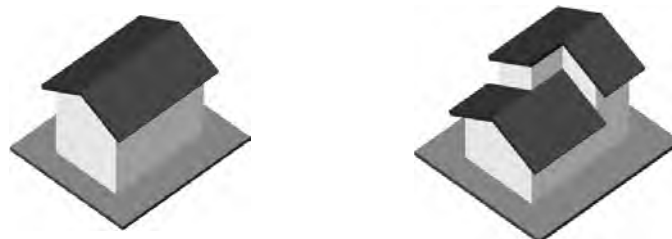
2. Scale and mass.

~~2. The scale and mass of a new dwelling should be similar to that of neighboring houses and not overwhelm them with disproportionate size or a design that is out of character.~~

~~3. Height and rooflines.~~ Height and rooflines should be consistent with the style of architecture of the house, and complement qualities of neighboring residential structures such as type, slope, size, material, and color.

3. Additions and accessory buildings.

~~4. Additions and accessory structures should maintain and complement the look and appearance of the existing primary structure so that they do not appear as an addition or new building. They should respect the architectural style, scale, rhythm, and building elements of the existing primary structure. An addition should complement and balance the overall form, mass, and composition of the existing primary structure on the property. An addition shall and maintain the same floor-to-floor height of the original structure.~~



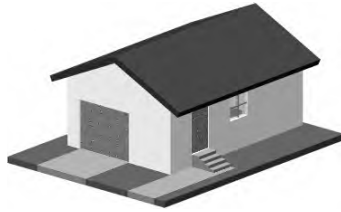
Discouraged

Encouraged

Figure 3-~~740~~: Massing

4. Orientation.

Dwellings ~~shall~~should be oriented ~~themselves~~ to the street with a clearly identifiable front door and windows ~~that face the street. Front windows and the front door contribute to~~to put “eyes on the street.” ~~which helps neighborhood security.~~



Discouraged



Encouraged

Orient the front of the house and entry to face the street

Figure 3-8: Orientation

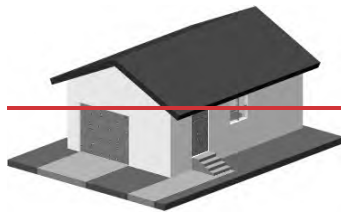
5.

5. Privacy for residences.

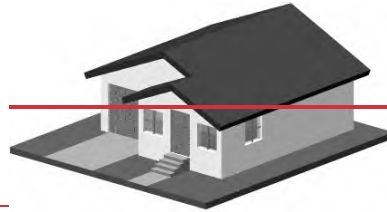
New houses and/or accessory buildings should maintain privacy by staggering the placement of new windows to the opposing neighbor's windows and avoid locating balconies that directly overlook a neighboring patio or yard.

6. Exterior lighting.

Exterior lighting includes all lighting fixtures on front façades, all security lighting, and all landscape lighting. Adequate exterior lighting should be provided on the front of the house to contribute toward neighborhood safety and security at night. Recessed porches should be lit. Exterior lighting should be shielded and positioned to confine light spread on-site and not into neighboring properties.



Discouraged



Encouraged

Orient the front of the house and entry to face the street

Figure 3-11: Orientation

~~6. Privacy for residences. New houses and/or accessory buildings should minimize their visual impact on adjacent properties. New windows and second floor balconies should be placed where they promote maximum privacy between properties. Windows should be offset or staggered from neighboring windows to maintain privacy between houses. Avoid locating balconies to directly overlook a neighboring patio or yard.~~

~~7. Exterior lighting. Exterior lighting includes all lighting fixtures on front façades, security lighting, and landscape lighting. Adequate exterior lighting shall be provided on the front of the house to~~

~~ensure neighborhood safety and security. Recessed porches shall be lit. Exterior lighting shall be positioned so that no direct light extends into neighboring properties or public right-of-ways.~~

3.17 3.5 Sustainable Design Guidelines

Building green means reducing the use of resources, minimizing harmful impacts to the environment, and creating healthier environments for people. Green buildings can incorporate both passive, low-tech design, such as daylighting; and active, high-tech strategies and systems, such as photovoltaic panels. To be most effective, green building strategies should be incorporated into all phases of a project. ~~from early programming and budgeting, to design and construction, to commissioning, operations, and maintenance and post-use demolition/recycling.~~

1. Site Design and Passive Solar Design

- Buildings should be sited oriented and designed to maximize the use of sunlight and shade for energy savings, ~~and respect the~~ without compromising the solar access of ~~adjacent buildings-neighbors.~~
- Buildings should be clustered for shade, and incorporate features like ~~protective-protected~~ courtyards, ~~and~~ recessed
- windows and doors, ~~and insulated walls.~~
- ~~To reduce energy use, the e~~ East and west walls of buildings should be shaded with evergreen trees to reduce summer heat gain. South walls should be shaded with deciduous trees to increase winter heat gain.
 - ~~Walkways and plazas should be designed to collect stormwater, where feasible.~~

2. Water Efficiency

- Walkways and plazas should be designed to collect stormwater so it can be retained on-site.
- ~~To reduce water use and maintenance costs, the majority of p~~lant materials should be
- drought tolerant and ~~require relatively low-~~ maintenance.



Landscape materials should be drought-tolerant and low-maintenance. Pervious, non-vegetative ground cover interspersed with native plantings can provide a sustainable, attractive landscaped area.

3. Building Design

- ~~Green roofs should be considered to reduce solar gain and retain stormwater on-site. The provision of a green roof should strongly be considered to reduce solar gain and to~~
- ~~reduce the quantity of water entering the storm drain system.~~
- Solar panels on roofs should be considered to ~~capture solar energy for internal use of the project~~ generate power on-site and provide energy savings.
- Arcades, covered walkways, ~~and,~~ trellises ~~and passages~~ should be incorporated to provide ~~pedestrian shade and reduce building energy use.~~ ~~sheltered areas for pedestrian circulation as well as to shade the buildings to reduce energy usage.~~



Green roofs serve several purposes for a building, such as absorbing rainwater, providing insulation, and helping to lower urban air temperatures and mitigate the heat island effect. Parking structures and other buildings offer potential for solar roofs, which capture sunlight and transform it into energy.

3.18 ~~3.6~~ Street and Sidewalk ~~Standards and~~ Guidelines

The Plan Area has a mix of commercial, institutional, and residential uses. While both paved sidewalks and sidewalks with parkways can be found within the area, this Specific Plan differentiates the sidewalk treatment between residential and non-residential frontages as follows:

- All existing parkways ~~facing-fronting~~ residential uses ~~shall-should~~ be retained.
- All other street segments (with institutional, commercial or ~~mixed-use~~mixed-use zone frontages) ~~shall-should have paved sidewalks instead of parkways.~~
~~have paved sidewalks instead of parkways.~~



Existing parkways along residential frontages include turf and drought tolerant landscaping.



Existing sidewalks along non-residential parkways have a mix of paving materials.

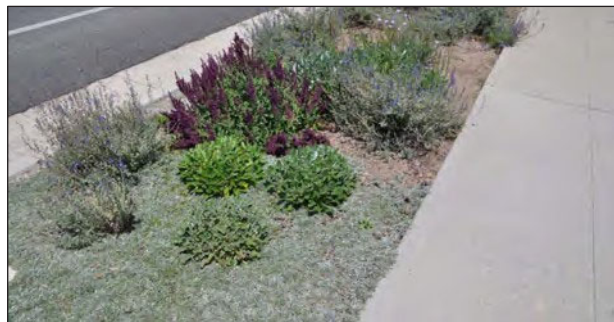
Each property owner is responsible for the maintenance of the parkway that fronts their lot. Low-maintenance ~~d~~drought-tolerant landscaping is encouraged in ~~the~~ residential parkways.



Sidewalks along non-residential frontages should have attractive paving materials and patterns. Tree wells can also become a part of a Low Impact Development strategy.

As new development occurs in the Mixed-Use Zones, the adjoining property owner/developer ~~shall~~ should follow apply the following ~~standards and~~ guidelines for upgrading ~~roadways and sidewalks~~ the public right of way. Property owners in other areas are also encouraged to follow these standards and guidelines.

- ~~• New or reconstructed roadways and sidewalks shall be designed in coordination with the County Department of Public Works (DPW).~~
- ~~• Green Streets and Low Impact Development (LID) strategies, such as the use of vegetated swales and decomposed granite, shall be utilized to manage stormwater, improve water quality, reduce flows and enhance watershed health.~~
- New sidewalks should be graded to allow stormwater run-off into adjacent unpaved areas, such as planter strips and parkways.
- Sidewalks should be designed ~~or modified such~~ so that street furniture, signage, lighting, landscaping, etc. do not interfere with a continuous pedestrian path. ~~The pedestrian path should be clearly identified in project plans.~~
- ~~• Street treatments in which stormwater runoff is captured and retained shall be located within street medians and parkways. Stormwater Best Management Practices per the California Stormwater Quality Association's Stormwater Best Management Practice Handbook shall be applied.~~
- Decorative non-vegetative groundcover, such as river cobble, pebbles, and crushed stone, ~~should be used in streetscape composition that allow proper drainage are encouraged. Non-vegetative groundcovers shall allow proper drainage.~~ Any material, such as crushed tires, ~~that may leach harmful chemicals into the soil or generate heat is not permitted~~ should be avoided.
- ~~• Sidewalks shall comply with all Americans with Disabilities Act (ADA) standards.~~



Drought-tolerant landscaping in the parkways is an attractive replacement for turf.

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4.1 Introduction

This Chapter describes the Mobility Strategy for the Willowbrook TOD Specific Plan, which includes recommendations for the roadway network as well for improving bicycle, pedestrian, bicycle, and transit circulation.

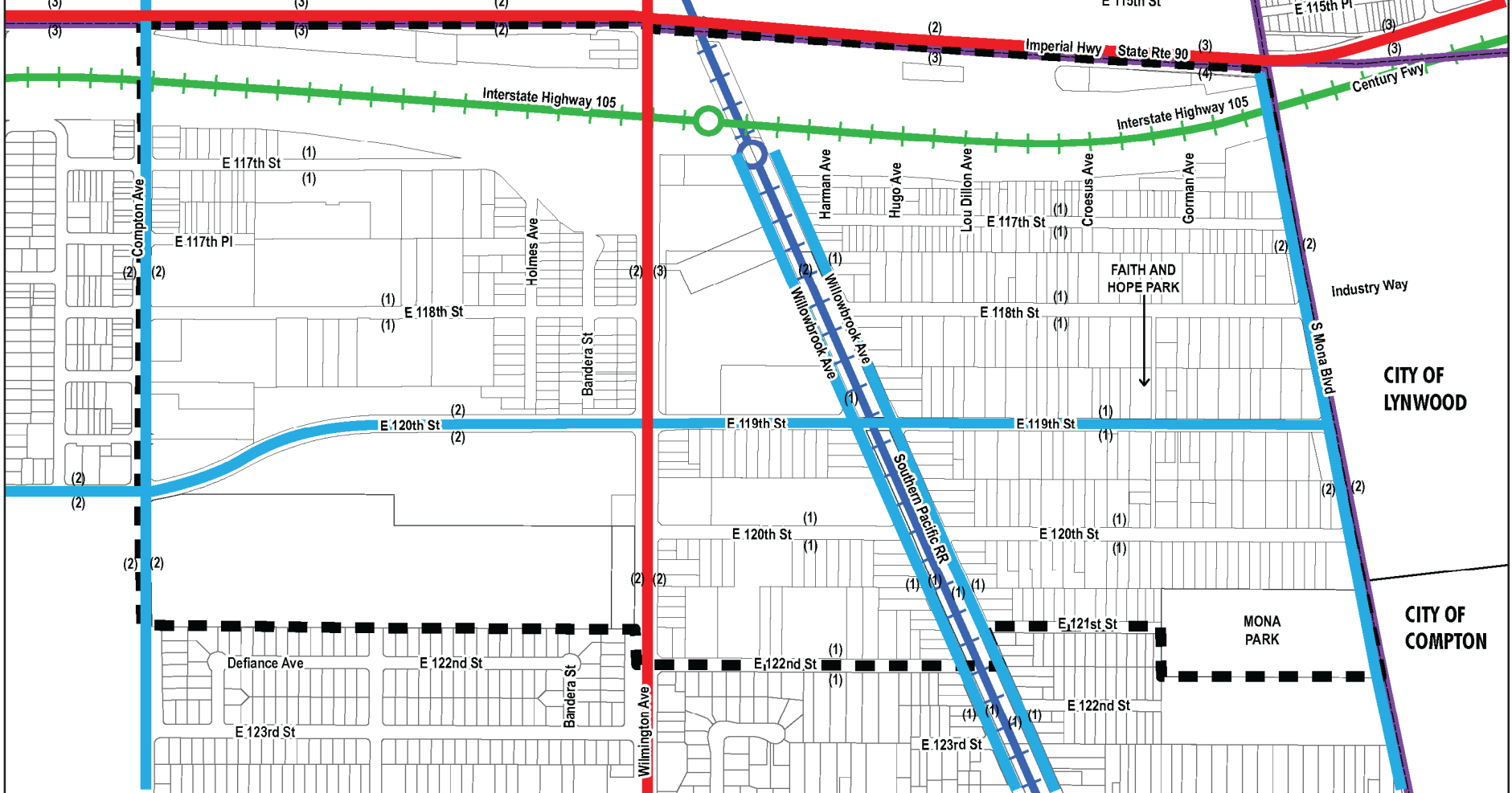
The overall goal ~~for mobility in the Plan Area~~ is to enhance connectivity and ~~the ease of movements for non-auto transportation modes, particularly~~ encourage active transportation ~~pedestrians and bicyclists. These enhancements will support healthy transportation options.~~








4.2 Background

The Plan Area is centered on the Willowbrook/Rosa Parks Station, ~~a transfer station with stops~~ for both the Metro Blue-A Line and the Metro Green-C Line.

~~Generally, the~~ The ~~existing~~ circulation system in the area is automobile-oriented and ~~auto~~-dominated. Even with numerous shuttles and bus routes serving the Plan Area and connecting to the Metro Station, ~~the~~ the ~~rail~~ Metro station is generally inhospitable and generally difficult to access for pedestrians, ~~and the station area is inhospitable to transit users. There are however numerous shuttles and bus routes serving the Plan Area and connecting to the rail station.~~

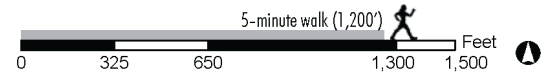
~~The local community has~~ Community members expressed safety concerns over ~~the~~ high speed of ~~vehicle~~ traffic on key roadways and the poor pedestrian environment at most intersections and on many of the sidewalks. There are few locations to conveniently safely cross streets and, many intersections are without a clear roadway and pedestrian markings crosswalks, and a poor pedestrian sidewalk environment — particularly on the The east side of Wilmington Avenue by near the Willowbrook/Rosa Parks Station is particularly inhospitable. There is a lack of pedestrian infrastructure in Some areas, with have unpaved pathways along streets and most areas have inadequate poor lighting. Furthermore, there is a lack of bicycle infrastructure including bike lanes and on streets in the Plan Area, and inadequate bicycle bike parking make riding bicycles inconvenient and less safe.



-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro C Line
-  Metro A Line
-  Major Roadway
-  Secondary Roadway
-  (x) Number of Lanes

**Figure 4-1
Street Network**

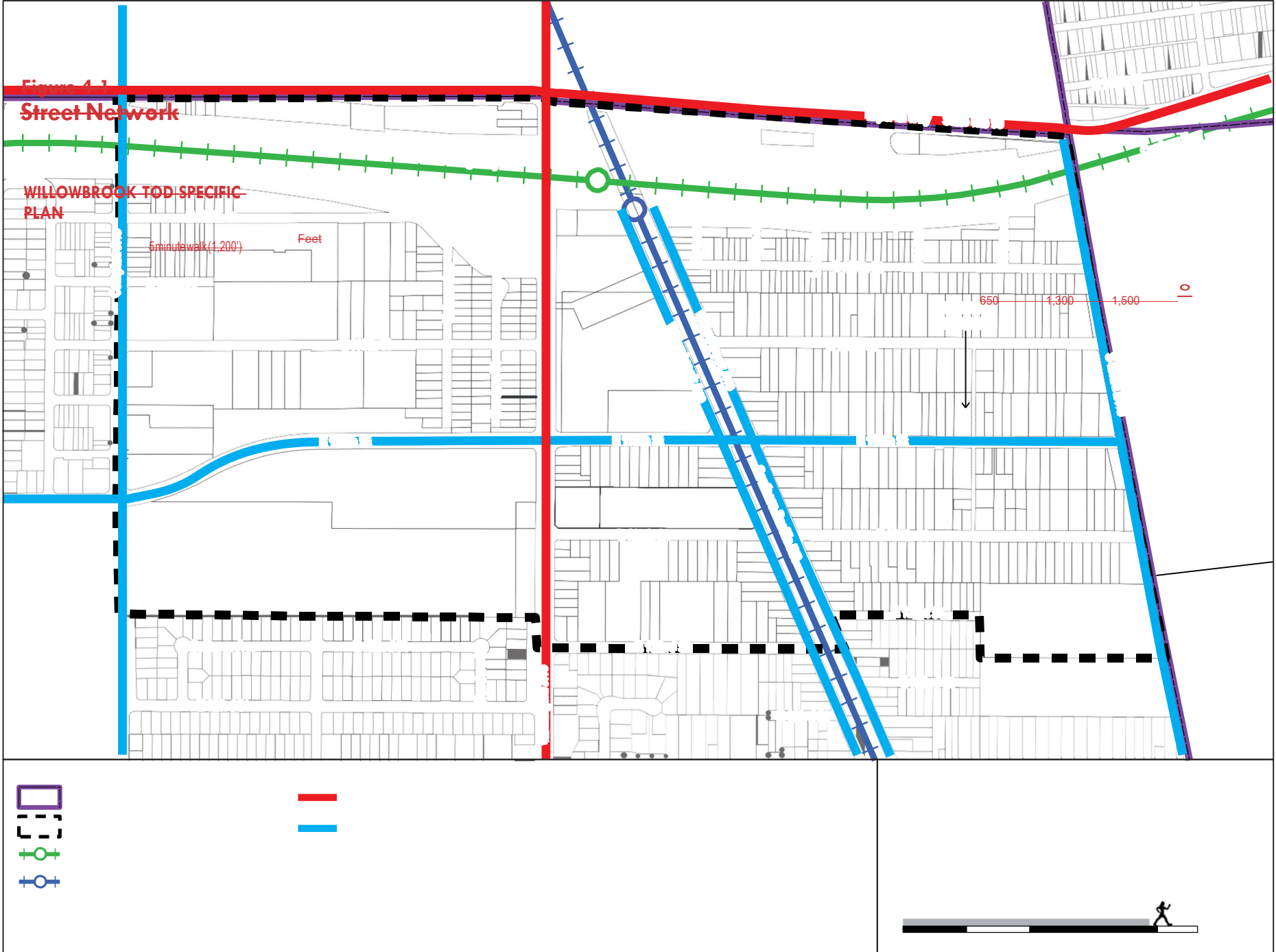
WILLOWBROOK TOD SPECIFIC PLAN



County of Los Angeles
Willowbrook TOD Specific Plan Area
Metro Green Line

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Major Roadway-Secondary-
Roadway Number of Lanes
(x)



4.3 Roadway Network

The roadway system, shown on Figure 4-1, ~~provides the backbone circulation system~~ is the basis for all modes of transportation. While historically ~~street systems have been oriented to serving the automobiles,~~ the roadway system should provide a network of "Complete Streets" also accommodate complete streets that provides for the safe and efficient circulation of transit, bicycles, and pedestrians, ~~as well as automobiles.~~

~~The Major Roadways (as identified in the General Plan) will continue to be~~ main streets in the Plan Area are Wilmington Avenue and Imperial Highway. ~~Secondary Roadways (as identified in the General Plan) will continue to be~~ Secondary streets are Compton Avenue, Willowbrook Avenue and Mona Boulevard in the north-south direction, and 120th Street/119th Street in the east-west direction. ~~The number of traffic lanes and roadway lane configurations will generally remain the same, except where otherwise noted in the Specific Plan (for example where road diets will be implemented).~~ The existing street system will be largely maintained ~~in its current configuration,~~ with some new ~~some changes/ improvements designed to improve safety,~~ access, and ~~circulation, and walkability improvements.~~ ~~The Major Roadways (as identified in the General Plan) will continue to be~~ Wilmington Avenue and Imperial Highway. Secondary Roadways (as identified in the General Plan) will continue to be Compton Avenue, Willowbrook Avenue and Mona Boulevard in the north-south direction, ~~and 120th Street/119th Street in the east-west direction. The number of traffic lanes and roadway lane configurations will generally remain the same, except where otherwise noted in the Specific Plan (for example where road diets will be implemented).~~

~~The following street enhancements, shown in Figure 4-2, are intended to improve circulation for bicycles and pedestrians in the Plan Area. These are also~~ and ~~described in more detail in Sections 4.4 and 4.5.~~

Road Diet and Bicycle Lanes on 120th Street

The section of 120th Street between Compton Avenue and Wilmington Avenue will be reduced from four traffic lanes to three lanes ~~and on-street parking removed,~~ with a to accommodate a bicycle lane in each direction ~~and no on-street parking. This is~~ as part of the Willowbrook Area Access Improvement Project.

Willowbrook Avenue West

The southbound section of Willowbrook Avenue ~~West~~ between the Willowbrook/Rosa Parks Station and 119th Street, will be reduced from two lanes ~~southbound to~~ a bicycle path installed on the west side of the street and a one lane dedicated busway southbound, ~~and a bicycle path installed on the west side of the street. This lane will be a dedicated busway.~~

Road Diet and Bicycle/Pedestrian Trail on Mona Boulevard

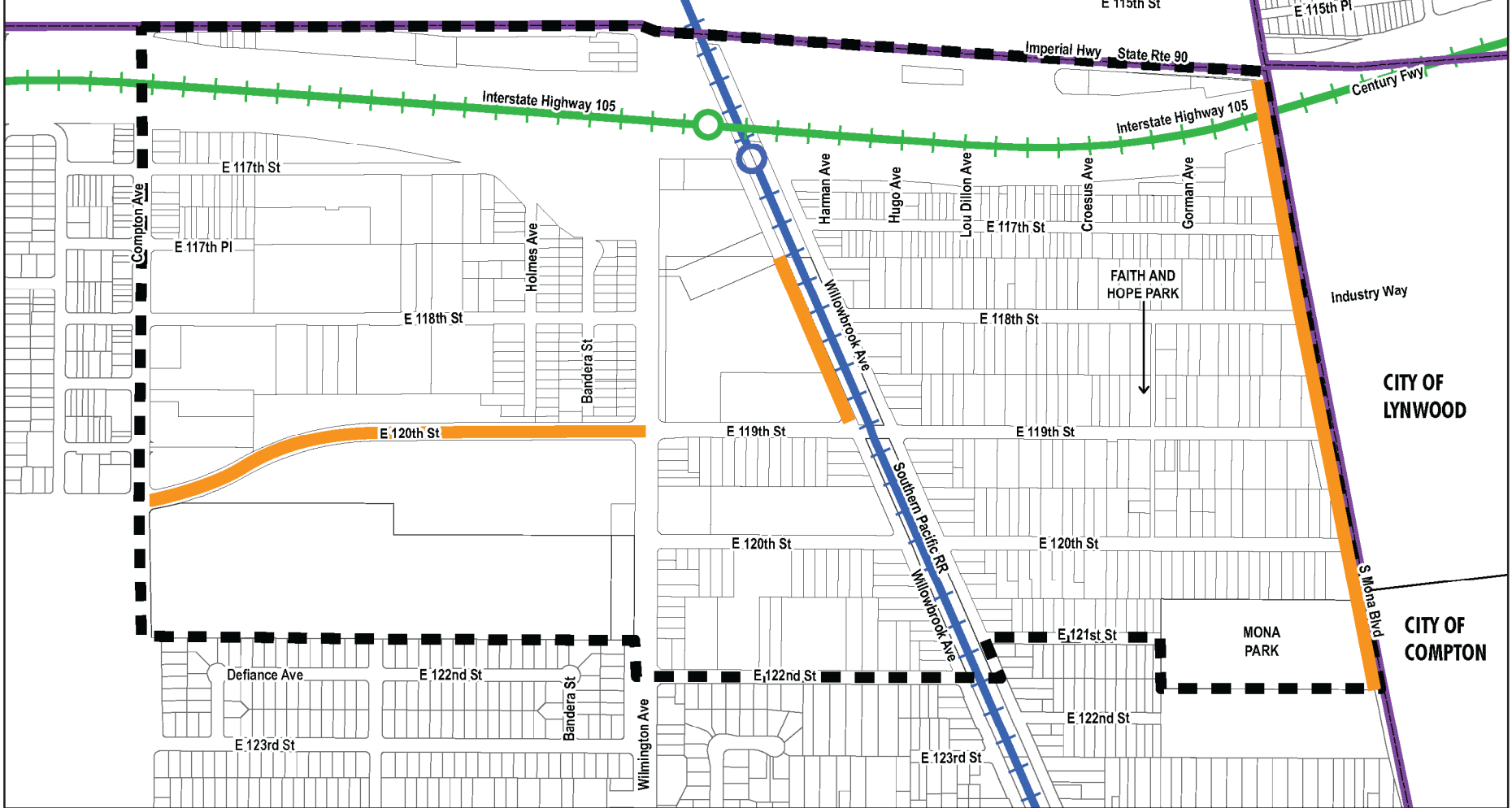
Mona Boulevard from the I-105 Freeway to 124th Street will be ~~converted-reduced~~ from a four lanes street to a three lane street, to accommodate and a paved, Class I pedestrian/bicycle trail ~~installed on the west side of the street. On-street parking will remain. Most properties on the west side of Mona Boulevard take their access from the east-west streets. There are twelve existing driveways and curb cuts that the trail will have to safely accommodate. Parking will be retained on the west side of the street. The pedestrian/bicycle trail shall include a paved section to meet Class I design guidelines. The design of the trail will be subject to the requirements of DPW.~~ Figure 4-3 illustrates a typical cross section of reconfigured Mona Boulevard.

~~The majority of properties on the west side of Mona Boulevard are accessed from the east-west streets. However, because there are some driveways and curb cuts on Mona Boulevard (approximately twelve along the 3,000 foot section), the pedestrian/bicycle trail must be designed to safely accommodate those driveways, with adequate signage and surface material details. (It is noted that typical street~~

~~conditions often require driveway access across sidewalks and across bicycle lanes).~~

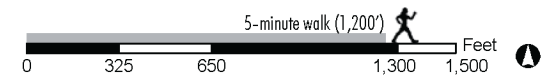
Rename 120th Street between Compton and Wilmington Avenues

Currently, the segment of 120th Street ~~west of Wilmington Avenue~~ (between Compton and Wilmington Avenues) ~~confusingly~~ aligns with 119th Street east of Wilmington Avenue, ~~which causes confusion~~. This segment ~~of 120th Street, which fronts~~ fronting the MLK Medical Center, should be renamed: ~~A via a~~ community contest ~~should be organized to select an appropriate street name,~~ similar to the one that named that for “Healthy Way” on the MLK Medical Center ~~Center~~ campus.

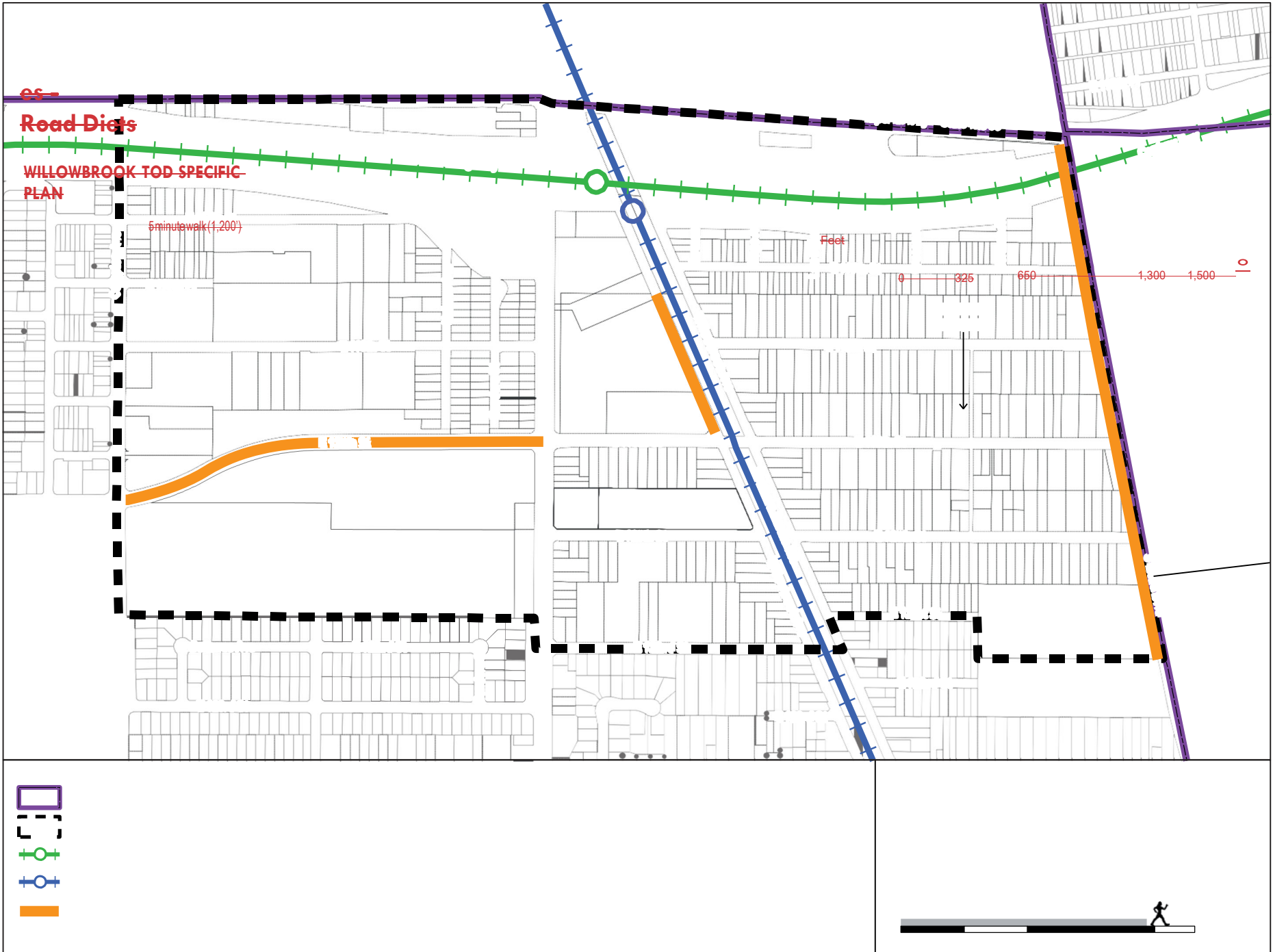


**Figure 4-2
Street System Changes -
Road Diets**

WILLOWBROOK TOD SPECIFIC PLAN



**Street-
System-
Change**



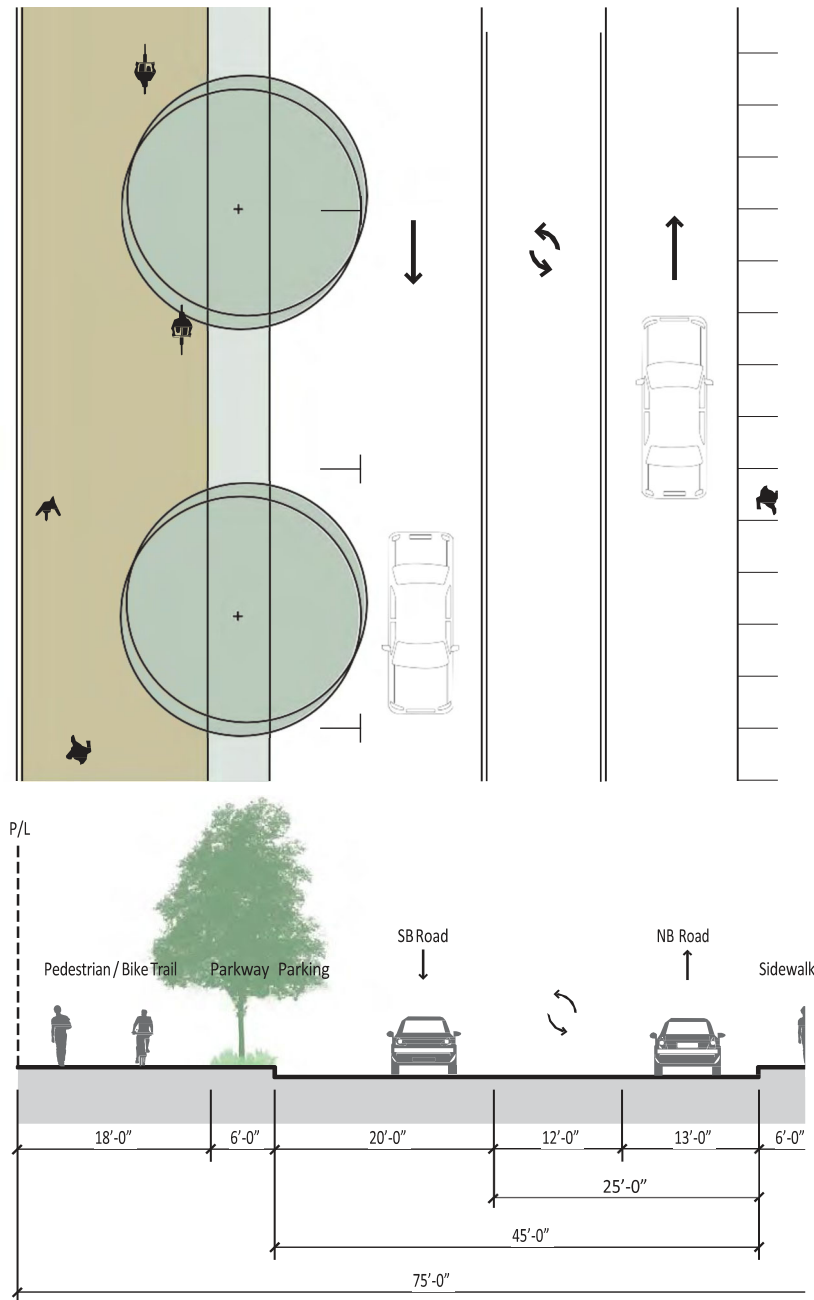
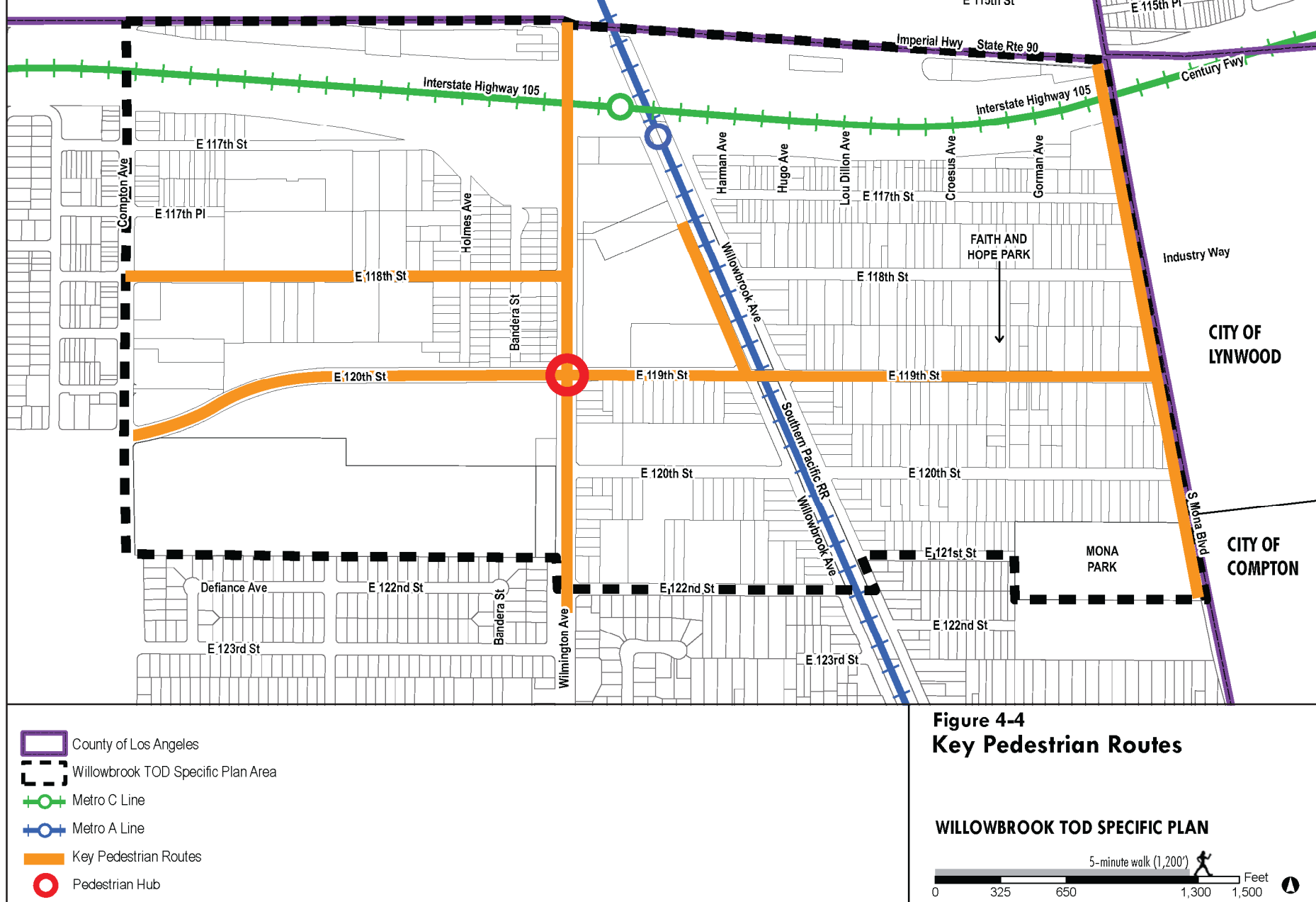


Figure 4-3: Proposed Mona Boulevard Section



Examples of a Class I bicycle path and pedestrian trail



County of Los Angeles
 Willowbrook TOD Specific Plan Area
 Metro Green Line
 Metro Blue Line

Key
 Pedestr
 ian

Routes

WILLOWBROOK TOD SPECIFIC
PLAN

5 minute walk (1,200')
Pedestrian Hub

Feet



4.4 Pedestrian Circulation

The key pedestrian routes in the Plan Area are ~~shown in Figure 4-4. The backbone of the pedestrian system is formed by~~ Wilmington Avenue in the north-south direction and 120th/119th Street in the east-west direction ~~shown in Figure 4-4.~~ These corridors connect activity centers of the Willowbrook/Rosa Parks Station, the Kenneth Hahn Plaza, and the MLK Medical Center. ~~Campus. They also cross at the~~ Their intersection ~~of Wilmington Avenue and 120th/119th Street — which~~ is the functional pedestrian hub of the Plan Area. Section 5.2C in Chapter 5 discusses and illustrates concepts for special treatment for this intersection.

~~There are four a~~ Additional key ~~elements of the pedestrian system routes. are~~ East-west 118th Street between Compton Avenue and Wilmington Avenue ~~— which connects the connects the~~ CDU campus to ~~the rest of the Plan Area activity centers.~~ Willowbrook Avenue West between 119th Street and the Willowbrook/Rosa Parks Station ~~— providing access from connects~~ residential areas to the Metro Station, ~~and East-west~~ 119th Street between Willowbrook Avenue and Mona Boulevard ~~provides access from the connects~~ residential areas to ~~the activity centers within the Plan Area. And finally,~~ Mona Boulevard ~~also provides north-south pedestrian access on the east side of the Plan Area including access connects~~ to Mona Park, MLK Elementary School, and Dr. Ralph Bunche Middle School ~~on the east side of the plan area.~~

Pedestrian Sidewalk Improvements

Sidewalks currently exist on most streets ~~in the Plan Area,~~ although some are narrow or substandard ~~in quality.~~ Sidewalks will be improved as new development occurs, ~~and through the landscaping provisions contained in this Willowbrook TOD Specific Plan.~~

Specific recommendations include:

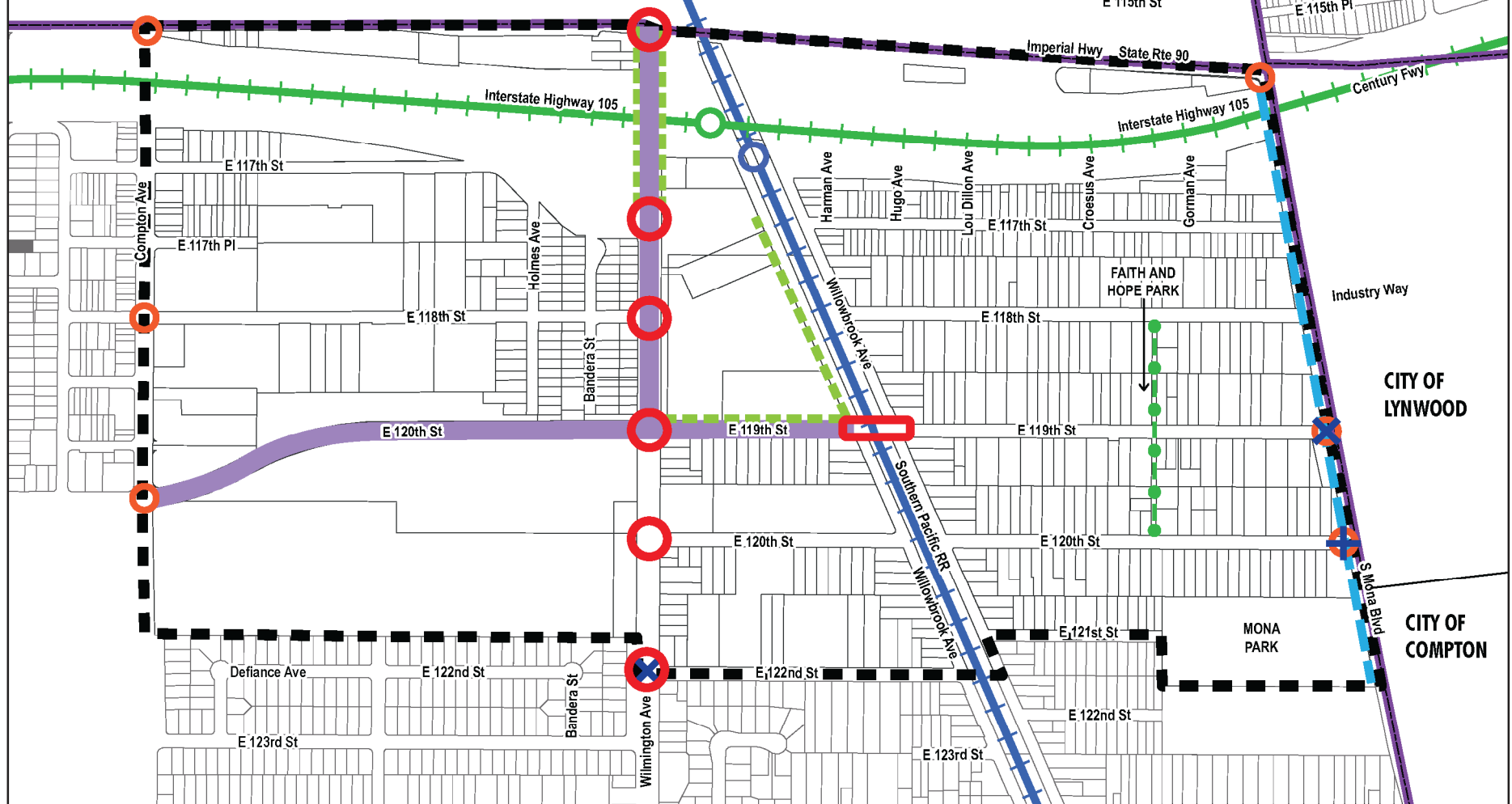
- ~~Adding a~~ A sidewalk ~~will be added~~ to the currently unpaved west side of Willowbrook Avenue West
- ~~between the~~ Willowbrook/Rosa Parks Station and 119th Street.
- Improving The sidewalks on Wilmington Avenue between the I-105 Freeway Off-Ramps and Imperial Highway ~~will be improved~~ through widening and streetscape improvements, including ~~improved~~ pedestrian-scaled street lighting.
- Widening sidewalks where feasible as new development occurs ~~Streetscape standards will provide for wider sidewalks where feasible as redevelopment occurs, for example, along~~ the north side of 119th Street between Wilmington and Willowbrook ~~Avenues and Willowbrook Avenue.~~

Pedestrian Oriented Intersection Improvements

~~To enhance the pedestrian environment and to calm traffic, a~~ A number of pedestrian-oriented and traffic calming intersection improvements will be implemented throughout the Plan Area. ~~These will be based on a menu of improvements that includes the following:~~

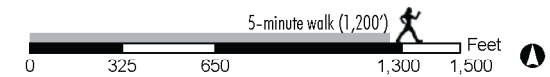
- Adding high visibility crosswalks at both existing and new intersections recommended in this Specific Plan.¹
- Adding passive pedestrian detection and pedestrian push buttons for crosswalks at traffic signals at intersections.
- ~~Adding pedestrian audio and~~ countdown ~~pedestrian signals and audio signals to~~ crosswalks ~~at~~
- ~~intersections.~~
- Adding advance stop lines to signalized intersection approaches.
- ~~Adding sidewalk bulb-outs and extensions, or reducing curb returns,~~ on intersection corners
- ~~where feasible.~~

-
- 1 Design of any high visibility crosswalks should be reviewed on a case-by-case basis, and consider ADA access, maintenance cost, and feasibility. Pavers or textured surfaces should not be considered in the pedestrian walking path of the crosswalks.*
-



**Figure 4-5
Pedestrian Improvements**

WILLOWBROOK TOD SPECIFIC PLAN



- | | |
|------------------------------------|--|
| County of Los Angeles | Pedestrian Improvements - Key Location |
| Willowbrook TOD Specific Plan Area | Pedestrian Improvements - Key Location |
| Metro C Line | Add Pedestrian Trail |
| Metro A Line | Pedestrian Improvements - Secondary Location |
| New Traffic Signal | County Willowbrook Area Access Improvements |
| New Pedestrian Signal | Green Alley of Family and Fellowship |

- | | |
|------------------------------------|--|
| Willowbrook TOD Specific Plan Area | Pedestrian Improvements - Key Location |
| Metro Green Line | Add Pedestrian Trail |
| Metro Blue Line | Pedestrian Improvements - Secondary Location |
| New Traffic Signal | County Willowbrook Area Access Improvements |
| New Pedestrian Signal | Green Alley of Family and Fellowship |



For all bulb-out locations, where two curb ramps are proposed per corner, curb ramps shall be situated close enough to the intersection so that pedestrian visibility is not hindered. Any proposed landscaping shall not interfere with sight distance of pedestrians waiting to cross. The intersection geometrics shall be evaluated to determine if bulb-outs can fit within the proposed curb-to-curb width.

~~These measures will~~ Bulb outs and curb extensions provide additional sidewalk space and ~~facilitate pedestrian circulation, by reducing~~ reduce the width of the roadway ~~for pedestrians have to~~ to cross while making, providing additional sidewalk space, and making pedestrian crossings more visible to pedestrians and motorists. Recommended locations are shown in Figure 4-5 and a typical diagram is illustrated in Figure 4-6.

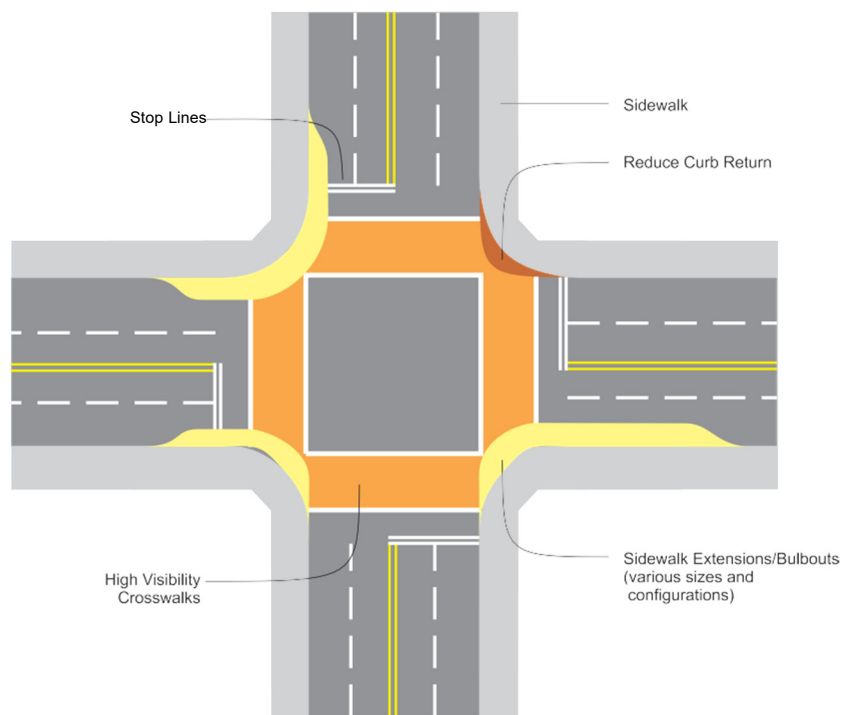


Figure 4-6: Pedestrian-oriented intersection improvements

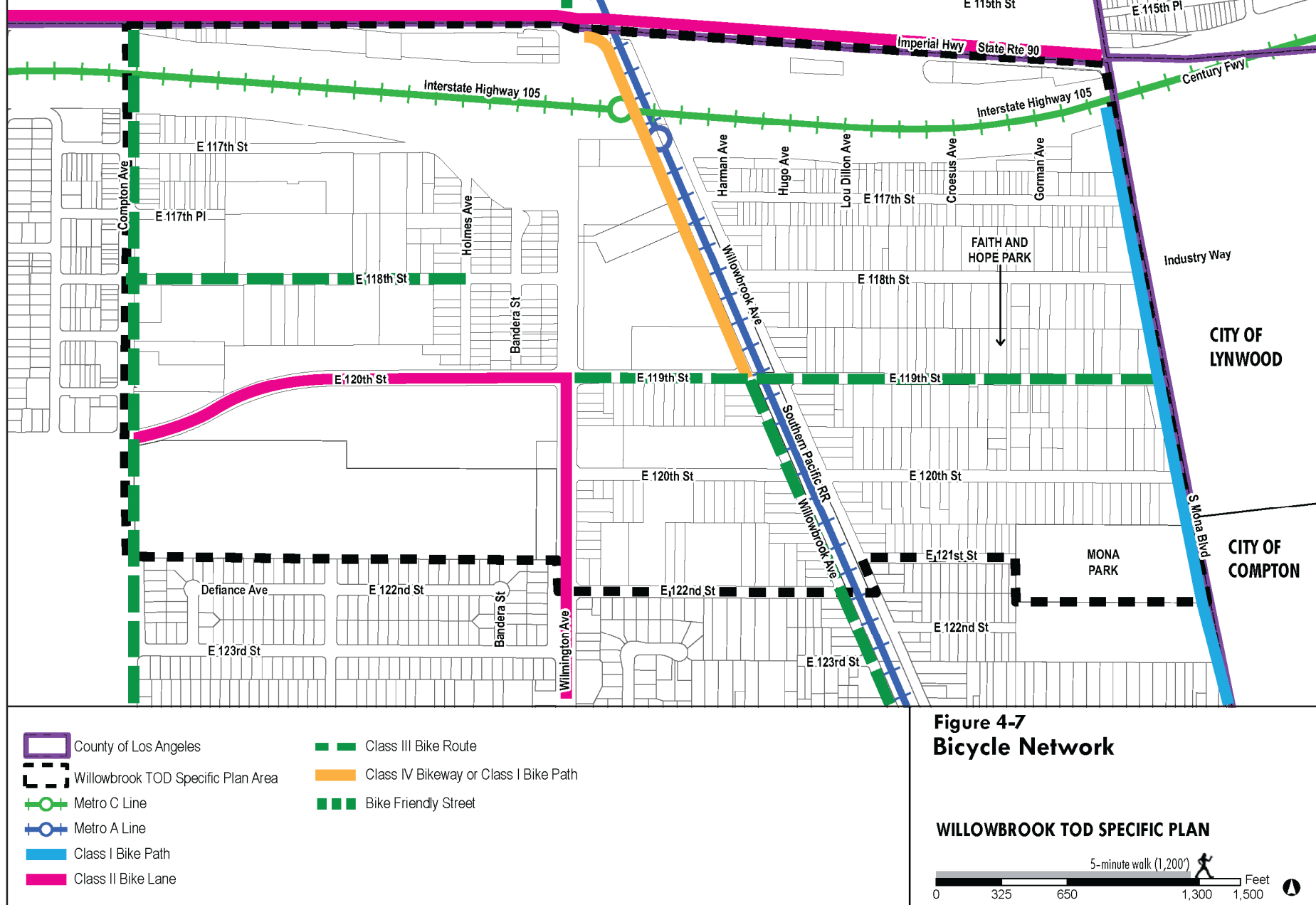
The type of improvements will follow the concepts identified in the “Los Angeles County Transit-Oriented Districts Access Study, SCAG and County of Los Angeles.” The specific improvements to be implemented at each location will be determined following detailed design studies to determine applicability and feasibility and the ultimate configuration.

The improvements at Wilmington Avenue and I-105 eastbound ramp will add a crosswalk across Wilmington Avenue for access to the Willowbrook/Rosa Parks Station. Other improvements being considered by DPW include:

- New traffic signals at Wilmington Avenue and 122nd Street and at Mona Boulevard and 119th Street.
- A signalized pedestrian crosswalk on Mona Boulevard and 120th Street for pedestrians crossing to Dr. Ralph Bunche Middle School.

The locations for such improvements are shown in Figure 4-5, and listed below.

- Wilmington Avenue and Imperial Highway
- Wilmington Avenue and I-105 Eastbound Ramp
- Wilmington Avenue and 118th Street
- Wilmington Avenue and 120th/119th Street
- Wilmington Avenue and 120th Street
- Wilmington Avenue and 122nd Street
- Willowbrook Avenue West and 119th Street
- Willowbrook Avenue East and 119th Street
- Mona Boulevard and Imperial Highway
- Mona Boulevard and 119th Street
- Mona Boulevard and 120th Street
- Compton Avenue and Imperial Highway
- Compton Avenue and 118th Street
- Compton Avenue and 120th Street



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**WILLOWBROOK TOD-SPECIFIC
PLAN**

5 minute walk (1,200')

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4-6:

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The type of improvements will follow the concepts identified in the “Los Angeles County Transit Oriented Districts Access Study, SCAG and County of Los Angeles.” The specific improvements to be implemented at each location will be determined following detailed design studies to determine applicability and feasibility and the ultimate configuration. However, curb extensions should not restrict the circulation of buses, trucks, emergency vehicles, and bicycles.

The improvements at Wilmington Avenue and I-105 eastbound ramp will add a crosswalk across Wilmington Avenue to facilitate access to the Willowbrook/Rosa Parks Station. If found to be warranted by DPW analyses, other specific improvements include:

- New traffic signals at Wilmington Avenue and 122nd Street, and at Mona Boulevard and 119th Street, to facilitate pedestrian crossings on long stretches of both streets currently without signalized crosswalks.
- A signalized pedestrian crosswalk on Mona Boulevard and 120th Street, to facilitate pedestrians crossing to Dr. Ralph Bunche Middle School.

4.5 Bicycle Circulation

The Specific Plan Bicycle Network, shown in Figure 4-7, includes a combination of Class I, Class II, and Class III ~~and Class IV~~ bicycle facilities that connect activity centers and neighborhoods to the Willowbrook/Rosa Parks Station, connect to adjacent communities, and provide a dedicated network for bicyclists to use safely and efficiently. The Bicycle Circulation System builds on the County’s Bicycle Master Plan. ~~Additional recommendations are provided to complete and connect the bicycle network, as described below.~~

Bicycle Facilities

- A Class I Bicycle Path consists of an exclusive bicycle facility, separated from roadways and traffic. A Bikeway, or cycle track, provides a right-of-way designated exclusively for bicycle travel within or adjacent to a roadway and which is protected from vehicular traffic. Some types of separation include, but are not limited to, grade separation, flexible posts, inflexible physical barriers, or on-street parking.
- A Class II Bicycle Lane consists of a designated striped lane along the curb lane of a street. It provides for one-way travel and is generally delineated with special striping and signage.
- A Class III Bicycle Route is for shared use ~~of bicycles~~ with automobile traffic. Signs are posted, which indicate the road also serves as a bicycle route, but no special bicycle lane for bicycles is striped. Symbols known as “sharrows” ~~There may also, however, be painted bicycle symbols on the roadways surface (known as sharrows) to indicate to motorists that bicycles also use the street.~~
- ~~A Class IV Bikeway, or cycle track, provides a right-of-way designated exclusively for bicycle travel within or adjacent to a roadway and which is protected from vehicular traffic. Types of separation include, but are not limited to, grade separation, flexible posts, inflexible physical barriers, or on-street parking.~~

~~Specific~~ Recommendations include:

- Class I Bicycle Paths or potentially a cycle track, will be implemented on Willowbrook Avenue West between 119th Street and Imperial Highway ~~— to provide access to the Willowbrook/Rosa Parks Station,~~ and on Mona Boulevard (west side) between Imperial Highway and 124th Street. ~~The facility on Willowbrook Avenue West could also be a Class IV cycle track.~~
- Class II Bicycle Lanes ~~will be implemented~~ on 120th Street between Compton Avenue and Wilmington Avenue, on Wilmington Avenue between 124th Street and 120th Street, and on Imperial Highway between Compton Avenue and Mona Boulevard.
- Class III Bicycle Routes on Compton Avenue, Willowbrook Avenue West south of 119th Street, and 119th Street between Wilmington Avenue and Mona Boulevard

- ~~Not all streets can support bicycle lanes due to insufficient width or the presence of on-street parking, which is also an important asset to the function and economic well-being of the~~

Bicycle Amenities

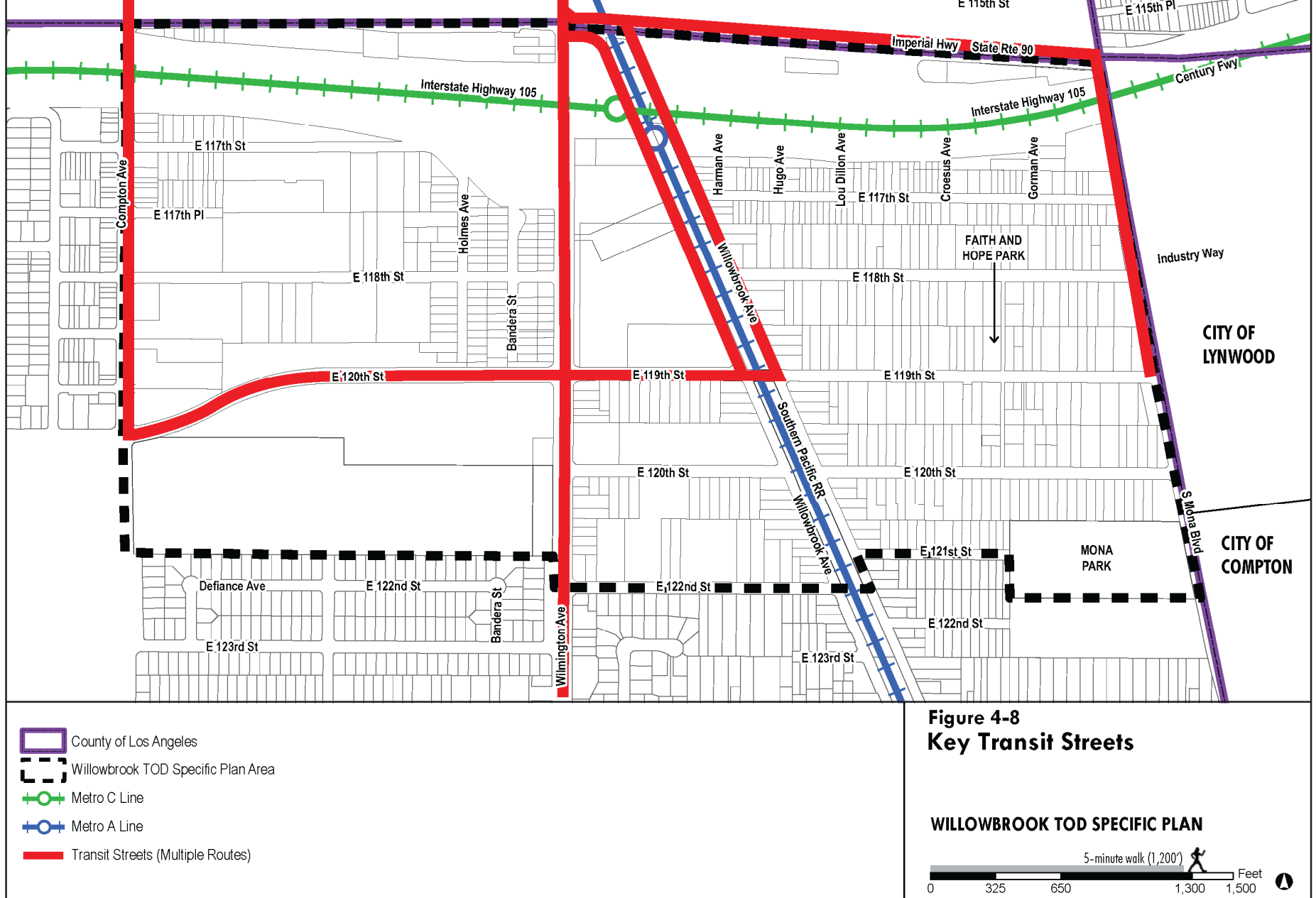
Bicycle parking can include bicycle racks and lockers, bicycle corrals (larger areas dedicated to bicycle parking), and bicycle stations. Bicycle stations can include repair facilities and a small bicycle shop as well as secure bicycle parking. Bike share programs provide bicycles that can be rented for a period of time and picked up and dropped off at any bicycle share stop in an area.

Bicycle Parking and Stations

Bicycle parking should be provided according to Title 22 for all new developments. Additional bicycle parking should be provided at Kenneth Hahn Plaza and the Willowbrook/Rosa Parks Station. Metro is planning to provide a bicycle station at the Metro station called a Metro Bike Hub. Bicycle stations and additional bike parking may also be appropriate on the MLK Medical Center Campus, the CDU Campus, and in the Northwest Subarea and should be provided by those facilities and uses.

Bike Share Program

A bike share program should be implemented in the Plan Area to encourage bicycling and improve the first/last mile connections to the Willowbrook/Rosa Parks Station. A program could be coordinated with neighboring jurisdictions.



County of Los Angeles

Willowbrook TOD Specific Plan Area

Metro Green Line

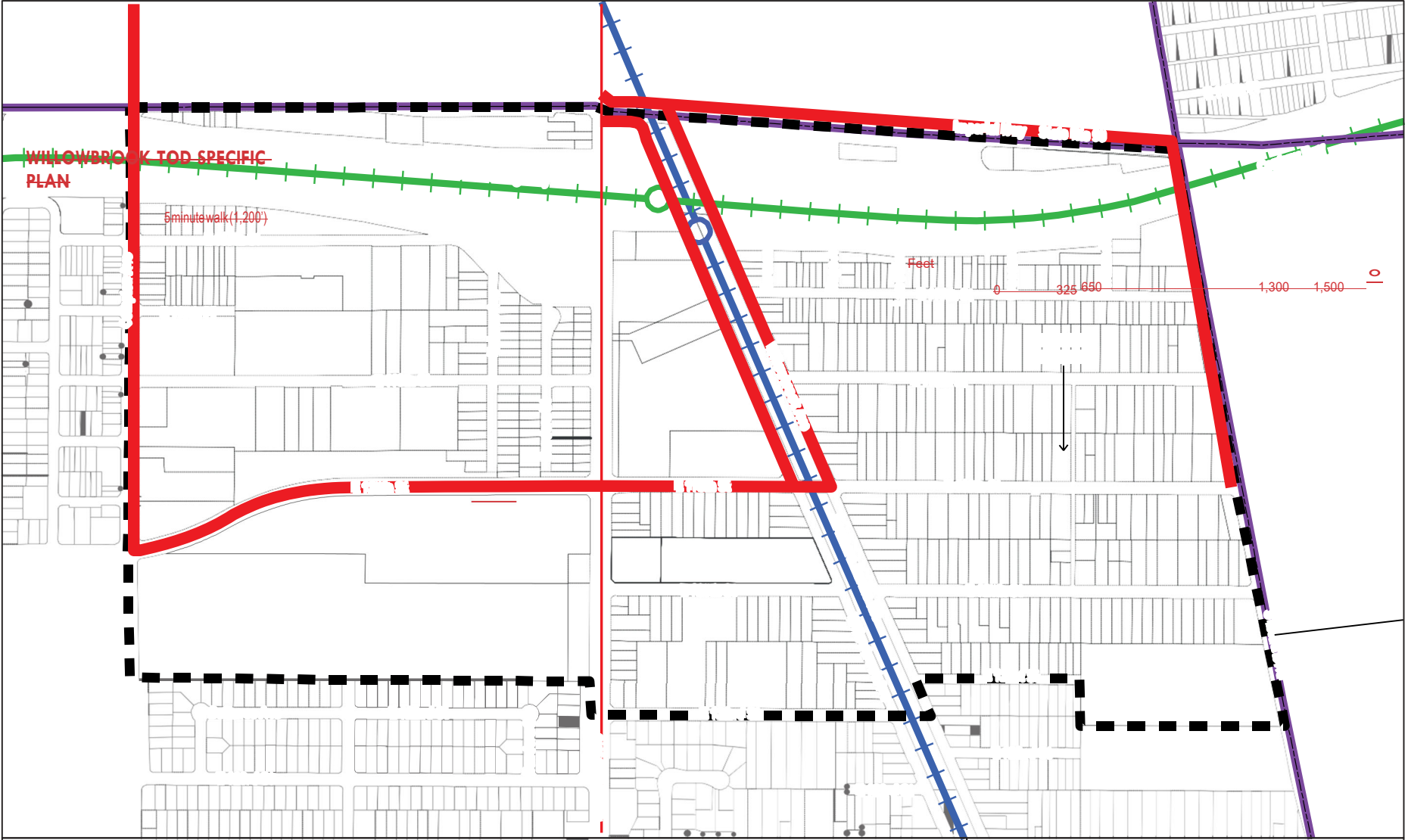
Metro Blue Line

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~~adjacent commercial uses or neighborhoods. Where there is insufficient roadway width to stripe bicycle lanes and/or to retain on-street parking, a connected network is achieved through the designation of Class III Bicycle Routes. Class III Bicycle Routes will be implemented on Compton Avenue, Willowbrook Avenue West south of 119th Street, 119th Street between Wilmington Avenue and Mona Boulevard, and on 124th Street throughout the Plan Area.~~

Bicycle Amenities

~~Bicycle parking can include bicycle racks and lockers, bicycle corrals (larger areas dedicated to bicycle parking), and at bicycle stations. Bicycle stations can include repair facilities and a small bicycle shop, as well as secure bicycle parking. Bike share programs can provide bicycles for sharing by the community where people can rent a bicycle for a period of time and pick it up or drop it off at any bicycle share stop in an area.~~

Bicycle Parking and Stations

~~Bicycle parking should be provided according to Title 22 for all new developments. Additional bicycle parking should be provided at key public locations to also serve existing uses. This includes the Kenneth Hahn Plaza and the Willowbrook/Rosa Parks Station. Metro is planning to provide a bicycle station at the rail station called a Metro Bike Hub. Bicycle stations should also be provided on the MLK Medical Center Campus and the CDU Campus, and in the Northwest Subarea where offices are located. These additional bicycle stations should be provided by those facilities and uses.~~

Bike Share Program

~~A bike share program should be implemented in the Plan Area to encourage the use of bicycling — which would reduce vehicle trips and provide a healthy alternative method of transportation. It would also improve the first and last mile connections to the Willowbrook/Rosa Parks Station. While such a program could primarily be internal to the Plan Area, it should also be coordinated with any other existing or planned bike share programs in adjacent areas or the County.~~

4.6 Transit Circulation

Station Access

Metro is currently working on improvement plans for the Willowbrook/Rosa Parks Station. These are expected to include enhancements to the station facilities and platforms, ~~the provision of~~ a shuttle stop and bicycle station, and access improvements ~~to station access,~~ including a crosswalk across pedestrian railroad track crossing the tracks south of the station to Willowbrook Avenue East to neighborhoods to the east.

Transit Service

Key transit streets ~~(with that offer multiple bus routes)~~ in the Plan Area are shown in Figure 4-8. ~~It is anticipated that c~~Current bus routes will be expected to continue to serve the Plan Area focusing on the Willowbrook/Rosa Parks Station. To encourage transit use, bus stops with benches, shade, landscaping and transit service information should be located ~~encouraged~~ at quarter mile spacing, ~~to reduce walk time access to bus routes, and the provision of bus stop amenities should also be encouraged (such as benches, shade, landscaping, and transit service information).~~

Shuttle Routes

Existing shuttle routes that are operated by the County and CDU should ~~be continued to facilitate alternative modes of transportation, and to~~ provide critical access for those without a car. Shuttle providers should also ~~consider extending~~ service to accommodate second-shift hospital workers.

Additional shuttle routes should be added in the Northwest Subarea and connect to the

Willowbrook/Rosa Parks Station. New shuttle services could be provided by the private sector as part of a comprehensive Transportation Demand Management Program.

4.7 Transportation Demand Management Program

A Transportation Demand Management (TDM) Program should be included in project development to implement strategies to take advantage of the high level of transit service and to reduce both vehicle trips and demand for vehicle parking. Strategies may include subsidizing transit passes, parking cash out programs, preferential parking for carpools, and facilitating carpools and vanpools. Such programs encourage the use of transit, bicycling, walking, and ride sharing and are generally most suitable for large employers and institutional uses.

TDM strategies could be particularly attractive to employers in new office type land uses in the Northwest Subarea by reducing capital costs associated to building parking. They could also appeal to CDU and the MLK Medical Center. A Transportation Management Organization (TMO) could also be established to facilitate these programs at an area-wide level and support individual participating employers and/or buildings.

Additional shuttle routes should be added to serve new development in the Northwest Subarea and connect the land uses to the Willowbrook/Rosa Parks Station. These new shuttle services could be provided by the private sector as part of a comprehensive Transportation Demand Management Program (see Section 4.7 below).

The potential combination of the various shuttle routes could be considered, which improves the overall effectiveness of service and avoids duplication between routes, as long as the needs of individual land uses and institutions continue to be met.

4.7 Transportation Demand Management

A Transportation Demand Management Program will be developed by any development within the TOD to implement strategies, take advantage of the high level of transit service, and to reduce both vehicle trips and the number of parking spaces provided. Such programs include the encouragement of use of transit, bicycling, walking, and ride-sharing. These types of programs are generally most suitable and most effective for large employers, institutional uses, and office uses and could be attractive to employers in new office type land uses in the Northwest Subarea as they could reduce the capital costs needs of building parking. The Program should include the Northwest Subarea, CDU, and the MLK Medical Center.

Transportation demand management and trip reduction strategies could include, but are not be limited to:

- Encouraging use of transit, including subsidizing transit passes.
- Parking cash-out programs.
- Encouraging ride-share.
- Providing preferential parking for carpools.
- Facilitating formation of carpools and vanpools.
- Site and building design to facilitate use of transit, bicycling, and walking.

A Transportation Management Organization (TMO) could also be established to facilitate these programs at an area wide level and support individual employers and/or buildings in participating to the fullest extent possible.

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Streetscape Improvements

5.1 Introduction

The purpose of this chapter is to describe recommended streetscape improvements in the Plan Area that serve to fulfill the Specific Plan goal of improving to improve the quality of life for residents with improvements to the public realm. Some of these recommendations are conceptual and the County will need to further assess and supplement these recommendations, as noted, on a case-by-case basis.

Streetscape improvements are intended to enhance and unify the visual and spatial experience of bicyclists, pedestrians, and drivers f the driver, pedestrian, and the bicyclist, and help provide key linkages between the activity centers and neighborhoods. Some recommendations are conceptual and should be evaluated on a case-by-case basis in the Plan Area. The streetscape gives the user a sense of direction and place. These enhancements also enhance connectivity and the ease of movement for pedestrians and bicyclists, supporting healthy transportation options.

5.2 Streetscape Improvements

Recommended streetscape improvements consist of an interrelated palette of street trees, street furniture, street lighting, wayfinding signage, public art, and landscaping. Public art can also have a role in the streetscape palette. These elements are illustrated in (See Figure 5-1), and described in more detail in the following sections:

~~A. Willowbrook Area Access Improvements Project~~

DPW has streetscape improvements planned for Wilmington Avenue (Imperial to East 120th Street) and East 120th/East 119th Street (Compton to Willowbrook) as a part of the Willowbrook Area Access Improvements (WAAI) Project. The streetscape design includes new sidewalks, Class II bicycle lanes, solar pedestrian lights, street trees, street furnishings, bus shelters, and Low Impact Development features such as storm capture underground storage facilities and permeable pavers. The WAAI Project also includes signage and wayfinding services with the theme of “willow and brook.” The Specific Plan builds on this project by continuing the design themes of the WAAI Project to the rest of the Plan Area.

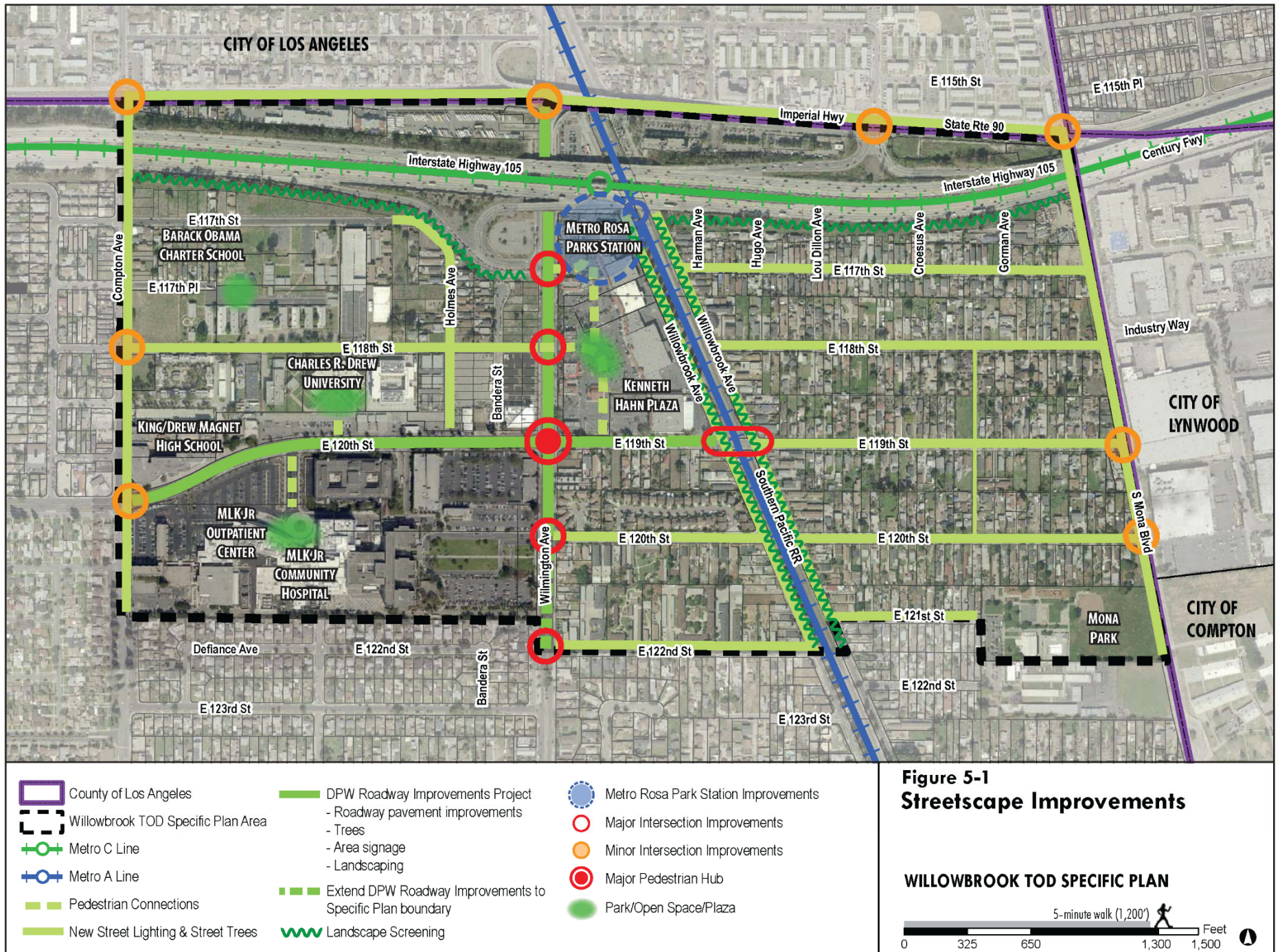
A. Willowbrook Area Access Improvements Project

DPW has streetscape improvements planned for Wilmington Avenue (Imperial to East 120th

Street) and East 120th/East 119th Street (Compton to Willowbrook) as a part of the Willowbrook Area Access Improvements (WAAI) Project. The streetscape design includes new sidewalks, Class II bicycle lanes, solar pedestrian lights, street trees, street furnishings, bus shelters, and Low Impact Development features such as permeable paving and storm capture underground storage facilities. The WAAI Project also includes signage and wayfinding services with the theme of “willow and brook.” The Specific Plan carries the design themes of the WAAI Project to the rest of the Plan Area.

B. Green Alley Project

DPW is installing a green alley between East 118th Street and East 120th Street called “The Green Alley of Family and Fellowship.” The project connects East 118th Street to Faith and Hope Park at East 119th Street, and then continues to East 120th Street near the access way to Mona Park. It retains residents’ vehicular access to their homes while making the alley more pedestrian-friendly through the use of~~consists of~~ colored concrete pavement; curb and driveway reconstruction at the alley entrances; crosswalk enhancements; pedestrian scale lighting; monument and wayfinding signs; and decorative planter walls distributed along its length ~~that hold potted plants~~ that neighbors can use to grow vegetables, berries, and flowers.



C. Streetscape Improvements at the Intersection of Wilmington Avenue and East 120th/East 119th Street

The two major pedestrian streets ~~within the Plan Area~~ are Wilmington Avenue in the north-south direction and East 120th/East 119th Street in the east-west direction. These two streets connect the major activity areas ~~as well as connect~~ and across the rail tracks ~~with to~~ the residential neighborhoods to the east. They ~~also intersect to form intersection forms~~ the pedestrian core of the Plan Area. ~~This intersection is the center of the Plan Area~~ and should have special treatments ~~to mark it as such~~. Building on the improvements planned ~~per that build on~~ the WAAI Project (~~see, the Specific Plan proposes additional improvements at this intersection as illustrated in~~ Figure 5-2). These include a bulb-out on the southeast corner ~~and special paving designs~~. A gateway structure ~~across Wilmington Avenue~~ could also be appropriate. Examples of gateway structures from other communities are shown ~~below~~.

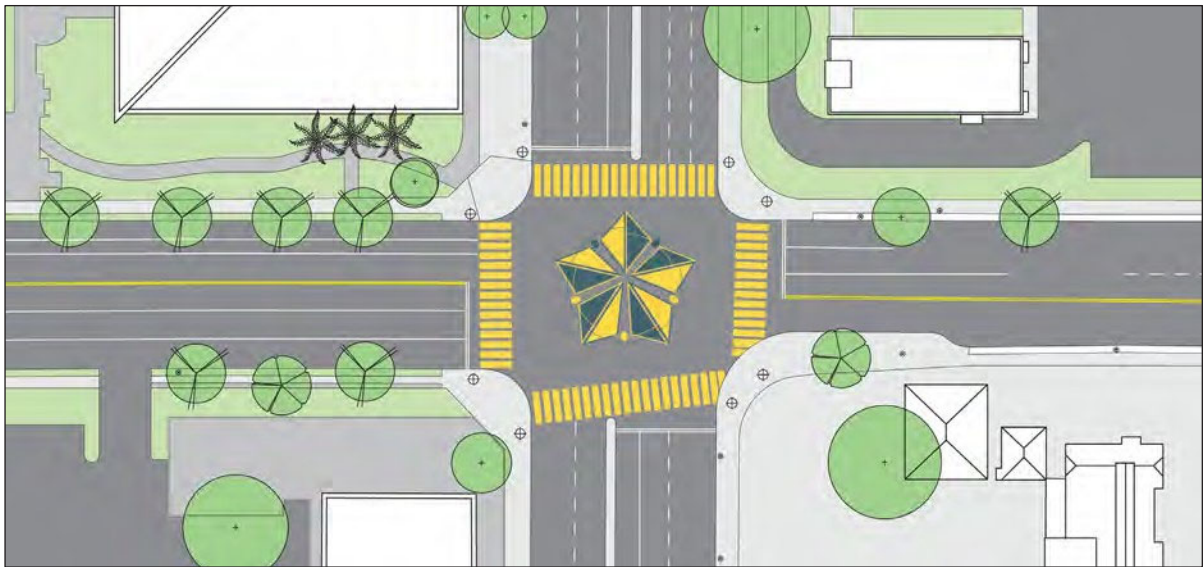
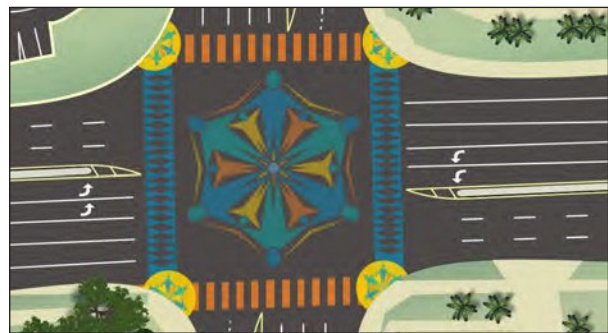


Figure 5-2: Wilmington Avenue and East 120th/East 119th Street intersection



Examples of paving designs that highlight special intersections.

intersection, and additional paving designs to mark this intersection as special. A gateway structure across Wilmington Avenue could also be explored. Examples of gateway structures from other communities are shown below:



Gateway structures that mark entries ~~to special areas~~ can also be used to celebrate the heart of a community.

D. Pedestrian Amenities

~~Elements of an Engaging, pedestrian-oriented streetscape include:~~ street trees (described in Section ~~5.23E~~), street lights, street furniture, wayfinding, and landscaped open space. ~~In some instances, public art and water features may can~~ also be appropriate ~~where it serves to interpret and draw attention to the history or culture of the area.~~ In general, ~~pedestrian amenities should be~~ focused along ~~the~~ major streets and locations that have the ~~highest amount of most~~ pedestrian activity.

- ~~1. Street Furniture. Various styles of street furniture can be found in the Plan Area.~~ A consistent palette of street furniture will help unify the area, ~~as well as and~~ provide essential seating, trash receptacles and bicycle racks. ~~As part of The WAAI Project, is a first step in this process. As a part of this project, existing bus shelters, benches, trash receptacles and bicycle racks fixtures will be replaced or refinished installed to match the selected design theme. The WAAI street furniture palette is shown below. Trash receptacles similar to those proposed by the WAAI Project should be added at all intersections in the Plan Area. The feasibility of installation will depend on the site conditions at each intersection. ADA accessibility requirements must be followed to ensure that a clear path of travel is not impeded. Additional trash receptacles may be installed as pedestrian activity levels increases and these should be continued throughout the Plan Area.~~

- a. ~~Compatible street furniture Site furnishings (benches, trash receptacles and bicycle racks, etc.) are also is also encouraged in setback areas, paseos, plazas, and courtyards and other publicly accessible spaces as appropriate and where access will not be impeded. The selected site furnishings should be compatible in style with the buildings and selected to bring comfort, scale~~

and design expression to the streetscape. It also must be highly durable and easy to maintain. All elements of the furniture palette should be uniform.



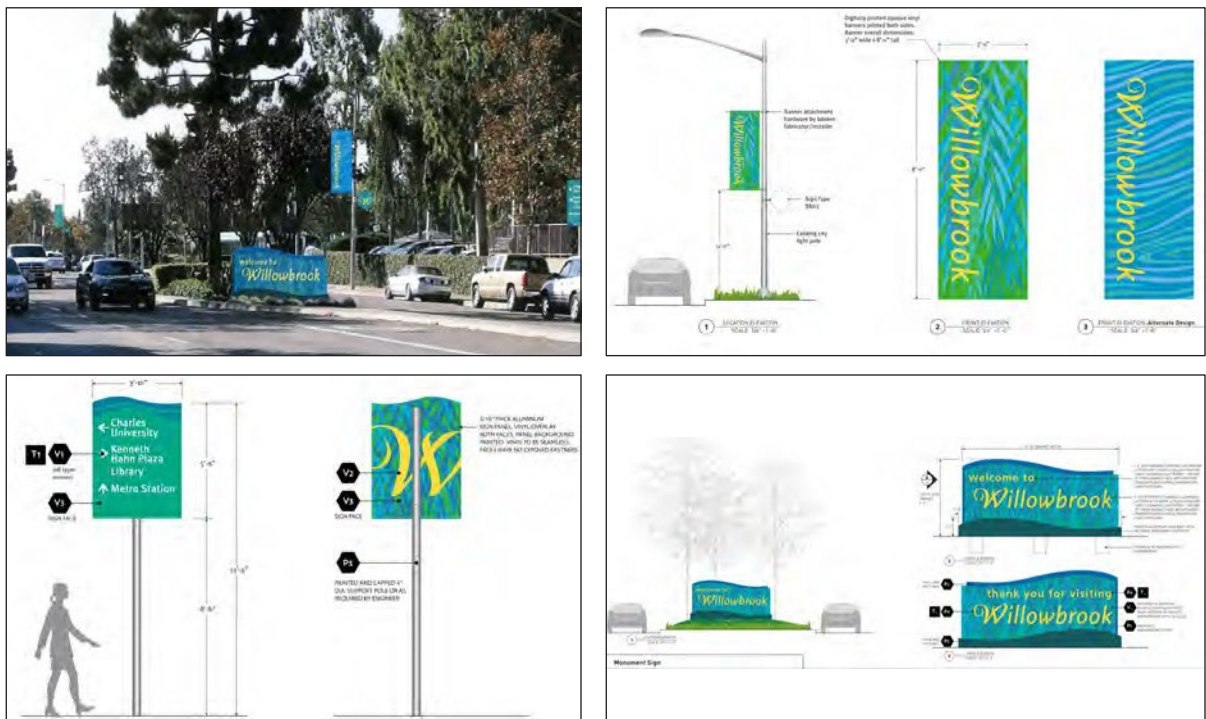
The selected street furniture palette in the Willowbrook Area Access Improvements project includes Landscape Forms' Presidio bench and trash receptacle. (Source: County of Los Angeles Department of Public Works)

2. Wayfinding. Wayfinding helps people orient themselves in physical space and navigate from place to place. A system of wayfinding signs, markers, and monument signs with a consistent design is part of the WAAI Project. This wayfinding should be expanded to the entire Plan Area and particularly at the Willowbrook/Rosa Parks Station.



The selected street furniture palette in the Willowbrook Area Access Improvements project includes Landscape Forms' Presidio bench and trash receptacle in a silver metallic finish. (Source: County of Los Angeles Department of Public Works)

2. Wayfinding. Wayfinding helps people orient themselves in physical space and navigate from place to place through effective signage, markers, and/or monuments. It is how people choose a path within the built environment. A system of wayfinding signs and entry monument signs is a part of the WAAI Project. As shown below, these signs have a consistent design with a coordinated color palette that creates a unique theme (willow and brook) that is both recognizable and sensitive to its context. The Specific Plan recommends expanding this wayfinding system to the entire Plan Area. One of the key locations for this expansion is at the Willowbrook/Rosa Parks Station. Directional signage at this location should direct motorists and pedestrians to the rest of MLK Medical Center, CDU, Kenneth Hahn Plaza, as well as other public facilities and public parking.



Banners, directional signs and monument signs are a part of the Willowbrook Area Access Improvements Project. (Source: County of Los Angeles Department of Public Works)

3. Street Lighting. Lighting promotes safety and helps to create an appealing walkable environment. Lighting provides spatial definition to the sidewalk, adds ambiance to neighborhood settings, and affords a sense of security to users. The WAAI Project adds pedestrian-scaled lights

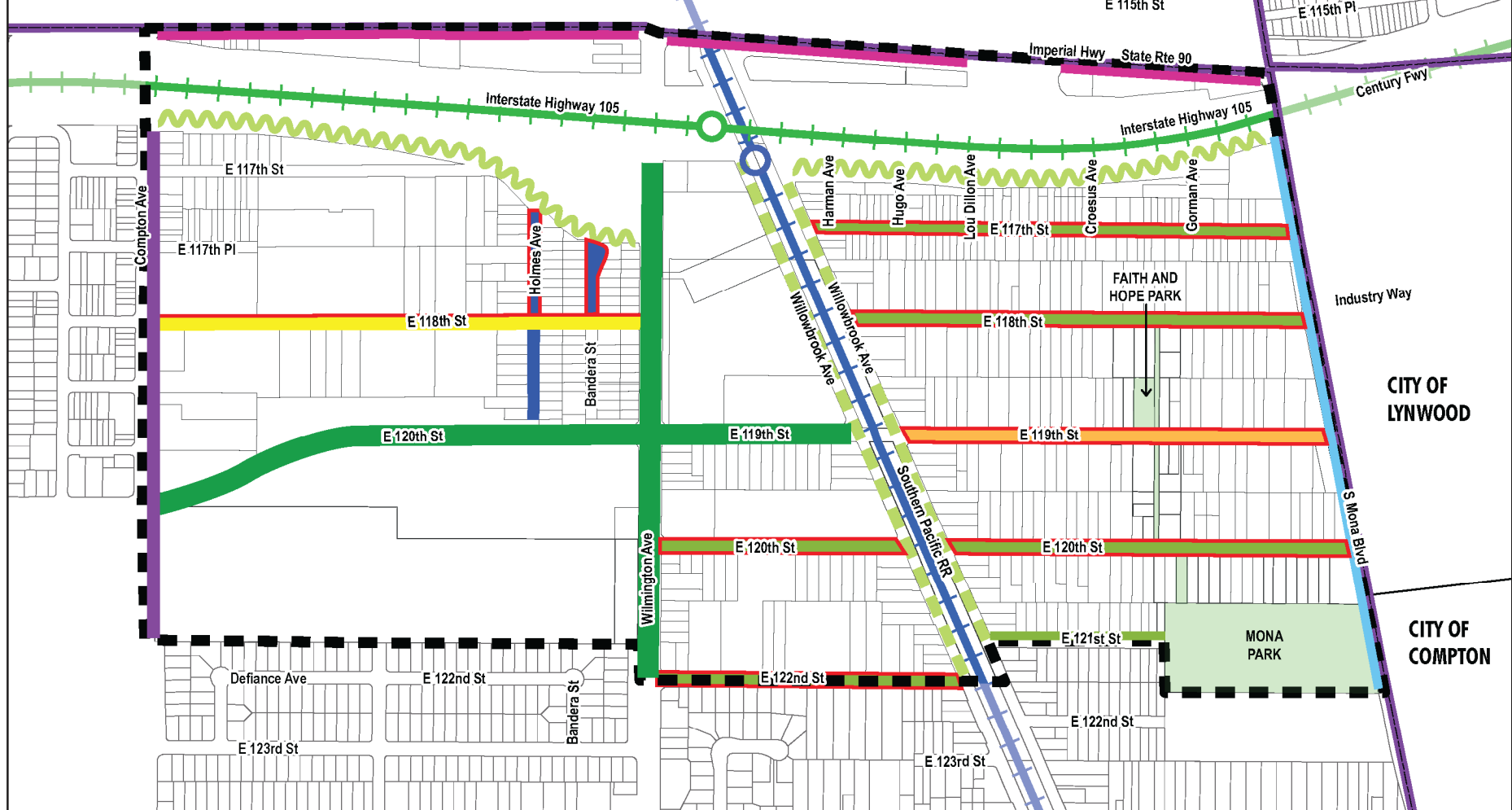
~~to recommends lighting on the key pedestrian streets. The Specific Plan recommends adding pedestrian-scaled lighting to the and that lighting should be extended to all streets in the Plan Area. Light fixtures should adhere to Dark Sky Association guidelines and Southern California Edison (SCE) standards for maintenance.~~

- ~~4. **Public Art.** Public art should be incorporated at the Willowbrook/Rosa Parks Station and early in project design particularly along major streets and at activity centers.~~

~~3.~~

E. Street Trees

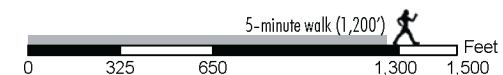
~~A mix of street tree species can be found in the Plan Area. A complimentary palette of carefully selected street trees will provide shade and a pleasant sidewalk experience, add seasonal color, and expand the urban forest. Table 5-1 and Figure 5-3 illustrate the street tree designations for the streets within the Specific Plan area. Figure 5-4 illustrates the Street Tree Palette from DPW Urban Forestry Unit's Maintenance District Street Tree List and street trees planned in the WAAI Project are incorporated. Tree spacing recommendations for each street and tree type noted in Table 5-1 are based on the canopy size of a mature tree of each species, the land uses along the streets, presence or absence of overhead power lines, and the available planting area. Final placement will be determined by DPW following all street tree and planting guidelines.~~



- | | | |
|--|------------------------------------|---|
| County of Los Angeles | Existing Screening Vegetation | Callery Pear |
| Willowbrook TOD Specific Plan Area | Bronze Loquat or Pink Trumpet Tree | Chinese Pistache |
| Metro C Line | Natchez Crape Myrtle | Pattern of
- Natchez Crape Myrtle
- Brisbane Box
- London Plane Tree |
| Metro A Line | Chinese Flame Tree | Turf Parkways (maintain existing) |
| Fraser's Photinia alternating with existing Canary Island Pine | Brisbane Box | |
| | Holly Oak | |

**Figure 5-3
Street Tree Designations**

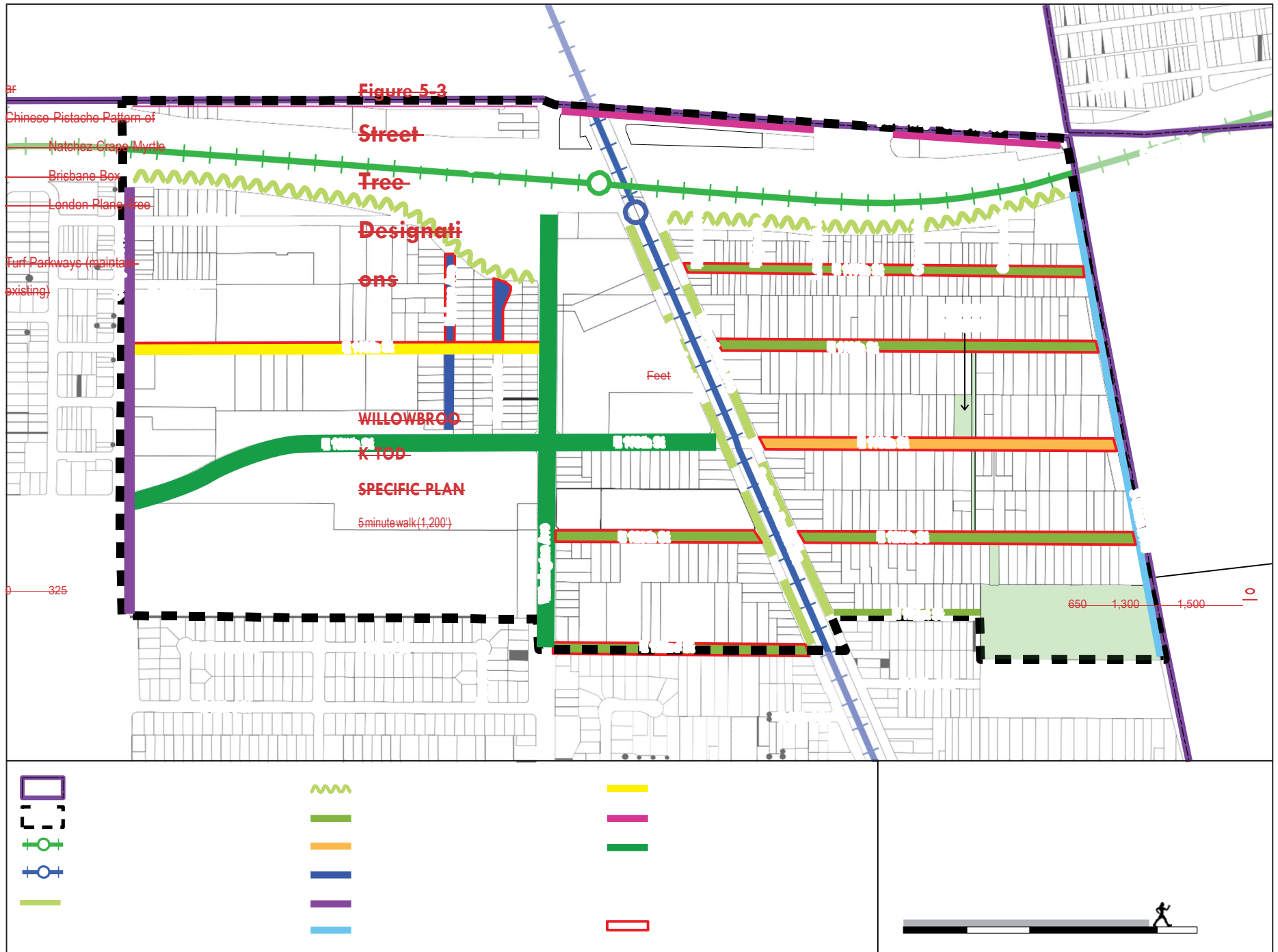
WILLOWBROOK TOD SPECIFIC PLAN



- | | | |
|--|------------------------------------|---|
| Willowbrook TOD Specific Plan Area | Bronze Loquat or Pink Trumpet Tree | Chinese Pistache |
| Metro Green Line | Natchez Crape Myrtle | Pattern of
- Natchez Crape Myrtle
- Brisbane Box
- London Plane Tree |
| Metro Blue Line | Chinese Flame Tree | Turf Parkways (maintain existing) |
| Fraser's Photinia alternating with existing Canary Island Pine | Brisbane Box | |
| | Holly Oak | |

Holly Oak

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remainder of the streets in the Plan Area. The selected light fixtures should also adhere to guidelines set forth by the Dark Sky Association to protect the area's view of stars, as well as to avoid glare. Light fixtures in the public right of way shall also follow the Southern California Edison (SCE) standards for maintenance.

4. **Public Art.** Public art can complement and enhance the Plan Area, and make positive contributions to the public's experience and understanding of life in the area. Arts and culture help to enrich everyday life, stimulate the local economy, enhance the environment, celebrate the nature, engage citizens, and foster neighborhood pride. Art brings the community together, creating situations not merely for enjoyment and beauty, but also for dialogue, thought, and growth.

— There are opportunities to encourage this evolving identity of the area by further integrating public art into the fabric and streetscape, such as at the Willowbrook/Rosa Parks Station, along major streets and activity centers.

E. Street Trees

The Plan Area has an inconsistent palette and pattern of street trees. Except for a few street segments, a mix of street tree species can be found in the Plan Area. This lack of uniformity contributes to the inconsistent and fragmented character of the Specific Plan area streets. A palette of carefully selected street trees will unify the variety of existing and future land uses in the Plan Area. In addition, street trees will provide shade, add seasonal color, define the street edge, and add to the urban forest. A uniform pattern of street trees will also increase connectivity and sense of cohesion on a pedestrian and auto-oriented scale, as well as offer a pleasant sidewalk experience.

Table 5-1 lists and Figure 5-3 illustrates the street tree designations for the streets within the Specific Plan area. Figure 5-4 illustrates the Street Tree Palette. The street trees for Wilmington and East 120th/East 119th Street (between Compton and Willowbrook Avenues), as planned in the WAAI Project, are also incorporated in this list. It also lists the existing street tree type found on each street. The designated street trees are also on the DPW Urban Forestry Unit's Maintenance District street tree lists.

Tree spacing recommendations for each street and tree type are also noted in Table 5-1. These are based on the canopy size of a mature tree of each species, the land uses along the streets, presence or absence of overhead power lines, and the available area for planting along the sidewalks and/or parkways. The final spacing will be determined by DPW and will need to consider field conditions including driveway aprons, street lights, and utilities on a block-by-block basis.

The following street tree and planting guidelines shall be adhered to for new street trees in addition to the standards established by DPW:

- All plant material shall meet the minimum standard of the American Nurserymen and Landscape Association and California Department of Agriculture Regulations, and be local container or field-grown material.
- All trees shall be planted in accordance with established County planting standards.
- The minimum planting size for a tree in parkways and medians is a 24-inch boxed container.
- Median and parkway trees shall be trimmed to retain a trunk space clear of branches of at least 6 feet from grade at maturity.
- All street trees that are not placed in parkways shall be placed in a tree well per DPW specifications. Table 5-1 lists the minimum tree well dimensions.

Figure 5-4: Street Tree Palette



Pink Trumpet Tree
Handroanthus impetiginosus
 Deciduous
 Height: 30-50 feet
 Canopy: 30 feet
 Bloom: Spring
 Water: low to moderate
 Powerline Friendly: Yes



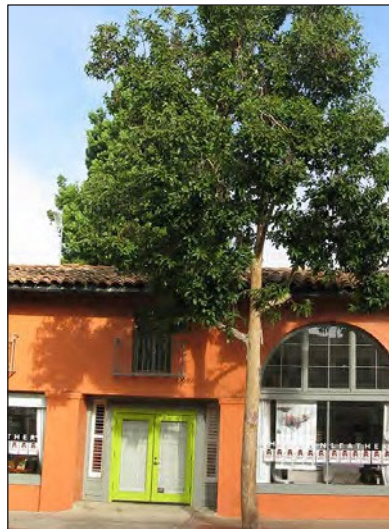
Bronze Loquat
Eriobotrya deflexa
 Evergreen
 Height: 25-30 feet
 Canopy: 20 feet
 Bloom: Winter
 Water: low to moderate
 Powerline Friendly: Yes



Chinese Flame Tree
Koelreuteria bipinnata
 Deciduous
 Height: 25-45 feet
 Canopy: 40 feet
 Bloom: Summer
 Water: moderate
 Powerline Friendly: Yes



'Natchez' Crape Myrtle
Lagerstroemia indica x fauriei
 'Natchez'
 Deciduous
 Height: 15-25 feet
 Canopy: 25 feet
 Bloom: Summer
 Water: low to moderate
 Powerline Friendly: Yes

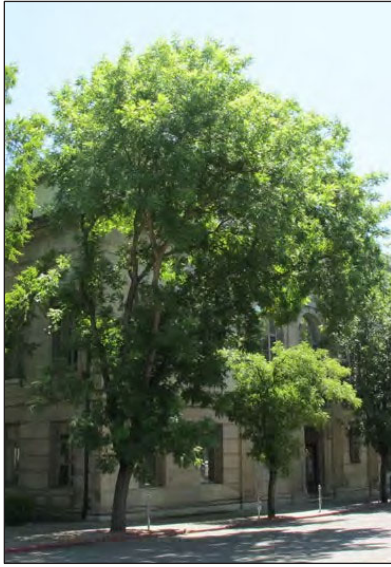


Brisbane Box
Lophostemon confertus
 Evergreen
 Height: 30-60 feet
 Canopy: 40 feet
 Bloom: Summer
 Water: low to moderate
 Powerline Friendly: No



Holly Oak
Quercus ilex
 Evergreen
 Height: 30-65 feet
 Canopy: 50 feet
 Bloom: Spring, Summer
 Water: low to moderate
 Powerline Friendly: No

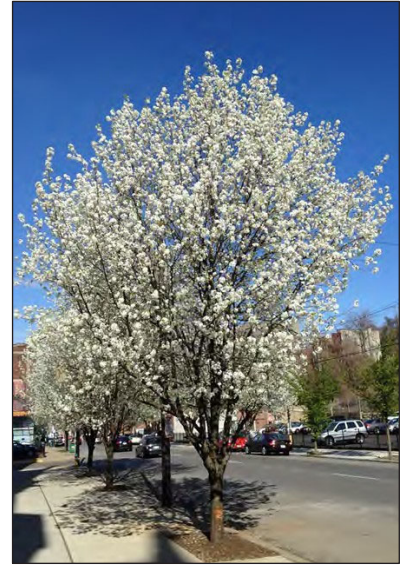
Figure 5-4 (continued)



Chinese Pistache
Pistacia chinensis
 Deciduous
 Height: 60 feet
 Canopy: 50 feet
 Bloom: Winter
 Water: low to moderate
 Powerline Friendly: No



London Plane Tree
Platanus x acerifolia
 'Bloodgood'
 Deciduous
 Height: 40-80 feet
 Canopy: 70 feet
 Bloom: Spring
 Water: moderate
 Powerline Friendly: No



Callery Pear 'Aristocrat'
Pyrus calleryana
 Deciduous
 Height: 35-45 feet
 Canopy: 35 feet
 Bloom: Spring
 Water: moderate
 Powerline Friendly: Yes



Canary Island Pine
Pinus canariensis
 Evergreen
 Height: 65-80 feet
 Canopy: 40 feet
 Bloom: None
 Water: low to moderate
 Powerline Friendly: No



Fraser Photinia
Photinia x fraseri
 Evergreen
 Height: 10-15 feet
 Canopy: 20 feet
 Bloom: Spring
 Water: moderate
 Powerline Friendly: Yes

Table 5-1: Street Tree Designations in the Specific Plan Area
Street Tree Designations in the Specific Plan Area

Street Names	Existing	Specific Plan (2015)	Recommended Spacing * (in feet)	Minimum Tree Well Dimensions ** (in feet)
North-South				
Compton Avenue (East side only)	Victorian Box <i>Pittosporum undulatum</i> Lemon Bottlebrush <i>Callistemon citrinus</i>	Brisbane Box <i>Lophostemon confertus</i>	30'-40'	5 sf
Holmes Avenue	Mixed	Chinese Flame Tree <i>Koelreuteria bipinnata</i>	30'-40'	5 sf/ Turf Parkway
Wilmington Avenue	Mixed	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i> London Plane Tree (Maintain Existing) <i>Platanus x acerifolia</i>	-- -- --	
Willowbrook Avenue	Canary Island Pine <i>Pinus canariensis</i>	Fraser Photinia <i>Photinia x fraseri</i>	Alternating between existing	Parkway
Mona Boulevard (West side only)	None	Holly Oak <i>Quercus ilex</i>	40'-50'	Turf Parkway
Bandera Street	None	Chinese Flame Tree <i>Koelreuteria bipinnata</i>	30'-40'	5 sf/ Turf Parkway
East-West				
Imperial Highway (South side only)	Mixed	Chinese Pistache <i>Pistacia chinensis</i>	30'-40'	5 sf
117th Street	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/ Turf Parkway
118th Street (West of Wilmington Avenue) (North side only)	Mixed	Callery Pear <i>Pyrus calleryana</i>	30'-40'	5 sf/ Turf Parkway
118th Street (East of Wilmington Avenue)	London Plane Tree <i>Platanus x acerifolia</i>	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/ Turf Parkway
119th Street (East of Willowbrook Avenue)	Mixed	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i>	20'-30'	4 sf/ Turf Parkway
120th Street (East of Wilmington Avenue)	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/ Turf Parkway
120th Street (West of Wilmington Avenue)	Canary Island Pine <i>Pinus canariensis</i>	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i>	-- --	
121st Street (North side only)	None	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/ Turf Parkway
122nd Street (North side only)	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/ Turf Parkway

*.Recommended spacing ~~is based on~~ considers the canopy size of mature ~~tree~~ tree canopy, available planting areas ~~along~~ sidewalks/parkways, and adjacent land uses.

**~~Minimum Tree Well Dimension:~~ Optimal sizes provided. ~~Actual size and planting shall be dependent on sidewalk width, adjacent land uses and tree type.~~

F. Parks and Open Space

Existing open space resources in the Plan Area include Mona Park, Faith and Hope Park, and the MLK Fitness Garden. Mona Park is a 8.4-acre recreational facility that includes a softball and multi-purpose fields, a gymnasium, and a swimming pool ~~and. It~~ features year-round recreational programming including youth sports, the summer lunch program, after-school snacks, and after-school day camp programs. ~~The~~ Faith and Hope Park opened in 2016 ~~on the corner of East 119th Street and will be connected north to 118th Street and south to Mona Park via~~ and the planned Green Alley of Family and Fellowship. ~~Another~~ ~~Other~~ important open space resources ~~are the MLK Fitness Garden at the MLK Center for Public Health that incorporates a public walking path with fitness equipment and the planned Fellowship Garden of Love at Holmes Community Garden is the MLK Fitness Garden at the MLK Center for Public Health. Primarily located along the 120th Street frontage, the MLK Fitness Garden incorporates a walking path and par course fitness equipment for employees and the public.~~



Faith and Hope Park opened in 2016 along East 119th Street.



MLK Fitness Garden is located along 120th Street.



Mona Park connects to East 120th Street along a landscaped paseo (shown on the right).

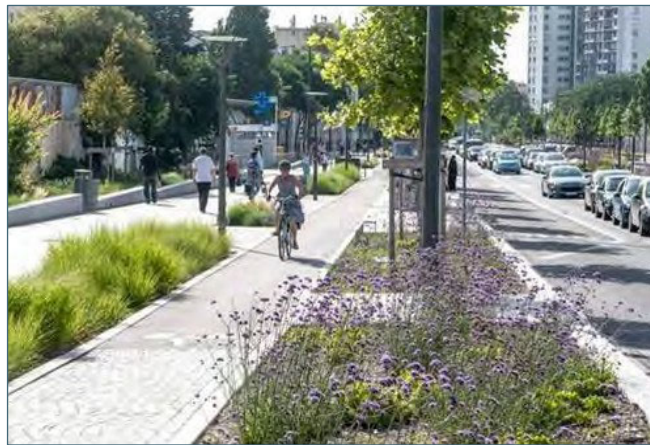
The recommended strategies for parks and open space build upon the existing and planned park spaces in the Plan Area. These strategies include:

- Exploring joint-use opportunities policy with the schools ~~to better utilize existing and future open space resources.~~
- Encourage ~~Encouraging~~ new development to provide public open space as a community benefit. Potential locations are shown in Figure 5-1.
- Considering building pocket parks and community gardens on County-owned vacant lots.
- ~~Connect existing and future parks and open spaces with the street network to allow easy pedestrian~~
- ~~and visual~~ Improving bicycle and pedestrian access to existing and future parks and open spaces.

G. Mona Boulevard Bicycle/Pedestrian Path

~~Mona Boulevard provides access from the residential areas to the key activity centers in the Plan Area.~~ Mona Boulevard ~~also~~ provides north-south pedestrian access on the east side of the Plan Area including access to Mona Park, the MLK Elementary School and the Dr. Ralph Bunche Middle School.

Mona Boulevard ~~from between~~ the I-105 Freeway to 124th Street ~~will is proposed to~~ be converted from a ~~four lane four lane~~ street to a three-lane street, ~~and with~~ a pedestrian/bicycle trail ~~installed on along~~ the west side of the street ~~that meets~~. ~~Parking will be retained on the west side of the street.~~ ~~The pedestrian/bicycle trail shall include a paved section to meet~~ Class I design guidelines. ~~The design of the trail will be subject to DPW requirements. A typical cross section is shown in~~ Figure 4-3 in Chapter 4. ~~of this Specific Plan illustrates a typical cross section of reconfigured Mona Boulevard.~~



The Mona Boulevard Bicycle/Pedestrian Path ~~will provide for (a Class I Bicycle Path) connection, completing the bicycle network in the Plan Area.~~ ~~The pedestrian path~~ will connect ~~the~~ residential neighborhoods with Mona Park, MLK Elementary School and Dr. Bunche Middle School.

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Infrastructure

6.1 Introduction

The purpose of this chapter is to provide an overview of existing and planned infrastructure facilities, and to outline recommended infrastructure upgrades for the Plan Area. The recommended upgrades are based on analyses of the capabilities and capacities of existing facilities—and projected infrastructure needs based on build-out of the Willowbrook TOD Specific Plan. Figure 6-1 shows how the Plan Area has been divided into “groups” for the purposes of the analyses.

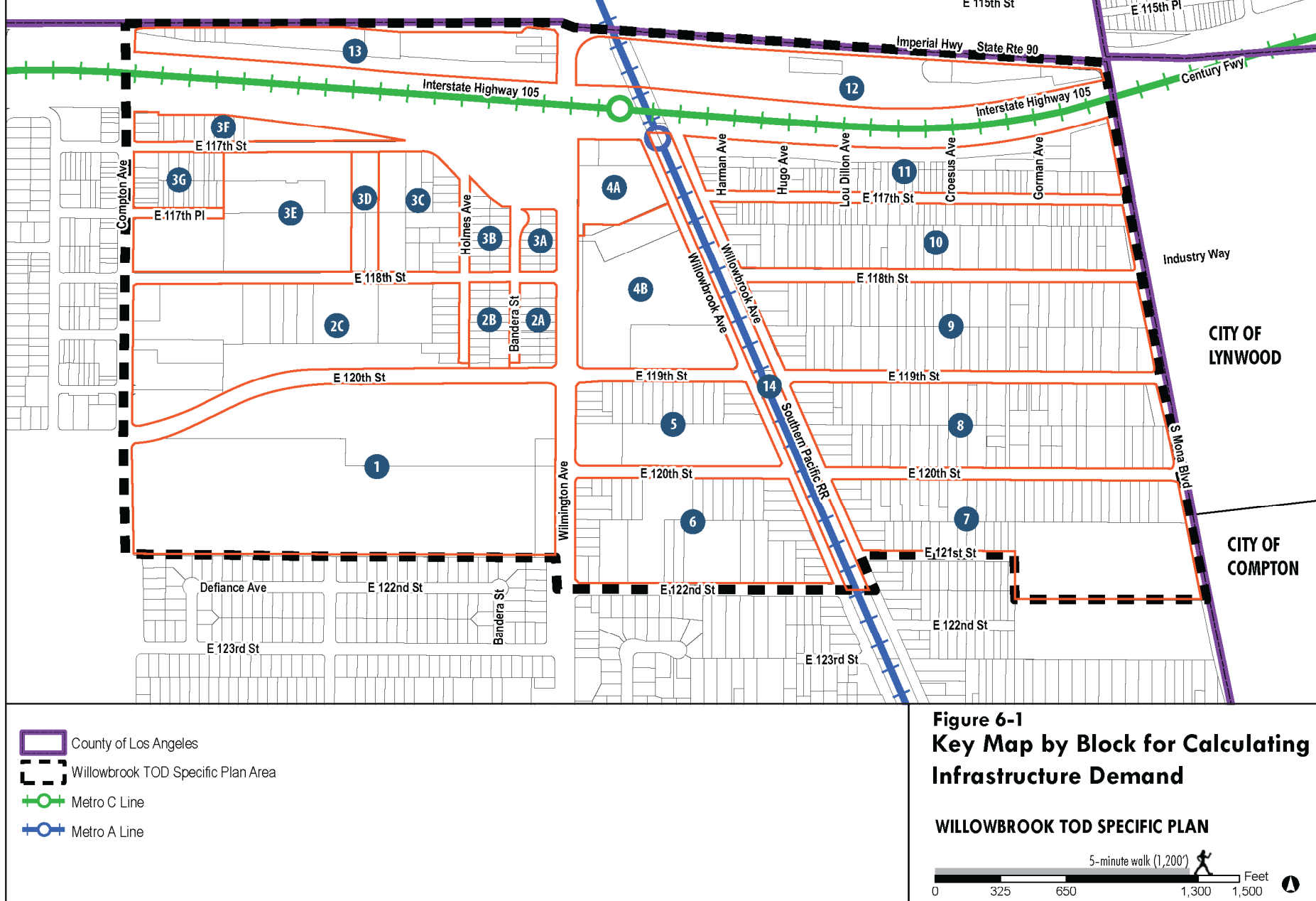
6.2 Water System

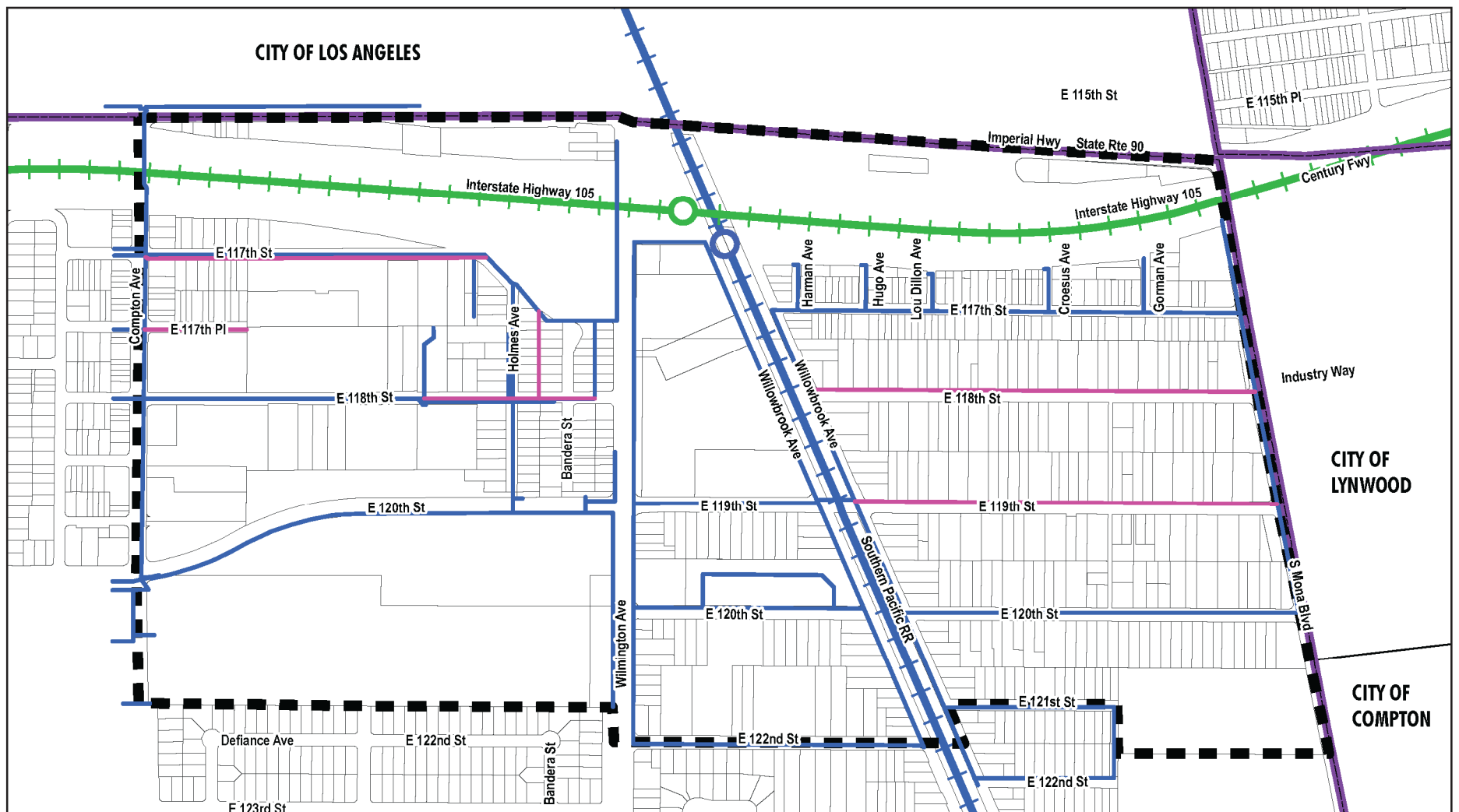
Three water purveyors serve the Plan Area: Liberty Utilities (formerly Park Water Company), Golden State Water Company and Los Angeles Department of Water and Power (LADWP). Park Water Company provides water service to the area west of Wilmington Avenue, and Golden State Water Company provides water service to the area east of Wilmington Avenue. There are potable water lines under almost all streets and alleys within the Plan Area. The sizes of the potable water lines vary from 4 to 12 inches. The existing pipes that distribute water services in the Plan Area include steel pipe (ST), asbestos cement pipes (AC), ductile iron pipes (DI), and cast iron pipes (CI). Figure 6-2 shows the layout of the existing water lines. This figure also illustrates the upgrades necessary to meet the projected demand.

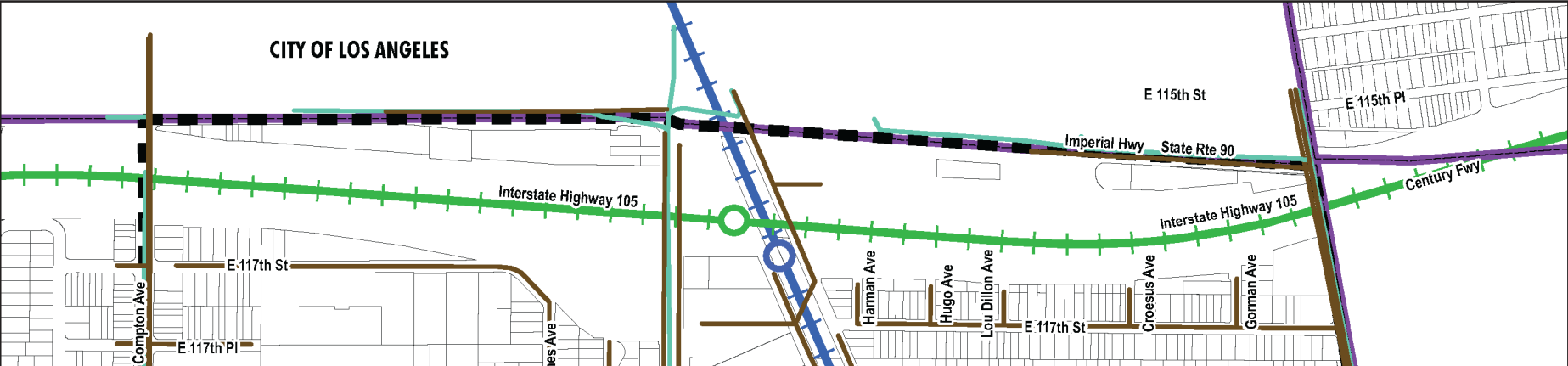
As shown in Figure 6-2, except for Group 14 (which remains unchanged), almost all development capacity groups under total build-out of the Specific Plan have some increase in land use density, which in turn results in an increase in water demand. Most of the existing water lines in the Plan Area are 8 inches in diameter and above. The 8 inch pipes generally have the capacity to handle the increase in water demand/load under total build-out of the Plan Area. However, to meet the demand of water consumption, including water services for fire prevention purposes, such as demand of new fire hydrants and fire sprinkler systems in the new developments in the post total “build-out” condition, the following upgrades in water line pipe sizes are recommended:

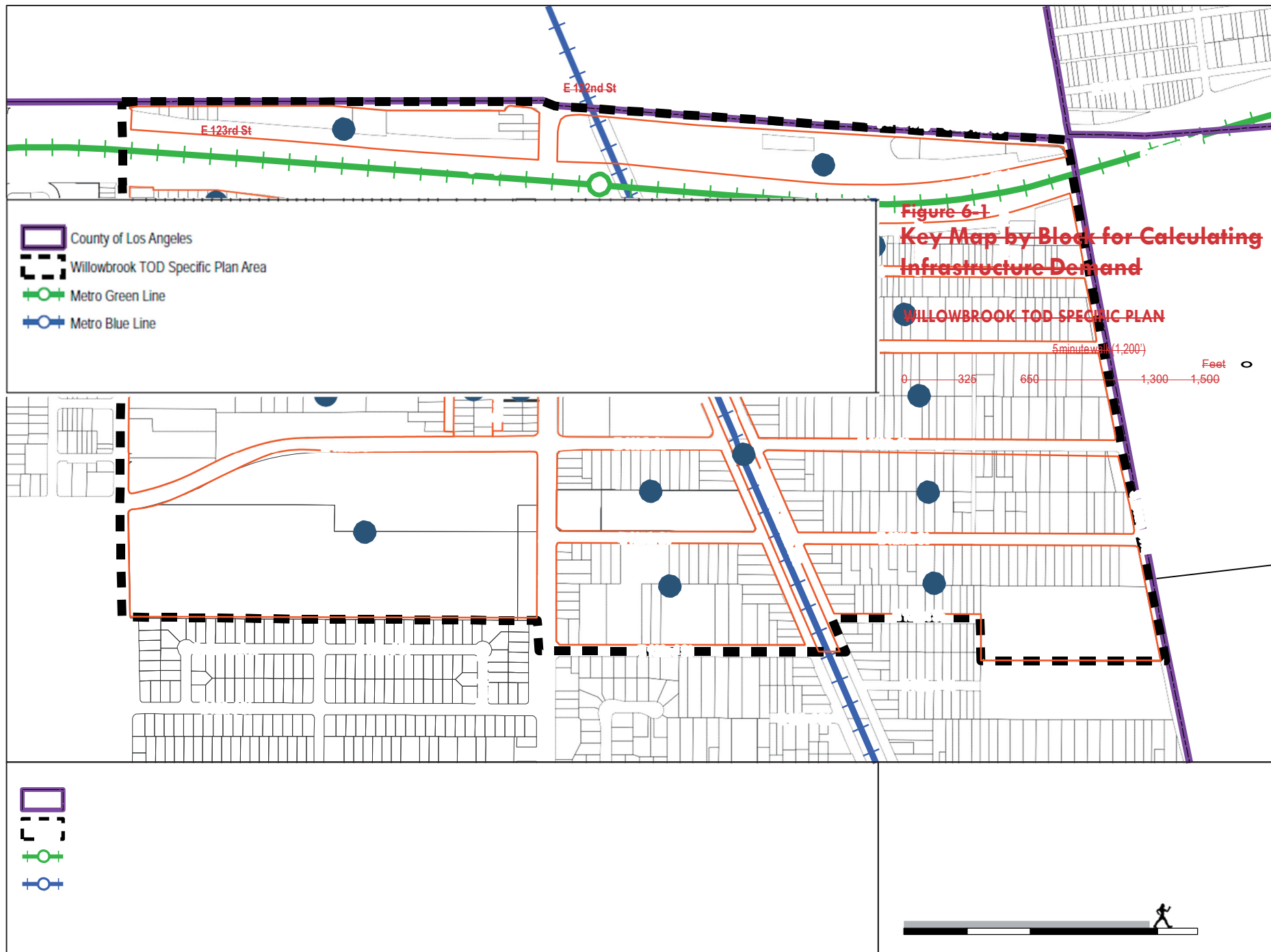
Group 2A, 2B:

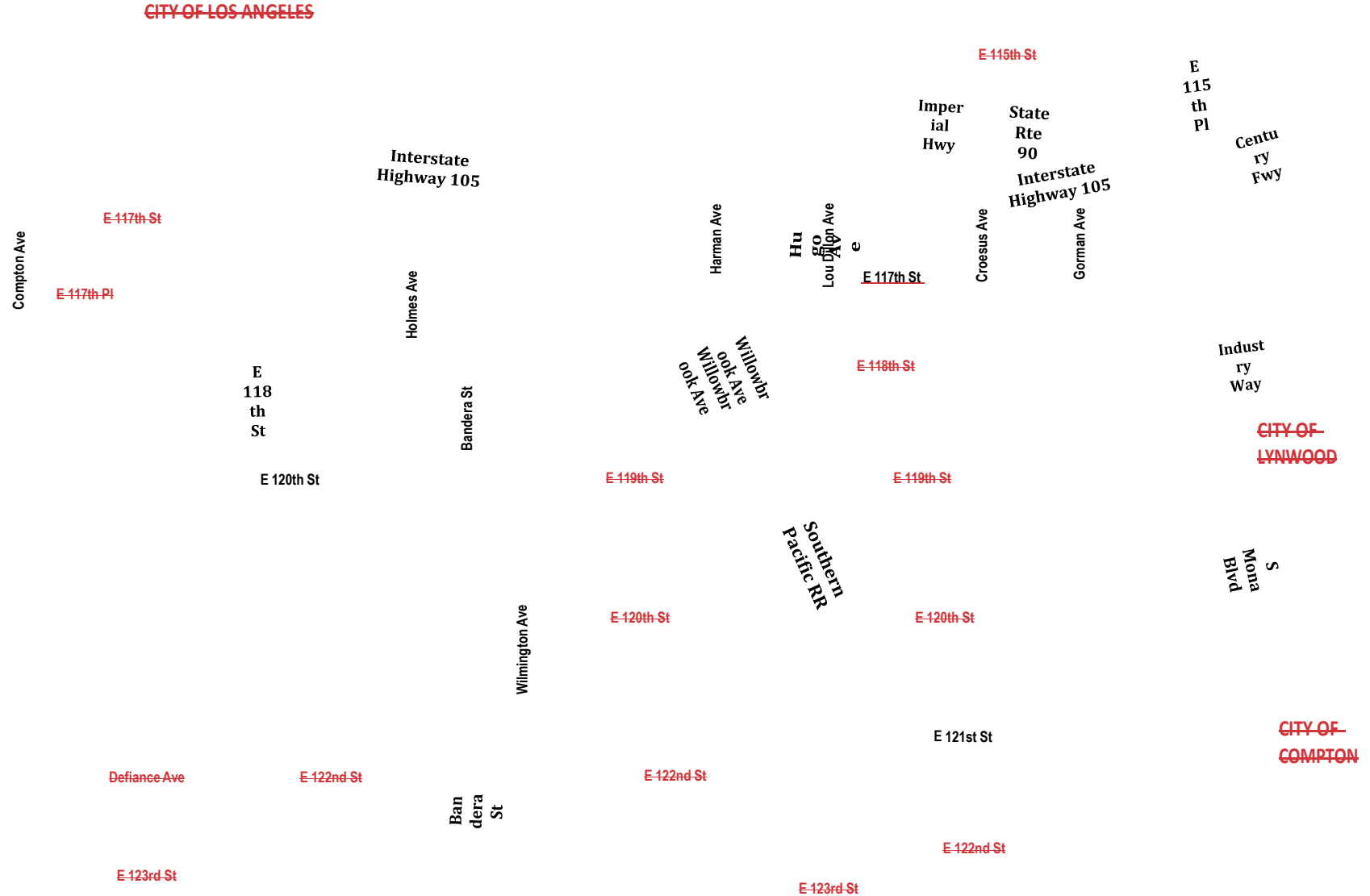
Replace existing water line of 4 inch CI on 118th Street north of this area with 8 inch DI (or PVC C-900).











d
Water System

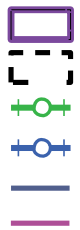
**WILLOWBROOK TOD SPECIFIC
PLAN**

5 minute walk (1,200')

Upgrade 4 or 6" DI Water to 8" DI Water

Foot

0 325 650 1,300 1,500 0



CITY OF LOS ANGELES

E 415th St

Interstate
Highway 105

Imper
ial
Hwy

State
Rte
90

Interstate
Highway 10

E 417th St

Compton Ave

E 417th Pl

E
118
th
St

Holmes Ave

Bandera St

Harman Ave

Hu

GO
Lou
Dillon Ave

e

Croesus Ave

E 417th St

Gorman Ave

Willowbr
ook Ave
Willowbr
ook Ave
Willowbr
ook Ave

E 120th St

E 119th St

E 119th St

Southern
Pacific RR

|

|

Wilmington Ave

E 420th St

|

E-120th St

|

Defiance Ave

E 122nd St



E 122nd St

Ban
dera
St

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E 423rd St

E 423rd St

|

County of Los Angeles
Willowbrook
Metro Blue Line
Existing Sewer S

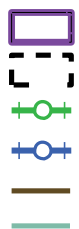
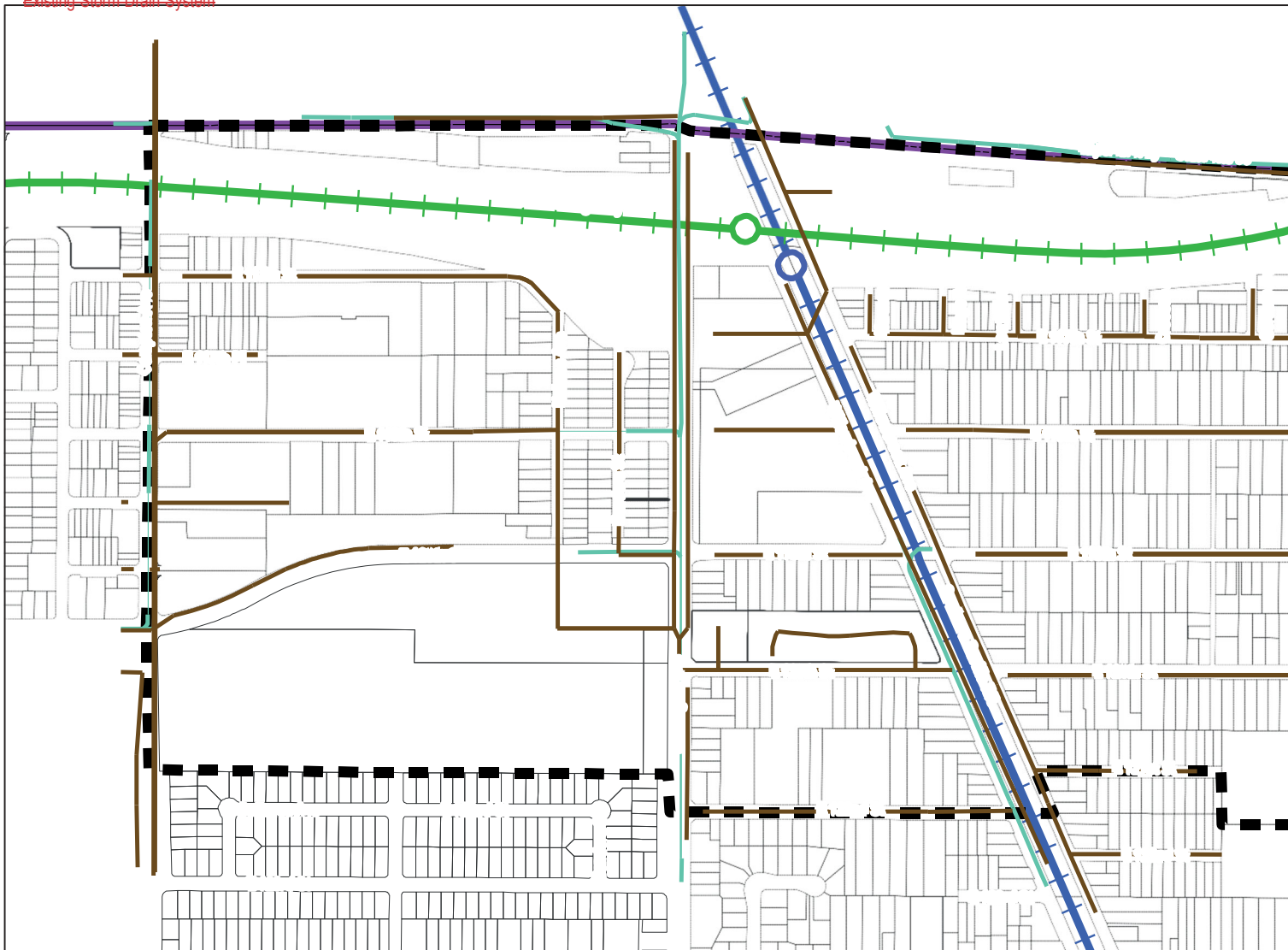
- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Existing Sewer System
- Existing Storm Drain System

Figure 6-3
Existing Sewer and
Storm Drain System

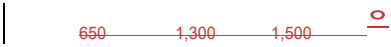
WILLOWBROOK TOD SPECIFIC PLAN
5 minute walk (1,200')

Feet

Existing Storm Drain System







Group 3B:

Replace existing water line of 6 inch CI in the alley between Holmes Avenue and Bandera Street with 8 inch DI (or PVC C-900).

Group 3C, 3D, 3E, 3F, 3G:

Upgrade existing water line of 4 inch CI on 117th Street from Compton Avenue to Holmes Avenue and the 4 inch CI in 117th Place with 8 inch DI (or PVC C-900).

Group 8, 9:

Upgrade existing water line of 6 inch CI and 4 inch CI in 119th Street to 8 inch DI (or PVC C-900) from Willowbrook Avenue to Mona Boulevard.

Group 9, 10:

Replace existing water line of 6 inch CI and 4 inch CI in 118th Street with 8 inch CI from Willowbrook Avenue to Mona Boulevard.

All three water purveyors will evaluate future development within their service areas and conduct their own detailed analysis, and will ultimately decide whether the water distribution facilities are required to be upgraded.

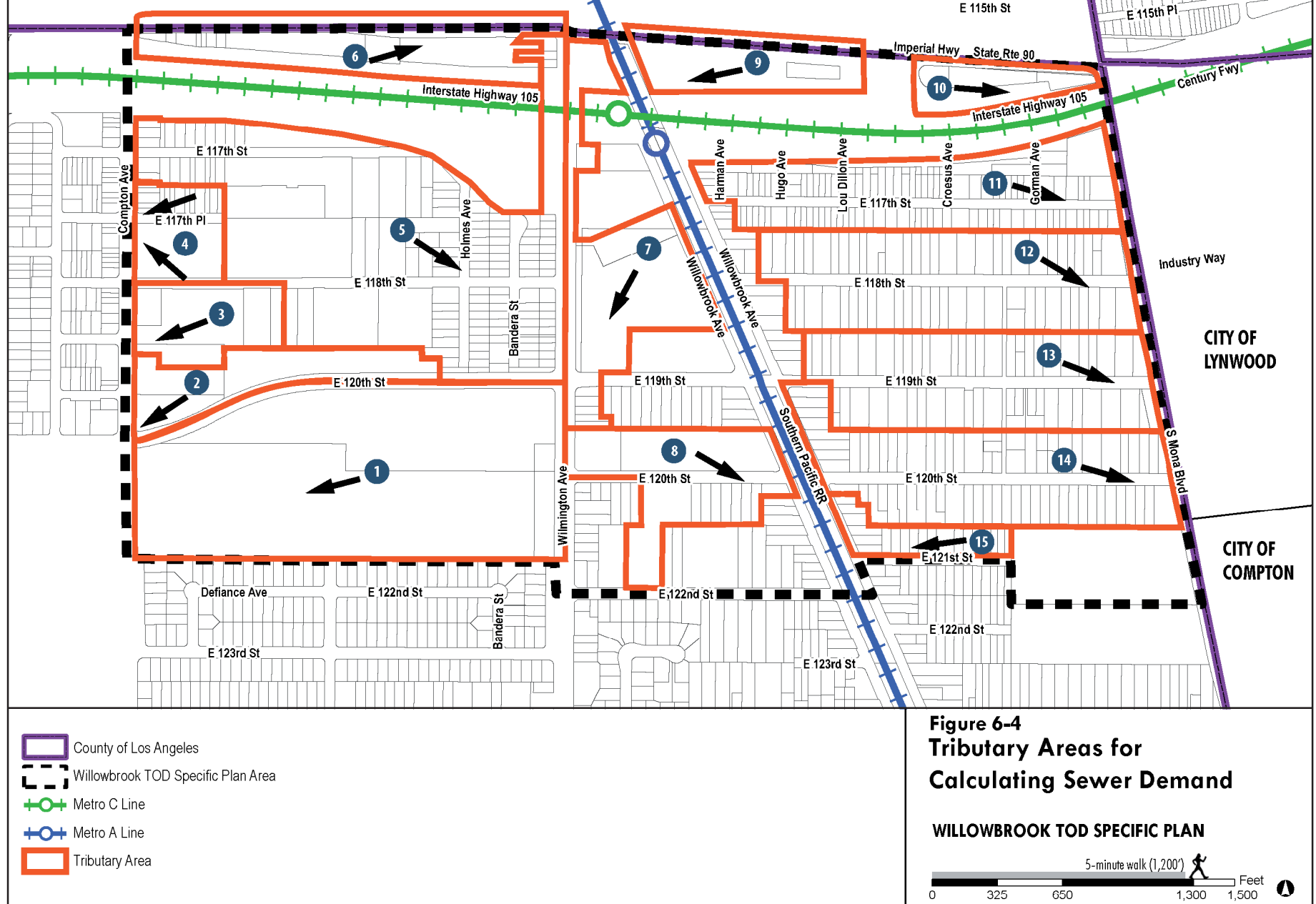
6. 3 Sewer System

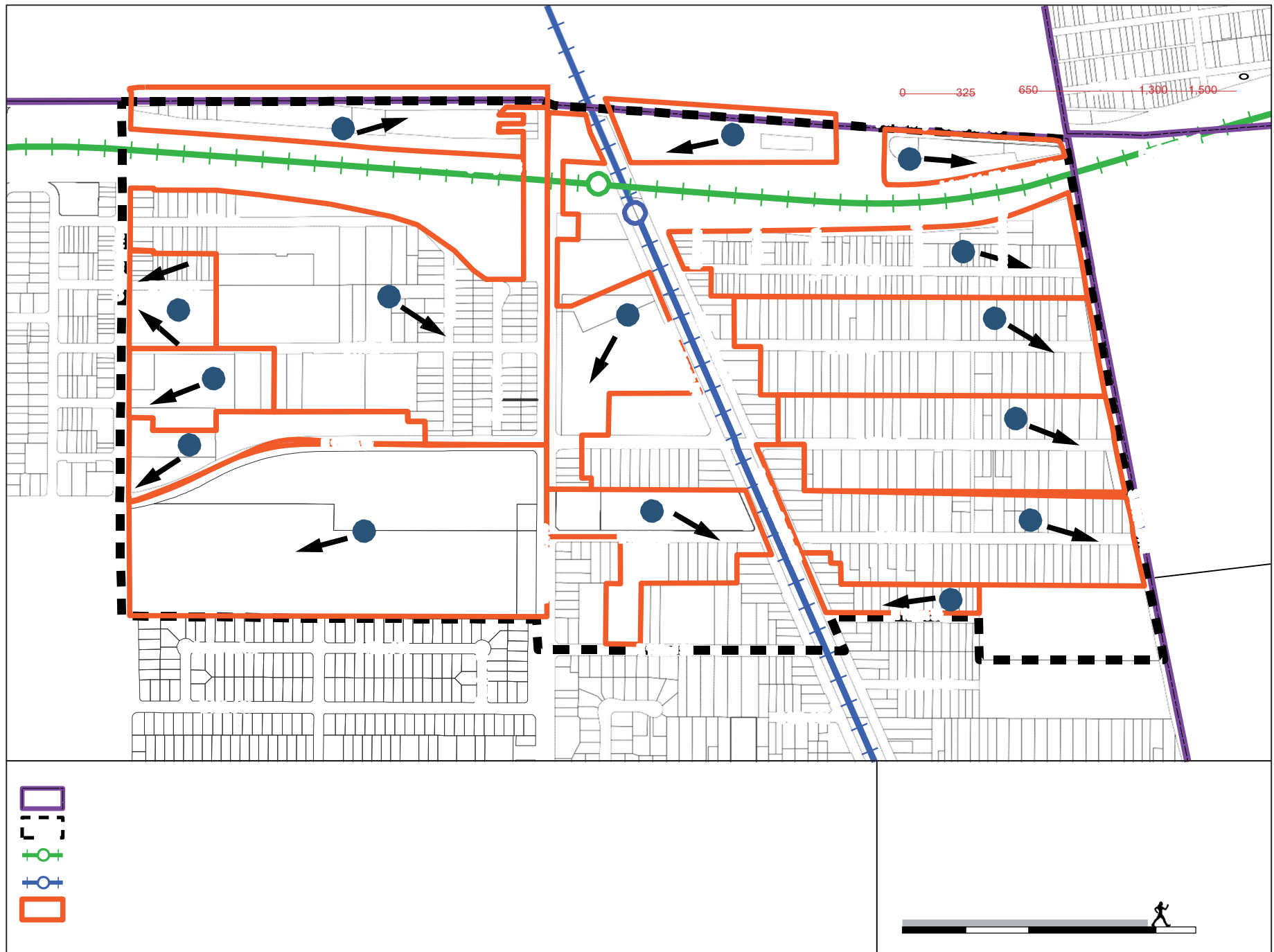
The sewer lines serving the Plan Area are owned and maintained by DPW and the Sanitation Districts of Los Angeles County (LACSD). The sewer lines within the area are mainly Vitrified Clay Pipes (VCP). The sewer mains are all under the major streets with smaller lateral lines connecting directly to residential buildings or businesses (Figure 6-3).

As mentioned earlier in the water system section, except for Group 14 (which remains unchanged), almost all development capacity groups under total build-out of the Specific Plan have some increase in land use density, which in turn results in an increase in sewage effluent throughout the Plan Area. The cumulative sewage effluent by each Tributary Area (shown in Figure 6-4 and Table 6-1) has been calculated using the Zoning Coefficient for runoff provided for a typical sewer area study by the DPW Land Development Division. All existing sewer mains in the Plan Area are 8" or above in diameter. Using the minimum allowable slope of 0.24 percent, the design capacity for the existing sewer mains are at least 0.27 cfs (some pipes are larger than 8 inch). There are five (5) existing sewer collection points that may not be adequate to meet the future cumulative sewage effluent.

One of the purposes of this study is to identify the deficiencies, if any, of the existing utilities when compared to the future developed condition. This study has identified that the developments in Tributary Areas 5, 7, 8, 11, and 16 have the potential to exceed the existing sewage capacity of their connecting sewer mains. The rest of the groups should remain below the sewage capacity.

Since the design capacity refers to future full build-out conditions there is no real justification to upgrade any of these sewer mains at this time. Regardless, DPW will require a detailed sewer area study of any new development within Tributary Area 5. The sewer area study will determine whether any upgrade will be required for sewer system network at the time of that specific development. The LACSD will also review all new sewer infrastructure upgrades as DPW will send improvement plans for the district to review.





**Table 6-1:
Sewer Area Study Calculations by Tributary Area**

Street Name	Pipe		Area (Acres)	Calculated Flow (cfs)	Cumulative Calculated Flow* (cfs)	Cumulative Depth (inches)	Comment
	Size (inches)	Slope (%)					
Tributary Area 1 (Compton Ave)	8	0.24					<i>Not Part of Calculations</i>
Medical Buildings & Residential (R-3)			N/A				<i>Private Sewer Line</i>
Tributary Area 2 (Compton Ave)	8	0.24			0.1836	3.2	
Commercial			3.60	0.0540			
High School (1,675 people x 20 gal/day x 2.5)			N/A	0.1296			1,600 students & 75 staff
Tributary Area 3 (Compton Ave)	8	0.24			0.1350	2.8	
Residential (R-3)			4.50	0.1350			
Tributary Area 4 (Compton Ave)	8	0.24			0.1041	2.4	
Lincoln ES 2.5 x ((280 students x 15 gal/day) + (50 staff x 20 gal/day))			N/A	0.0201			280 students & 50 staff
Mixed-Use 2 (60 du/acre)			1.40	0.0840			
Tributary Area 5 (Wilmington Ave)	8	0.24			1.9023	6.4	
Lincoln ES 2.5 x ((280 students x 15 gal/day) + (50 staff x 20 gal/day))			N/A	0.0201			280 students & 50 staff
Charter ES 2.5 x ((336 students x 15 gal/day) + (35 staff x 20 gal/day))			N/A	0.0222			336 students & 35 staff
Mixed-Use 2 (60 du/acre)			31.00	1.8600			
Tributary Area 6 (Imperial Hwy)	8	0.24			0.1061	2.5	
Commercial			7.07	0.1061			
Tributary Area 7 (Wilmington Ave)	8	0.24			0.4437	5.4	
Mixed-Use 1 (30 du/acre)			14.79	0.4437			
Tributary Area 8 (Willowbrook Ave)	15	0.12			2.7945	12.1	
Residential (R-3)			11.69	0.3507			
Residential (R-2)			3.60	0.0720			
Residential (R-1)			2.86	0.0257			
Tributary Area 9 (Willowbrook Ave)	8	0.24			0.0356	1.4	
Commercial			2.37	0.0356			
Tributary Area 10 (Mona Blvd)	8	0.24			0.0323	1.4	
Commercial			2.15	0.0323			
Tributary Area 11 (Mona Blvd)	8	0.24			0.2887	4.2	
Residential (R-3)			0.99	0.0297			
Residential (R-2)			12.95	0.2590			
Tributary Area 12 (Mona Blvd)	8	0.24			0.1654	3.1	
Residential (R-1)			18.38	0.1654			
Tributary Area 13 (Mona Blvd)	8	0.24			0.1516	3.0	
Residential (R-1)			16.84	0.1516			
Tributary Area 14 (Mona Blvd)	8	0.24			0.2006	3.4	
Residential (R-3)			2.17	0.0651			
Residential (R-1)			15.05	0.1355			
Tributary Area 15 (Willowbrook Ave)	8	0.24			0.1310	2.8	Manhole 48
Residential (R-3)			3.70	0.1110			Includes 117th to 119th
Residential (R-1)			2.22	0.0200			
Tributary Area 16	12	0.24			1.0468	7.1	Trunk Line
Residential (R-3)			19.00	0.5700			at Oris and Willowbrook
Residential (R-2)			16.64	0.3328			
Residential (R-1)			16.00	0.1440			

* Based on current land use and coefficients per DPW.

Sewer laterals are sewer pipes which connect with buildings with sewer mains that run in the street. In general, new or upgraded sewer laterals are required for new buildings, with the cost of the installation borne by the developers.

6.4 Storm Drain System

The storm drain system serving the Plan Area is maintained by the Los Angeles County Flood Control District (LACFCD). The storm drain system within the Plan Area consists of mostly underground Reinforced Concrete Pipes (RCP) ranging from 24 to 54 inches (Figure 6-3).

Build-out of the Plan Area will generate little or no increase in runoff to the existing drainage system, since the area is completely developed. Approximately 80-90 percent of the existing Plan Area is impervious. It is predicted that the new development will not directly trigger any need for upgrades to the County's existing storm drain major backbone facilities, mainly due to the Low Impact Development (LID) Ordinance requirements for percolation and on-site detention for new development, which will stabilize and/or even reduce runoff in the Plan Area.

There are no recommendations to upgrade the existing storm drain system within the Plan Area.

LID Practices and Project Requirements

In October 2008, the County adopted the LID Ordinance as part of Los Angeles County Code Title 12, Chapter 84 to require the use of LID principles in all development projects except road and flood infrastructure projects. Unlike traditional stormwater management, which collects and conveys stormwater runoff through storm drains, pipes, or other conveyances to a centralized stormwater facility, LID uses site design and stormwater management to maintain the site's pre-development runoff rates and volumes. The goal of LID is to mimic a site's pre-development hydrology by using design techniques that infiltrate, filter, store, evaporate, and detain runoff close to the source of rainfall.

LID practices or stormwater quality control measures can be categorized into the following types:

- *Retention-based stormwater quality control measures*
Examples: bio-retention, infiltration basin, dry well, permeable pavement.
- *Bio-filtration*
Example: bio-filtration area.
- *Vegetation-based stormwater quality control measures*
Example: stormwater planter (or planter box), vegetated swale, green roof.
- *Treatment-based stormwater quality control measures*
Examples: sand filter, constructed wetland, proprietary treatment control measures.

All new development that requires new grading in the Plan Area will require preparation of a hydrology study to demonstrate that building sites are free from flooding hazard. New development will be required to mimic the site's pre-development runoff by choosing the appropriate LID practice most suitable for the site. A proposed project must demonstrate that any proposed improvement, including filling, does not raise the flood level upstream or downstream of the project. As required by the County, developers shall prepare National Pollution Discharge Elimination System (NPDES) reports, such as the Water Quality Management Plan (WQMP) and Stormwater Pollution Prevention Plan (SWPPP), to ensure the quality of water is preserved and adverse environmental impacts are minimized.

6.5 Electrical System

Southern California Edison (SCE), which is a private utility company, provides electrical power service to the Plan Area. SCE sets its own service standards, with oversight from the California Public Utilities Commission (CPUC), and facility improvement strategies. Currently, there is a network of aerial electrical facilities creating a power grid that supplies sufficient electrical service to the Plan Area. There is no major deficiency or functional problem in the power supply facilities within the Plan Area. The specific locations of the existing underground and overhead electrical lines are illustrated in Figure 6-5.

The decision to upgrade the power supply facilities and the number of upgrades to meet the demand of future development will be determined by SCE in coordination with the County after developers have submitted their building plans. Demand for services and the ability to serve new developments are generally determined on a case-by-case basis.

Most of the electrical distribution facilities in the Plan Area are aerial facilities. The Specific Plan recommends that existing aerial electrical facilities be placed underground whenever funding is available. Underground electricity provides higher reliability, is safer in general, and is also less unsightly. One of the potential funding mechanisms is CPUC Rule 20. CPUC Rule 20 is a set of policies and procedures established by the CPUC to regulate the conversion of overhead electric equipment to underground facilities, a process often referred to as “undergrounding.” Rule 20 determines the level of ratepayer funding for different undergrounding arrangements.

CPUC Rule 20

Under Rule 20, undergrounding projects are financed by utility rate money, combined rate funds and local tax proceeds, or private funds, depending on whether Rule 20A, Rule 20B or Rule 20C provisions apply.

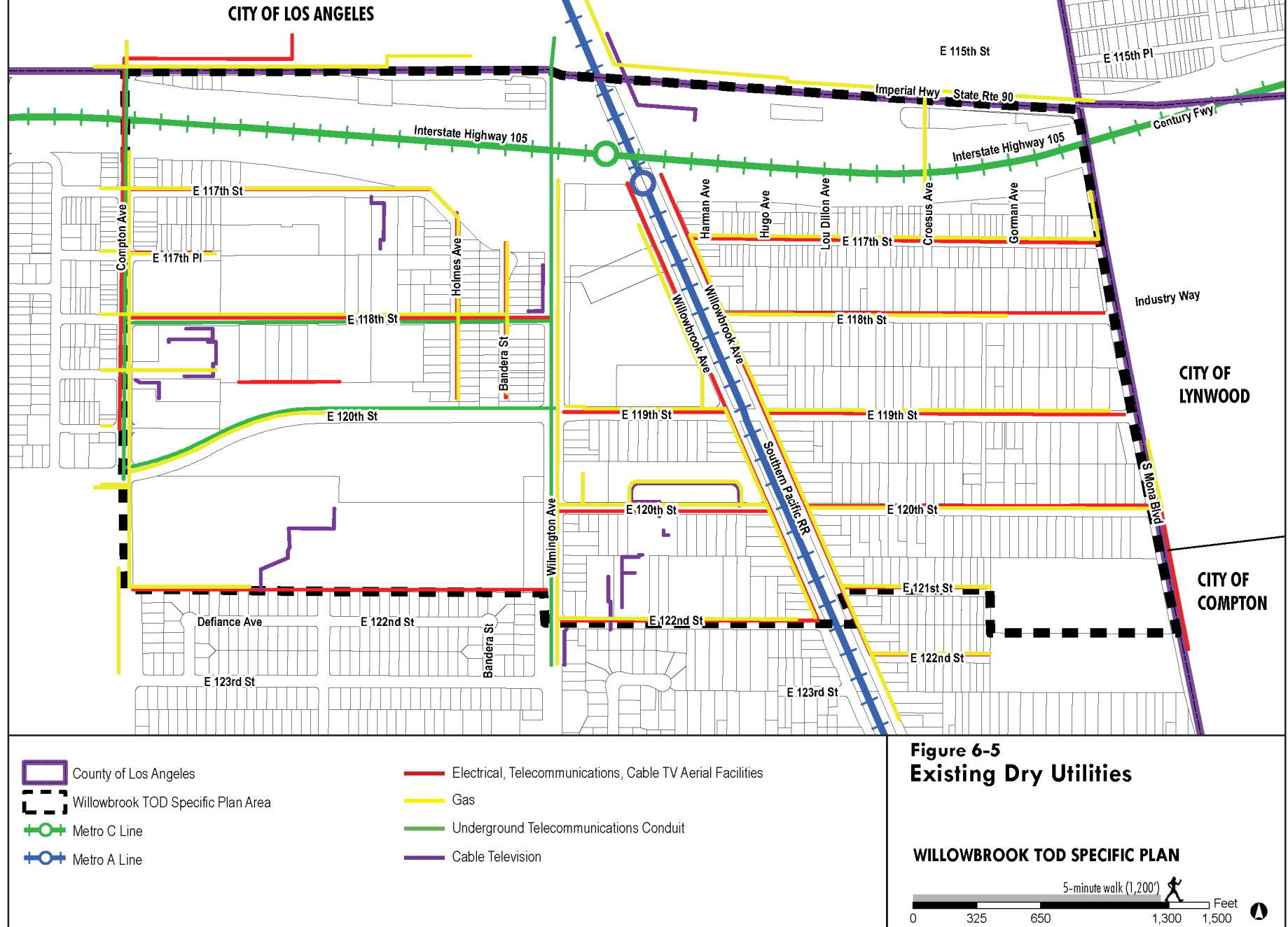
Rule 20A: *Rule 20A projects are paid for by all SCE customers and ratepayers, not just those who live in locations where facilities will be undergrounded. To qualify for full funding through utility rate proceeds, projects must produce a benefit to the general public, not just customers in the affected area, by satisfying one or more of these criteria:*

- *The location has an unusually heavy concentration of overhead facilities.*
- *The location is heavily traveled.*
- *The location qualifies as an arterial or major collector road in a local government’s general plan.*
- *The overhead equipment must be located within or pass through a civic, recreational or scenic area.*

Using CPUC formulas, SCE allocates rate funds to communities for undergrounding based on previous allocations, the ratio of customers served by overhead facilities to all the customers in the community, and the fraction that customers in the community represent of all SCE customers. Local governments use these formulas to project allocations, which allow them to prioritize projects and develop project schedules. Because funds are limited, local governments sometimes must wait and accumulate their allocations before starting an undergrounding project.

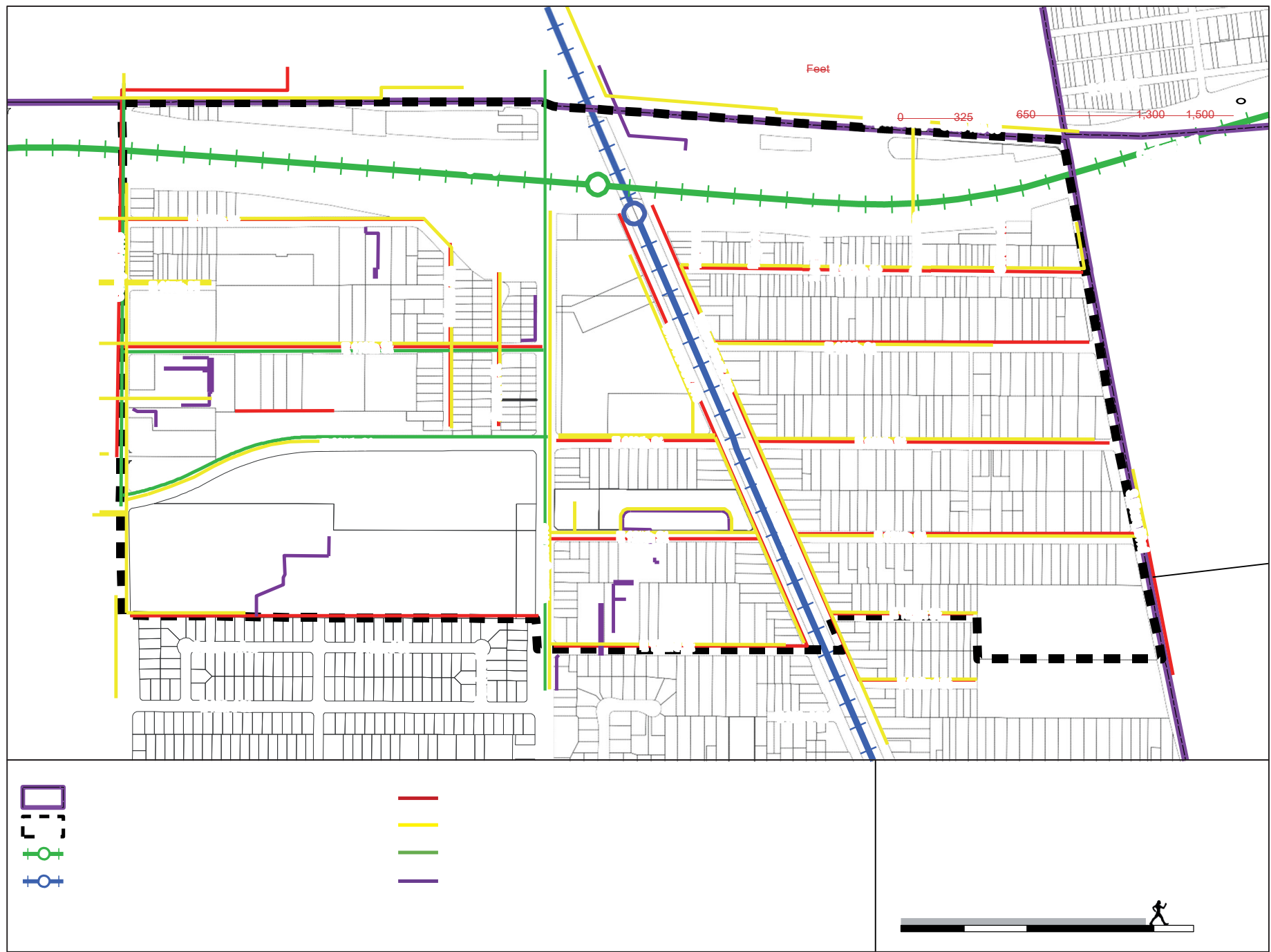
Rule 20B: *If an area is not eligible for Rule 20A or if local government cannot or chooses not to rely on the Rule 20A allocation process, Rule 20B allows rate funds to subsidize an undergrounding project. The subsidy includes an amount equal to about 5 to 20 percent of the total cost. The remaining cost is funded by local governments or through neighborhood special assessment districts.*

Rule 20C: *Rule 20C enables property owners to pay for undergrounding electric lines and equipment if neither Rule 20A nor 20B applies.*



Existing Dry Utilities

WILLOWBROOK TOD SPECIFIC PLAN



It is also worth noting that, prior to any undergrounding process, SCE will take the lead in contacting the other low voltage utilities that might be sharing the power poles with SCE aerial facilities, such as the telecommunication provider (AT&T) and cable TV provider (Spectrum) to coordinate and execute a joint trench. During this process, SCE will also reach out to DPW, Public Utility Division, for their assistance in contacting and coordinating with the aforementioned utility providers.

It is recommended that prior to approving new development, the County should contact the SCE regional manager in the Willowbrook area and set up an inspection at the new development site to determine whether the existing aerial SCE facilities in the area qualifies for Rule 20A funding.

6.6 Natural Gas System

The Southern California Gas Company/Sempra Utilities (The Gas Company), which is a private utility company, is the gas service provider for the Plan Area. Currently, gas pipelines are in all major streets in the Plan Area, specific locations of gas pipelines are illustrated in Figure 6-5.

The analysis on the capacity and capability to meet future demand will be conducted by The Gas Company in coordination with the County upon submittal of building plans by developers.

6.7 Telecommunications System

The Plan Area is within the service area of AT&T, which is a private utility company. AT&T is the provider of both local and long distance telecommunications in the Plan Area (Figure 6-5). The current local telecommunications network lacks the high speed internet service that will be in demand by future businesses and residents in the area.

AT&T will assess the demand for services and the ability to serve new developments on a case-by-case basis. The capacity and capability analysis for meeting future demands within the Plan Area will be conducted by AT&T after building plans are submitted by developers.

If there is “undergrounding” of existing aerial telecommunication facilities, SCE will reach out to AT&T and coordinate a joint trench. To save on costs, AT&T is likely to participate in the joint trench operation. SCE will dictate the layout of the trench, and the telecommunication lines would follow. According to the representative from AT&T, there are no major facility upgrades planned in the Plan Area in the near future. However, if there are facilities upgrades in the future, in most cases, AT&T will be responsible for the construction cost up front, and will recoup the cost later with the additional revenue from added customers.

6.8 Cable Television System

The cable service provider for this area is Spectrum, which is a private utility company. Most of the existing cable TV systems within the Plan Area are aerial facilities that share power poles with the electrical system, with a few existing underground conduits (Figure 6-5).

New land uses resulting from the Specific Plan will consist of both commercial and residential development; therefore, high speed internet and cable television services will be in demand. Similar

to a telecommunications system, Spectrum will assess the demand for services on a case-by-case basis and ultimately make the decisions concerning upgrades for the existing cable TV systems to meet the demand of the future developments.

If there is undergrounding of existing aerial telecommunication facilities, SCE will reach out to Spectrum and coordinate a joint trench. Similar to a telecommunications provider, Spectrum is likely to participate in the joint trench operation to save on construction costs.

In most cases, if there are facilities upgrades required, Spectrum will be responsible for the construction costs up front, and recouping the cost later on with the additional revenue from the added customers.

Economic Development Strategy

7.1 Introduction

The Plan Area is expected to undergo sizable growth in employment to support uses related to the hospital, medical offices, education and research and development, and retail and commercial offices. The purpose of this chapter is to provide the Economic Development Strategy for the Willowbrook TOD Specific Plan. The strategy provides economic analysis, a financing strategy, and policy recommendations to realize the full potential of Metro's investments in the redesigned Willowbrook/Rosa Parks Station and the County's investments in the MLK Medical Center Campus. The economic development strategy includes:

- Strategies that leverage public investments within the Specific Plan framework and build upon local assets to revitalize the Willowbrook community;
- Identification of potential community benefits from the implementation of the Specific Plan;
- Projections of employment and wage growth from implementation of the Specific Plan;
- Capital Improvement Program (CIP) that identifies and prioritizes a range of infrastructure and amenity improvements that benefit the community; and
- Financing approaches for the implementation of infrastructure and services.

The SCAG employment and demographic forecasts for the study area and the MLK Medical Campus Master Plan, as well as supporting documents and studies for the MLK Community Hospital and CDU informed the strategies.

7.2 Opportunities for Economic Development

The rehabilitation and expansion of the MLK Medical Campus, the redesign of the Willowbrook/Rosa Parks Station and the potential demand for ancillary medical services that may locate nearby, are the main economic opportunity drivers for the area. It is important to leverage these economic investments to provide employment and housing development opportunities to the Specific Plan area and the greater Willowbrook community.

7.3 Vision for a Community of Health and Wellness

The revitalized MLK Medical Campus is a catalyst for broader economic development and community service benefits to the Willowbrook and neighboring communities. By facilitating bicycle and pedestrian improvements and increasing transit access and housing choices, this Specific Plan helps realize the community's vision for health and wellness. This vision was inspired by ~~an~~ a ULI Technical Assistance Panel study in 2009. The ULI panel identified the entire master-planned area as having the potential to become a health-focused, transit-oriented district.

The Plan Area has several interconnected subareas located to the west of the rail tracks and south of the freeway: 1) MLK Medical Center Subarea encompassing the MLK Medical Center campus; 2) CDU Campus Subarea encompassing CDU and King Drew Magnet High School; 3) Northwest Subarea including Lincoln-Drew Elementary School, Barack Obama Charter Elementary School, a proposed community college workforce training center, and an expanded County library; 4) the Kenneth Hahn Plaza Subarea; 5) single and multi-family homes and sites with retail and mixed-use development potential on Wilmington Avenue; and 6) the Willowbrook/Rosa Parks Station Subarea.

7.4 Economic Development Strategy

The Economic Development Strategy recommends implementation strategies. Key components of the strategy are discussed in this section and summarized in Table 7-1. The steps and actions within the recommended implementation strategy are contingent upon funding and allocation of resources. The timeline identified is a relative estimate in comparison to other steps and actions within the strategy. Other key components are public financing and economic incentive methods for implementing the conceptual CIP in the Plan Area, as presented in Chapter 8.

The Economic Development Strategy presents a range of public and private financing techniques and related programs that could be used as part of the implementation strategy for the CIP, including land use incentive strategies, financing program initiatives, and public-private partnerships that build upon the substantial public and private investment into the MLK Community Hospital, CDU and the redesign of the Willowbrook/Rosa Parks Station.

Additionally, the CIP compiles the required public infrastructure and related costs to implement the Specific Plan. The CIP also identifies the projects that are essential to attract private investment within the Plan Area, and presents short-, mid- and long-range priorities over the anticipated build-out of the Plan Area.

Furthermore, the Economic Development Strategy presents five categories of actions to facilitate local economic development objectives. These categories highlight actions available to local County government to promote the vision of the Specific Plan. These actions include: a) institutional arrangements; b) targeted development opportunities; c) land use planning; d) infrastructure and services planning; and e) implementation and financing strategies.

**Table 7-1:
Implementation Strategy**

Implementation Step ²	Key Action ²	Coordinating Agencies	Timeframe ¹
Develop Institutional Arrangements	<ul style="list-style-type: none"> Work with the MLK Health and Wellness Community Development Corporation (MLK-CDC). 	County of Los Angeles Community Based Stakeholders	Short-Term
Foster Public-Private Partnerships	<ul style="list-style-type: none"> Coordinate with MLK Community Hospital and Charles Drew University to integrate local workforce training and capabilities with ultimate job growth, direct spin-off activities in healthcare services and allied office uses. 	County of Los Angeles MLK Community Hospital Charles Drew University MLK-CDC	Ongoing
Improve Kenneth Hahn Plaza	<ul style="list-style-type: none"> Work with Kimco Realty Corporation, the operator of Kenneth Hahn Plaza, to enhance the retail and dining experience and coordinate pedestrian, safety, and landscape improvements at the Plaza. Assist in tenant recruitment. Work with Kimco Realty Corporation to explore mixed-use development. 	County of Los Angeles Kimco MLK-CDC	Ongoing Ongoing Long-term
Continue Partnership with Los Angeles County Workforce Collaborative	<ul style="list-style-type: none"> Continue to collaborate with the Workforce Collaborative's initiative to coordinate workforce training programs for residents with public and private employers. 	County of Los Angeles LAC Workforce Collaborative LACDAG	Ongoing
Create Opportunities for Infill Mixed-Use Development	<ul style="list-style-type: none"> Coordinate with existing property owners on economic development opportunities of under-utilized and vacant sites. Coordinate with Compton Unified School District(CUSD) regarding the redevelopment of the school sites. Coordinate with the LACDAG regarding the use of the Successor Agency-owned parcels. 	County of Los Angeles LACDAG	Mid- to Long- Term
Pursue Local, State and Federal Infrastructure Funding Sources	<ul style="list-style-type: none"> Identify, monitor, and apply for other government funding sources, for infrastructure and transportation improvements (also see Tables 7-2, 8-2 and 8-3). 	County of Los Angeles LACDAG Metro	Ongoing
Establish an Enhanced Infrastructure Financing District (EIFD)	<ul style="list-style-type: none"> Conduct feasibility analysis and necessary elections for establishing an EIFD for financing public improvements and amenities. 	County of Los Angeles LACDAG	Short- to Mid- Term
Establish a Development Impact Fee (DIF) Program	<ul style="list-style-type: none"> Conduct feasibility analysis for establishing a DIF Program for equitably spreading infrastructure costs equitably over new development. 	County of Los Angeles LACDAG DPW/DRP	Short- to Mid- Term
Establish a Parking Management District in the Northwest Subarea	<ul style="list-style-type: none"> Consider a parking management plan in the Northwest Subarea to improve the availability of parking supply and pricing. 	Partnership between County of Los Angeles and Major Property Owners/Developers	Mid- to Long- Term
Establish a Business Improvement District (BID)	<ul style="list-style-type: none"> Marketing/promotional events/security/maintenance above existing public services; small scale capital improvements. Assessments must be benefit based. 	Property Owners/Business Owners	Mid- to Long- Term

1.The timeline identified is a relative estimate in comparison to the other actions included in the strategy.

Short-Term: 0-2 Years; Mid-Term: 2-5 years; and Long-Term: > 5 years;

Ongoing: occurring annually over development period.

2. The recommended steps and actions are contingent upon funding and allocation of resources.

Source: Stanley R. Hoffman Associates, Inc.

A. Institutional Arrangements

Community Development Corporation. Established in 2015, the MLK Health and Wellness Community Development Corporation (MLK-CDC) aims to develop an “urban family-health campus” adjacent to the new MLK Community Hospital, starting with re-purposing the former King-Drew hospital building into the MLK Community Wellness Center that will house a variety of service providers, including mental health, family services, job training and creation, outpatient clinical, bio- science, physical wellness, and neighborhood place-making.

The Specific Plan recommends the expansion of the MLK-CDC to provide a broader coverage of the Plan Area. The expanded MLK-CDC would direct the visioning, formulation and implementation of economic development strategies/plans and offer an effective means to achieve Specific Plan goals. The expanded MLK-CDC would operate under the auspices of the County with stakeholder roles at the county and community levels through adequate representation in executive roles. The MLK-CDC could take on the critical function of directing and coordinating various actors and available resources in pursuing targeted economic development opportunities for real estate development and identifying infrastructure and services in coordination with other partners. The MLK-CDC could also play a central role in workforce development through skills gap assessments and through leveraging existing programs at the national, state and county levels and instituting new ones, as necessary.

Public-Private Partnerships. Public-private partnerships provide effective means in the current constrained economy to leverage limited resources and achieve mutually desired goals. Potential partnerships between local and regional public agencies include those with specialized institutions in education, health care and research and development such as the MLK Community Hospital and CDU. Partnerships with the private and non-profit sectors enable the integration of local workforce capabilities with the ultimate job growth. Expanding these partnerships will help achieve the Specific Plan goals.

Los Angeles County Workforce Collaborative. Initiative for a Competitive Inner City (ICIC) is underway to help coordinate national, state and county wide workforce training programs, and for targeted collaboration with key stakeholders, including the private sector. Continuing to build on the work of the Workforce Collaborative helps the Specific Plan goals of improved employment opportunities for Willowbrook residents.

B. Targeted Development Opportunities

Several specific and targeted development opportunities have been identified for Willowbrook under the Willowbrook TOD Specific Plan, including:

Leveraging Health Care and Education Opportunities. Ongoing collaboration with the MLK Community Hospital should be leveraged for spin-off activities in health-care services and allied office uses. Similarly, ongoing partnership with CDU has the potential to spur research and development activities in healthcare and science programs for employers who want or need to be part of the medical community around MLK Medical Center.

Kenneth Hahn Plaza. The Plaza offers an opportunity for enhancing the retail and dining experience in the community through store location and enhanced design and capital improvements; the shopping center also has the potential for mixed-use, transit-oriented development.

C. Land Use Planning

Land Assembly. Land assembly incentives become important, particularly in a post-redevelopment environment, where encouraging the consolidation of smaller parcels into a larger, developable area results in more efficient development. While land assembly incentives are limited in the Willowbrook area, one incentive is the expedited processing of development that furthers the goals and objectives of the Specific Plan vision.

Use of Successor Agency Parcels. The economic development strategy recommends the catalytic use of parcels inherited by the successor agency to the County redevelopment agency to further incentivize the Specific Plan goals and objectives.

Reconfiguring Publicly Owned Parcels. Land resources already owned by agencies should be assessed for reconfiguration and consolidation for new development opportunities. Local public schools in the Northwest Subarea of the Plan Area have the potential to be reconfigured to make additional land resources available for private sector development under either a lease or sale arrangement.

D. Implementation and Financing Strategy

Various financing and implementation methods are viewed as important in the long-term context of the Specific Plan vision for a health and wellness community that builds on MLK Medical Center and CDU investments and attracts both residential and non-residential development, preferably with a medical employment related focus. These actions, their primary uses, responsible parties for implementation and phasing are summarized in Table 7-2.

Some of the techniques need to start early to build resources for the future, such as an Enhanced Infrastructure Financing District (EIFD), and others may be implemented after some development has occurred, such as a Parking Management District or a Business Improvement District (BID), but are still considered important as part of the overall strategy. Some of the techniques are exclusively for capital financing or operations and maintenance, and some of these techniques can provide both funding approaches. The strategy also includes the early establishment of public-private partnerships that can foster confidence in the attractiveness and vision of the Plan Area.

Core Strategies. The core, early strategies include maximizing infrastructure financing through an EIFD and establishing a development impact fee program for essential infrastructure, such as parking or street improvements, that spreads the costs equitably over new development. Also considered important in the short-term is the expansion of the non-profit MLK-CDC to direct the visioning, formulation and implementation of Specific Plan goals and objectives. Additionally, part of the mid- to long-term strategy would be to create a parking management plan that could address both the long-term supply and operations side of the parking system, including unbundling the cost of parking from the underlying rent or lease payments.

Along with maximizing these funding sources, the CIP for the Specific Plan, as discussed conceptually in this Chapter 7, identifies top community priorities to facilitate timely provision of public infrastructure and services.

Grant Funding Opportunities. Ongoing grant funding opportunities at the local level include Cap-and-Trade under the Low Carbon Transit Operations Program and the Metro and Transportation Development Act (TDA) funding. The Metro and TDA grant opportunities are generally funded through

**Table 7-2:
Summary of Potential Financing Techniques**

Financing Methods	Primary Uses	Coordinating Agencies	Timeframe ¹
Development Impact Fee (DIF)	<ul style="list-style-type: none"> Capital improvements necessitated by new development. AB 1600 requires that a reasonable relationship or “nexus” must exist between the amount of the impact fee and the project on which it is imposed. 	County of Los Angeles	Short- to Mid- Term
Special Benefit Assessment	<ul style="list-style-type: none"> Capital improvements necessitated by new development. Assessments must be benefit based. 	County of Los Angeles	Mid- to Long- Term
Landscape Maintenance District (LMD)	<ul style="list-style-type: none"> Landscaping improvements. Landscaping operations and maintenance. LMD assessments must be benefit based. 	County of Los Angeles	Mid- to Long- Term
Mello-Roos Special Tax	<ul style="list-style-type: none"> Capital improvements. Operations and maintenance. Can finance projects and services with both local and general benefits. 	County of Los Angeles	Mid- to Long- Term
Business Improvement District (BID)	<ul style="list-style-type: none"> Marketing/promotional events/security/maintenance above existing public services; small scale capital improvements. Assessments must be benefit based. 	Property Owners/Business Owners	Mid- to Long- Term
Community Development Block Grant (CDBG)	<ul style="list-style-type: none"> Federal program that provides direct annual grants to cities, counties, and states across the country. Grants are intended to revitalize disadvantaged areas, expand affordable housing and economic opportunities. 	County of Los Angeles Department of Regional Planning (DRP)	Mid- to Long- Term
Enhanced Infrastructure Financing District (EIFD)	<ul style="list-style-type: none"> Use of County's future ad valorem property tax increment for financing public improvements and amenities. Requires a 55 percent vote of the electorate to incur bonded indebtedness; no increase in local property taxes. 	County of Los Angeles	Short- to Mid- Term
Community Revitalization and Investment Authorities (AB 2)	<ul style="list-style-type: none"> Similar to an EIFD, AB2 allows use of the County's property tax increment for capital financing. Subject to a majority protest vote and subject to certain conditions related to serving disadvantaged communities. 	County of Los Angeles	Short- to Mid- Term
Community Development Corporation (CDC)	<ul style="list-style-type: none"> Community development corporations (CDCs) are 501(c)(3) non-profit organizations for local areas. Community-based and provides economic development, education, community and affordable housing services. 	County of Los Angeles/Community-Based Stakeholders	Short- to Mid- Term

Table 7-2 (Continued)

Financing Methods	Primary Uses	Coordinating Agencies	Timeframe ¹
California Infrastructure and Economic Development Bank	<ul style="list-style-type: none"> Infrastructure State Revolving Fund (ISRF) Program provides economic development financing to agencies and non-profit corporations. Bank has broad authority to issue tax-exempt and taxable revenue bonds. 	Governor's Office of Business and Economic Development	Mid- to Long- Term
Parking Management District	<ul style="list-style-type: none"> Local districts that regulate parking supply and rates to meet the parking needs of the area. They can promote transit use, ridesharing, and alternative modes of transportation. 	Partnership between County of Los Angeles and Major Property Owners/Developers	Mid- to Long- Term
Cap-and-Trade (Pursuant to AB 32)	<ul style="list-style-type: none"> AB 32 authorizes the use of market mechanisms through an annual auction of surplus emissions reduction credits to generate funds, a portion of which can be targeted for clean transportation and sustainable community plans. The Low Carbon Transit Operations Program provides funding assistance for transit agencies to reduce GHG gas emissions and serve disadvantaged communities. 	Caltrans in coordination with the California Air Resources Board (CARB)	Ongoing applications for grant funding
Metro and Transportation Development Act (TDA)	<ul style="list-style-type: none"> Two percent of total TDA funds are allocated for bicycle and pedestrian projects. 	Metro	Bi-Annual Metro "Call for Projects" application process
Metro - Various Grant Programs	<ul style="list-style-type: none"> Congestion Mitigation and Air Quality (CMAQ) funds - bicycle, pedestrian and wayfinding projects. Metro Express Lanes Net Toll Revenue Re-Investment Grant Program - transit uses and roadway improvements. Metro Wayfinding Signage Grant Pilot Program. Metro Open Streets Program - regional car-free events. Surface Transportation Program - bicycle and pedestrian improvements and on-system roadway improvements. 	Metro	Ongoing applications for grant funding
California Capital Investment Incentive Program (CIIP)	<ul style="list-style-type: none"> Existing State program that provides property tax abatement for up to 15 consecutive years for large capital investment by qualified manufacturing facilities (must exceed \$150 million). 	County and Governor's Office of Business and Economic Development	
Other Outside Grants and Loans	<ul style="list-style-type: none"> Potential TOD grants from State's Strategic Growth Council (SSGC). SCAG Sustainable Communities Planning grants. Business façade improvement program grants. Affordable housing grants. 	DRP; SSGC; SCAG	Ongoing applications for grant funding

1. The timeline identified is a relative estimate in comparison to other actions included in the strategy.
Short-Term: 0-2 Years; Mid-Term: 2-5 years; and Long-Term: > 5 years;
Ongoing: occurring annually over development period.

local sales tax revenue measures on an annual or biannual cycle and have their own competitive application process. Other grant funding opportunities are also available at the federal, State and regional levels.

Additionally, programs such as the State of California's Infrastructure and Economic Development Bank (I-Bank) provides low-cost loans for qualified local programs; and Community Development Block Grant (CDBG) funds – while CDBG funds have been significantly reduced in recent years – still provide an ongoing source of funding to meet local priorities as part of the overall strategy.

Traditional Financing Techniques. Some of the more traditional financing techniques include: special benefit assessments, Mello-Roos special taxes and landscape lighting districts and are seen as mid- to long-term techniques as significant development begins to occur. The special tax and landscape lighting districts can fund both capital and operations and maintenance costs. Benefit assessments are subject to majority protest of the property owners, and special taxes are subject to 2/3 approval of the electorate.

A BID is a public/private partnership created to perform a variety of services to revitalize and improve commercial neighborhoods above existing local governmental services. Currently, there may not be enough business interest to form a BID, but over the mid- to long-term, interest may increase, particularly if businesses see this as a joint effort to efficiently use their collective funding to increase the quality and overall competitiveness of the area.

Emerging Techniques. Community Revitalization and Investment Authorities (CRIA), which was recently enacted into law, authorizes the revitalization of disadvantaged communities through planning and financing infrastructure improvements and upgrades; economic development activities; and affordable housing via tax increment financing based, in part, on the former community redevelopment law. A CRIA has some similarities to an EIFD, so both would not be implemented. The CRIA does have the power of eminent domain while the EIFD does not; also the CRIA has a number of restrictions that relate to serving disadvantaged communities and providing affordable housing. Finally, the County also has a California Capital Incentive Program (CCIP) that provides property tax abatement for up to 15 years for large capital investments by qualified manufacturing facilities (must exceed \$150 million); however, under an EIFD, this program would probably not be applicable as sufficient property tax increment revenues would likely not be available for both purposes.

7.5 Community Benefits

Community benefits from the implementation of the Economic Development Strategy are summarized below:

Identification and Coordination of Economic Opportunities. One of the pivotal benefits of public-private partnerships is the ongoing leveraging of opportunities that benefit firms/enterprises and the local workforce. A mechanism to implement workforce programs in response to private sector skill demands establishes an approach for realizing the potential for jobs growth and payroll increases within the community.

Capturing Benefits of Large Public Investments. Significant investments within Willowbrook in institutional and public transit development are expected to catalyze future growth in residential and non-residential uses over time. However, capturing and actually realizing these projected benefits depends upon programs in place, including those outlined above, and the establishment of the

EIFD for capital improvements for community recapture of fiscal benefits from increased property valuation. Appropriate land use zoning, such as mixed-use, could further help incentivize the potential around large public investments.

Expanded Business Activities and Local Job Opportunities. A combination of the various initiatives outlined above could result in expanded business activities in spin-off industries associated with health care services and research and development, and an expanded spillover demand for local services, which would also benefit the local community. Land use planning serves a critical role in providing locational opportunities for such activities. The community would benefit from the expansion of better paying jobs in coordination with appropriate skills development initiatives, as discussed previously.

Better Transit Access and Jobs-Housing Balance. Public transit investments additionally provide the benefit of improving two-way access to jobs and housing opportunities. The ability of workers to access job opportunities in other parts of the County has the benefit of increasing community prosperity. On the flip side, local firms' ability to access a qualified workforce from a larger commute shed is an important factor in locational choice in addition to availability of land. Expanded housing opportunities over time could have the stabilizing benefit of improving the match of jobs and housing within the sub-regional economy. Capital improvements connecting and improving local community access to transportation infrastructure is an important community benefit in this regard to realize the above economic outcomes.

Affordable Housing. With the loss of redevelopment agency affordable housing set-aside funds, the decline in available affordable housing funds has been substantial. The County, through its Housing Element, will continue to address existing and new sources of funding for affordable housing programs, such as new market housing tax credit programs and programs that are currently being considered by the California State legislature, but have not yet been adopted.

Enhancing Overall Quality of Life. An overarching community benefit of expanded jobs, access to transit, and improved local services, local public infrastructure, and public amenities will enhance local quality of life.

Increasing Public Safety. The proposed street and access improvements under this Specific Plan would also have the benefit of improving public safety. A neighborhood with expanded business activities, work opportunities, transit access, and housing opportunities would likely reinforce community identity and citizen participation in local events, resulting in greater social and economic cohesion.

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Capital Improvement Program

8.1 Introduction

The conceptual CIP is summarized in Table 8-1 and outlines the general categories of improvements, estimated costs, responsible agencies and phasing. The CIP sets forth an ideal scenario and establishes priority projects contingent on the availability of funding. The total estimated costs in this conceptual CIP scenario are about \$73.3 million. The bulk of these costs are for the Willowbrook/Rosa Parks Station relocation and improvements in the Kenneth Hahn Plaza by Metro. The phasing for this work is identified as short-to-mid range, depending upon when the funding resources become available.

The major local infrastructure and accessibility costs are estimated for Streetscape Improvements at about \$16.8 million, followed by about \$3.5 million for Transportation-related Improvements. These improvements will be overseen primarily by DPW and funded by a variety of other sources that are presented in this chapter. The phasing for this work is estimated for short-to-mid to long- range. Again, this work depends on the availability of funding, particularly if the newly adopted EIFD legislation is used; this is because the EIFD's bonding capacity will increase over time as property tax increment increases with new development.

Finally, Public Parking Facility Improvements are identified as possible incentives for cost reductions for private sector development within the northwest quadrant. However, no specific plans or garage cost estimates are available at this time. An option for full construction of a public garage might be cost sharing garage subsidies with the private sector. The phasing of these costs is seen as mid-to-long- range because it is assumed that it will take five years or longer to create public infrastructure and amenities to begin to draw private sector investment.

8.2 Potential CIP Improvements

A detailed listing of potential transportation and related improvements is presented in Table 8-2 along with an identification of responsible lead and partner agencies, potential funding sources and phasing. Table 8-3 presents the potential utility improvements by private or enterprise utility companies also with their responsible lead companies and partner agencies, potential funding sources and phasing.

**Table 8-1:
Summary of Capital improvement Plan Priorities**

Improvement	Estimated Cost	Coordinating Agencies	Timeframe ¹
Land Assembly ²	N/A	County and School Districts	Short- to Mid-Term
Transit-Related/Transit-Station Improvements	\$ 53,000,000	Metro ³	Short- to Mid-Term
Transportation Improvements	\$ 3,456,500	County of Los Angeles Dept. of Public Works (DPW)	Short- to Mid-Term
Streetscape Improvements	\$ 16,835,315	DPW	Short- to Mid-Term
Public Parking Garage Improvements/Subsidies	N/A	Potential Parking District ⁴	Mid- to Long-Term
TOTAL	\$ 73,291,815		

1. The timeline identified is a relative estimate in comparison to other improvements in the CIP.

Short-Term: 0-2 Years

Mid-Term: 2-5 Years

Long-Term: >5 Years

2. Potential land resources are available from the successor agency to redevelopment and from public school districts possibly reconfiguring their local campuses.

3. About \$10.25 million from the U.S. Department of Transportation (DOT) TIGER competitive grant funds is currently available; TIGER is an acronym for Transportation Investment Generating Economic Recovery.

4. A parking management district will incentivize private development through reduction in parking costs and Management of parking supply.

Source: Stanley R Hoffman Associates, Inc.

**Table 8-2:
Streetscape and Transportation: Improvements, Responsibility, Potential Funding Sources**

Improvements	Coordinating Agencies	Potential Funding Sources	Timeframe ¹
Transit-related Improvements			
Upgraded Metro Rosa Parks Transit Station	Metro	Metro	Short-Term
Vehicular/Pedestrian Improvements			
Road diet on Mona Boulevard	County of Los Angeles Department of Public Works (DPW)	<ul style="list-style-type: none"> • Metro Call for Projects • Cap and Trade • County of Los Angeles Transportation Funds • Federal and State grants and loans • Transportation Development Act Funds 	Mid-Term
Road diet on Willowbrook Avenue West (between the Metro Station and 119th Street)	DPW		Mid-Term
Traffic signal - intersection of Mona Blvd. and 119th Street - <i>Traffic signal analysis and subsequent implementation</i>	DPW		Short-Term
Traffic signal - intersection of Wilmington and 122nd Street - <i>Traffic signal analysis and subsequent implementation</i>	DPW		Mid-Term
Pedestrian signal - intersection of Mona Boulevard and 120th Street - <i>Pedestrian signal analysis and subsequent implementation</i>	DPW		Short-Term
Pedestrian sidewalk paving on the west side of Willowbrook Avenue West between Metro Station & 119th Street	DPW / Metro		Mid-Term
Pedestrian sidewalk improvements on Wilmington Avenue between Imperial Highway and 117th Street (both sides)	DPW		Mid-Term
Pedestrian sidewalk improvements along 119th Street between Wilmington Avenue and Willowbrook West (north side only)	Private development		Mid-Term
Pedestrian Oriented Improvements Package (Standard Improvement package) – 12 locations <i>Standard Improvement package includes:</i> <ul style="list-style-type: none"> - add zebra-style crosswalks to all crossings - add pedestrian countdown signals and audio signals to all crossings - add passive pedestrian detection to all crossings - add advanced stop bars to all approaches - add bulb-outs or reduce curb returns on corners where feasible 	DPW		Mid-Term

Table 8-2 (Continued)

Bicycle Improvements			
Class I bicycle path and pedestrian trail on Mona Boulevard	DPW	<ul style="list-style-type: none">• Metro• DPW• Cap and Trade• Federal and State Grants	Mid-Term
Class I bicycle path/Class IV cycle track on Willowbrook Avenue West	Metro/DPW		Short-Term
Class II bicycle lane on Imperial Highway between Compton Avenue and Mona Boulevard	DPW		Mid-Term
Class II bicycle lane on Wilmington Avenue between 120th and 124th Streets	DPW		Mid-Term
Class III bicycle route signage/stripping on Compton Avenue, 119th Street (between Wilmington & Mona), and Willowbrook Avenue West (south of 119th Street)	DPW		Mid-Term
Implement Bicycle Stations	MLK, CDU, Private Development		Short- to Mid- Term
Streetscape Enhancements			
Wilmington/119th/120th Streetscape Project (DPW-funded)	DPW	<ul style="list-style-type: none">• Metro• DPW• Business Improvement District• Landscape and Lighting District	Short- to Mid-Term
Street trees on Specific Plan streets not included above	DPW		Long-Term
Pedestrian-scaled lighting on Specific Plan streets not included above	DPW		Long-Term
Street furniture	DPW		Mid-Term
Other Mobility Related Actions			
Community contest to rename 120th Street between Compton and Wilmington Avenues	DPW	<ul style="list-style-type: none">• Metro• County of Los Angeles• CDC	Short-Term
Setup Transportation Demand Management Program	DPW		Short-Term
Parking			
Parking Management District	DPW	<ul style="list-style-type: none">• County of Los Angeles, Enhanced Infrastructure Financing District• Future parking revenues	Mid-Term
	Property and Business Owners		

1. The timeline identified is a relative estimate in comparison to other identified improvements.

Timeline: Short-Term: 0-2 years; Mid-Term: 2-5 years; Long-Term: >5 years.

Source: Stanley R. Hoffman Associates, Inc., The Arroyo Group, The Mobility Group

Table 8-3:
Utilities: Improvements, Responsibility, Potential Funding Sources

Improvements	Oversight Responsibility - Lead and Partner	Potential Funding Sources	Phasing ¹
Evaluation of the need for water distribution lines upgrades in the Specific Plan area	Liberty Utilities, Golden State Water Company, and City of Los Angeles Dept. of Water and Power	Water Companies	Short- to Mid-Term
Detailed sewer study for Group 1	DPW and MLK Community Hospital	DPW through impact fees and connection charges	At time of development in Group 1
Other sewer related review by required by Sanitation Districts of Los Angeles County (LACSD)	Developers and LACSD	Development Impact Fees	Ongoing
Evaluation of demand for electrical services	<i>Primary responsibility:</i> Southern California Edison (SCE)	SCE through its utilities charges, as well as state and federal grants and	Ongoing
Evaluation of demand for telecommunication services	<i>Primary responsibility:</i> AT&T	AT&T funded through monthly phone charges	Ongoing
Evaluation of demand for cable services	<i>Primary responsibility:</i> Spectrum	Spectrum through its utilities charges, as well as state and federal grants and loans	Ongoing
Joint trenching with all utility providers	<i>Primary responsibility:</i> SCE <i>Secondary responsibility:</i> County of Los Angeles oversight and other private dry utilities companies	SCE in coordination with other utility providers	Ongoing

1. *Short-term:* 0-2 years.

Mid-term: 2-5 years.

Long-term: >5 years.

Source: Stanley R. Hoffman Associates, Inc., JMC² Civil Engineering

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22.46.3102 Appendix

A. References

1. Table 22.46.3102-A, below, identifies outdated references to Title 22 used in the Willowbrook TOD Ordinance and the following equivalent references in this Title 22.

TABLE 22.46.3102-A: REFERENCE KEY TO TITLE 22	
<i>Code Reference in Specific Plan</i>	<i>Title 22 Code Reference</i>
Chapter 22.16	Part 9 of Chapter 22.40
Chapter 22.18	Chapter 22.20
Chapter 22.20	Chapter 22.28
Chapter 22.110	Chapter 22.48
Chapter 22.112	Part 12 of Chapter 22.52
Chapter 22.114	Part 10 of Chapter 22.52
Chapter 22.126	Part 20 of Chapter 22.52
Chapter 22.158	Part 1 of Chapter 22.56
Chapter 22.172	Part 10 of Chapter 22.56
Chapter 22.180	Part 7 of Chapter 22.56
Chapter 22.192	Part 14 of Chapter 22.56
Chapter 22.352	Section 22.44.125
Section 22.26.020	Part 14 of Chapter 22.40
Section 22.140.030	Part 32 of Chapter 22.52
Section 22.140.200	Section 22.28.080
Section 22.140.220	Part 25 of Chapter 22.52
Section 22.140.290	Sections 22.40.465.G
Section 22.140.320	Part 19 of 22.52
Section 22.140.330	Section 22.56.1754

Section 22.140.410	Section 22.28.070.G
Section 22.140.420	Section 22.40.485.L
Section 22.140.560	Part 12 of 22.52
Section 22.140.600	Part 3 of 22.20

Unless specified in Subsection A.1, above, all other references to Title 22 are to the current version of Title 22 (Planning and Zoning Code) of the County Code.